

SMITHTOWN PLANNING BOARD

1 reduce nitrogen levels by 80 to 90 percent,
2 compared with the current onsite septic
3 systems, which have a negligible impact
4 upon nitrogen.

5 In fact, as the graph -- which you
6 will see up there on there on the west side
7 of the room -- as the graph depicts, the
8 future nitrogen loading from the proposed
9 development, which you will see to the
10 right there in the green bar, will be
11 approximately 70 percent less than the
12 as-of-right development, which is the bar
13 in the center of the graph, which would not
14 necessitate a treatment plant, but would
15 actually be less than the current
16 development on the property today, which is
17 the blue graph to the left -- the blue bar,
18 excuse me. Since, again, the nitrogen
19 essentially is not abated in the current
20 onsite septic system.

21 There has also been some discussion
22 regarding how many years it would take for
23 the discharge from the site to reach
24 Stony Brook Harbor. Inasmuch as the
25 nitrogen loading would be less with the

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1 developed property, the short amount of
2 time it actually takes to reach the harbor,
3 the quicker the improvement to the harbor
4 will actually be.

5 Another issue which has been
6 discussed in the media is the Town of
7 Smithtown's potential interest in having
8 the St. James Business District sewered and
9 connected to the proposed Gyrodyne
10 wastewater treatment plant for treatment.

11 In the event that such a sewer
12 district is established and the Town is
13 desirous of such a connection, under this
14 development plan, Gyrodyne would have the
15 capacity to treat such wastewater onsite,
16 and would be willing to discuss such a
17 relationship with the Town.

18 In closing, I'd like to state that
19 at the outset of this project, the Gyrodyne
20 board and executive team gave a clear
21 mandate to the Cameron team: To formulate
22 a development plan that responds to the
23 opportunities in delivering transformative
24 benefits through the implementation of a
25 smart, responsible, and sustainable

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1 development plan.

2 The development plan should be
3 sensitive to the distinctive attributes of
4 the property including: Its physical
5 landscape; road frontage of approximately a
6 half a mile along Route 25-A, a historic
7 corridor, the Washington spy trail;
8 Gyrodyne's history; and a plan recognizing
9 the opportunity to respond to community and
10 regional needs.

11 The DEIS document details how and
12 why this project will deliver on that
13 mandate. We look forward to hearing the
14 comments, questions, and concerns of the
15 community. Thank you very much.

16 **CHAIRMAN DeSORBE:** Thank you.

17 **BOARD MEMBER MARCHESI:** Thank you.

18 **CHAIRMAN DeSORBE:** This meeting is
19 being recorded. If anyone would like to
20 comment, please speak into the microphone,
21 and state your name and address for the
22 record.

23 Keep your comments to the Draft
24 Environmental Impact Statement only. And
25 please be brief and do not repeat comments

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1 made by previous speakers.

2 Right now, I would like to welcome
3 the Town of Brookhaven Supervisor, Mr. Ed
4 Romaine to comment on this matter.

5 **SUPERVISOR ROMAINE:** I would like to
6 thank this Board for giving me an
7 opportunity. I am not a resident of
8 Smithtown. I'm the supervisor of the
9 adjoining town, the Town of Brookhaven,
10 which the entire boundary -- the eastern
11 boundary of this project sits. To give me
12 an opportunity to speak, I appreciate that.
13 I have long remarks. I printed it out. I
14 am going to try to abbreviate them. We
15 have a great number of speakers. I will
16 try to keep this as brief as I possibly
17 can.

18 I'm here because this project will
19 impact the communities of northeastern
20 Brookhaven in a far-reaching and possibly
21 devastating way.

22 When this proposal was first put
23 forward, I submitted extensive comments
24 regarding the scope of the Environmental
25 Impact Statement. To date, I have not been

SMITHTOWN PLANNING BOARD

SEQRA-1
cont.

1 contacted regarding any of these concerns,
 2 nor have any of these changes been made to
 3 the plan reflecting these important issues,
 4 let alone -- this alone would make the DEIS
 5 flawed under the existing rules that SEQRA
 6 created.

7 This 75 acre project will
 8 undoubtedly be the largest development in
 9 the Smithtown area -- Smithtown/Brookhaven
 10 area for the next generation. It is taking
 11 place directly on the border of my town,
 12 within 300 feet of the Stony Brook historic
 13 district, and dependent on the
 14 infrastructure of Brookhaven Town.

15 I was interested to hear people talk
 16 about Stony Brook Road; it's in my town.
 17 No one ever talked to me about it. No one
 18 ever talked to me about it. Think about
 19 that. Put yourself in my place.

20 It's going to affect our
 21 groundwater, and yet there was absolutely
 22 no outreach by the developer or his
 23 consultants to address the concerns that I
 24 presented this Board. I had a
 25 representative come and give you written

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1 comments, and sent another letter in
2 expressing our concerns.

3 I know they are talking about a
4 hotel, and under certain circumstances that
5 ECON-2 might make sense; but consider this, we
6 have the Three Village Inn, the Hilton
7 Gardens at Stony Brook, which by the way is
8 planning an 85-room expansion. We have the
9 Stony Brook Holiday Inn Express, and
10 Danfords, and now I understand there may be
11 a hotel on Nesconset Highway where the
12 Watermill Caterers sit that obviously is
13 going to put this industry under a little
14 stress.

15 TR-6 The DEIS contemplates road
16 improvements within the Town of Brookhaven,
17 and contemplates a grade level crossing of
18 the Long Island Railroad, that would feed
19 traffic for this project directly into
20 Brookhaven Town and its roadways.

21 Despite that, we have not been
22 contacted. We know what this will do. You
23 know. You live here, like many of these
24 residents. 25-A is over its carrying
25 capacity. Stony Brook Road is over its

TR-3

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1 TR-3 | carrying capacity. And we are going to add
2 cont. | more?

3 GW-5 | I'm concerned about the harbor,
4 | which is unique, which our towns share; and
5 | Stony Brook creek, which our towns share.

6 EXEC-1 | I'm concerned about the impact of
7 | the sewer treatment plant, which is
8 | addressed in the DEIS -- sewage treatment
9 | plant for this facility. What has not been
10 | addressed for this facility is the impact
11 | of a regional sewage treatment plant and
12 | what that impact would be on groundwater,
13 | on the harbor, on the creek; and that's
14 | something that should be looked at.

15 SEQRA-1 | Coordination of any planning
16 cont. | project -- and you most of all as the
17 | Members of the Planning Board understand --
18 | with your villages and with your adjoining
19 | town should be undertaken.

20 SEQRA-3 | There should be discussions with the
21 | MTA, because the railroad runs right
22 | through this property. There should be
23 | discussions with the State University, and
24 | there has been very little. In fact, we
25 | spoke to the State University earlier today

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1 to confirm the lack of consultation on this
2 project.

3 **GW-6**

4 The density of the proposed
5 development -- the planned construction of
6 regional -- regional, I want to emphasize
7 that -- sewage treatment plant appears to
8 be directly in contrast to the low-density
9 zoning of the Village of the Head of the
10 Harbor, and the Village of Nissequogue, and
11 the Avalon Park, which had enacted
12 low-density zoning to preserve the local
13 waterways.

14 I would hate to see this Gyrodyne
15 proposal undermine the efforts -- the
16 long-term efforts to protect natural
17 resources simply because they have the
18 zoning classification based on a
19 World War II manufacturing that has ceased
20 to exist many years ago.

21 It is my belief that in Suffolk
22 County Subwatershed Plan, and the United
23 States Environmental Protection Agency's
24 Long Island Sound study, both of these
25 support the need to decrease the density
associated with water quality on this

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GW-6
cont.

75-acre parcel.

TR-3

Based on the existing zoning, an as-of-right development could generate 7,000 new vehicles per day considering -- excuse me -- considerably more than the proposed DEIS addressed.

These are concerns for me. I am definitely concerned about Stony Brook Road, which is overdeveloped at this point. It has too much traffic. I'm concerned that the legacy of this development will be diminished water quality and traffic jams, and people will say, why? Why did this happen? How did this happen?

Well, tonight, we are at ground zero. We are with the people that make the decisions. The people who serve this Board because they are concerned with the future of Smithtown, as I am with the future of Brookhaven.

You have to make a decision about some of these things. Let me talk about some of the things. I know they talked about open space.

By the way, getting back to traffic

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TR-6

1 congestion, my strong suggestion is that
 2 there be a traffic study in conjunction
 3 with Stony Brook University to ensure that
 4 the employees and students can safely
 5 access the university, and that emergency
 6 services can adequately serve the area,
 7 particularly Stony Brook Hospital.

8 We also need that study to take a
 9 look at local road improvements that should
 10 be taken to minimize additional traffic
 11 congestion created. We are at a tipping
 12 point. We are at a tipping point at 25-A.
 13 Ask anyone that has to drive from
 14 Stony Brook to Smithtown.

15 Also, I asked one of my attorneys
 16 whose husband does that trip. She said, it
 17 is worse and worse. You can't make it.
 18 Depending on the time of day, a trip that
 19 should take 15 minutes can take 45 minutes,
 20 and probably more. And these people can
 21 probably speak to the truth of that issue.

22 **UNIDENTIFIED WOMAN:** A lot longer.

23 **SUPERVISOR ROMAINE:** So you can
 24 hear -- there are facts, there's more
 25 facts, and then you what proceeds after

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OPEN-4

that. Okay. And that's what we have to think about, the commonsense of this.

Right now, they talk about open space. They talked that 36.5 acres or 48.7 percent of this site will be open space. That's not accurate. Think about that.

I will tell you why: No one element of the open space plan that was presented actually preserves open space on the site.

Open space is defined as undeveloped; free from residential, commercial, industrial, or institutional use, something that provides scenic beauty, cultural value, historic significance. Production of food and forest products, outdoor recreation, protection or restoration of ecological functions, wildlife diversity, and habitat for endangered plants and animal species, and mitigations of natural hazards such as flooding.

The open space is not 141 land bank parking stalls with the potential to be opened in the future and paved over with asphalt, nor is it the leaching fields that

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OPEN-4
cont.

you are going to use for the sewage treatment plant. That is not open space. And if you define that as open space -- so you can have engineers, you could have consultants, you could have everyone present it, but that is not the fact of what open space represents.

I will try to do it quickly.

ALT-1

I know that we have seen alternatives presented, and as you know, the title of this DEIS is a Map of Flowerfield Subdivision Application. Every single alternative that has been presented is the exact same subdivision map. The only thing that changes in the alternatives is the mix of use. That's not a subdivision alternative.

What the applicant has provided here is a site plan alternatives in the DEIS. That does not meet that standard. That does not meet the standard of providing alternatives.

GW-40

The STP location is where, right on the boundary of Brookhaven. Right next to single-family homes. That doesn't make

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GW-40
cont.

1 good planning. That makes planning where
2 you put two absolutely different types of
3 zoning together where you put an industrial
4 use such as a regional sewage treatment
5 plant next to single-family homes. That
6 doesn't make sense.

7 I know this property is interesting,
8 and I have to commend the people who own
9 this property. I think the State
10 University was buying it for about \$25
11 million. They wound up paying \$167
12 and-a-half million for 245 acres. So let
13 me say to the owners, you obviously know
14 how to cut a deal or get the maximum money
15 out of this.

16 But we are not talking about a deal
17 now. We are talking about the shape of our
18 community. We are talking about a
19 community where we live, and what we want
20 it to look like, and what we think it is
21 going to be after this is built in the
22 future, where our traffic is going to be,
23 what is the impacts to our groundwater.
24 Those types of things are the type of
25 things that we are talking about.

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AIR-1

One of the things the DEIS should address today, today above all days, is climate change. And I looked for it, and I didn't find it in this DEIS.

SEQRA-1

So clearly, I would have liked to have my Town consulted more. We would not -- I would have liked to have seen some of these issues addressed by the developer and his engineers, who are so interesting in presenting their view, to address the questions we raised months ago, years ago, when we sent written correspondence, they were not.

Until those questions are addressed, we will not be in favor of this project, and we will consider all options regarding this, particularly whether all the requirements of SEQRA was met.

I thank you for your attention, and I appreciate the difficult job you have.

Thank you very much.

CHAIRMAN DeSORBE: Thank you, Supervisor Romaine.

We are going to take comments from the rest of the audience. Remember that

SMITHTOWN PLANNING BOARD

1 this meeting is being recorded, and please,
2 when you come up, please state your name
3 and address into the microphone for the
4 record.

5 And also, there is a sign-in sheet
6 right there next to the podium on the
7 table. Can you please print your name and
8 address so that the stenographer can
9 clarify that when she does the minutes.

10 And so now, anybody else would like
11 to be heard on this matter?

12 **BOARD MEMBER MARCHESI:** We will take
13 one at a time in an orderly manner, and we
14 will let ladies go first; how is that?

15 **CHAIRMAN DeSORBE:** Please, after you
16 state your name and address, please sign
17 your name.

18 **MARIA T. HOFFMAN:** My name is Maria
19 Hoffman. I am here to represent
20 Assemblyman Steve Englebright who is in
21 Albany today, and wishes that he could be
22 here.

23 Good evening, Members of the
24 Smithtown Planning Board. I'm here
25 representing Assemblyman Steve Englebright.

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1 He is a member of the New York State
2 Assembly representing the 4th District,
3 which directly adjoins the proposed project
4 area.

5 As a neighbor, he wishes to
6 emphasize that Smithtown and Brookhaven
7 share New York State Route 25-A,
8 Stony Brook Harbor, and a remarkable sense
9 of overlapping cultural heritage and
10 environmental resources.

11 Unfortunately, the proposal before
12 you at this time from Gyrodyne, LLC would,
13 if approved, significantly impact the
14 quality of our life -- the quality of life
15 of our shared neighborhoods and area.

16 I'd like to articulate some defects
17 TR-29 in the DEIS as I see it. The density and
18 detail of what is proposed is profoundly
19 encumberous with our area. The subject
20 property is served by roadways that are
21 wholly inadequate to the proposed intensity
22 of development and use.

23 New York State Route 25-A,
24 Stony Brook Road, and Mills Pond Road are
25 the only streets that access the Gyrodyne

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TR-29
cont.

property; and they were each designed and built for less intensive use than has been imposed upon them already by the surrounding suburban and institutional development.

Stony Brook Road, for example, is already so oversubscribed that at certain times of the day, cars trying to travel it crawl bumper to bumper in a traffic quagmire.

In that Stony Brook Road and the much smaller Mills Pond Road are the only north/south travel routes that could access the Gyrodyne site.

It is clear that the significant new traffic that would be created by the proposed overdevelopment would create a midtown-Manhattan-like traffic nightmare on each of these roads and within the residential neighborhoods that they run through in both Smithtown and Brookhaven Towns.

TR-2

Similarly, New York State Route 25-A is already overcapacity -- already at overcapacity. Inevitably, the proposed

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1 project would further compromise the
 2 predictable safe and efficient passage on
 3 this arterial that directly links together
 4 Smithtown, St. James, Head of the Harbor,
 5 and Old Stony Brook.

HIS-2

6 However, our communities share more
 7 than just a roadway. The agrarian
 8 landscape and historic scenic vistas,
 9 within which Gyrodyne is situated are our
 10 communities -- excuse me, situated, connect
 11 our communities' residents to their history
 12 and heritage.

13 These locally and nationally
 14 significant sites and landscapes attract
 15 tourists, who will appreciate the beauty
 16 and cultural significance, as well as
 17 patronize our local merchants while
 18 visiting.

19 Tourism is Long Island's number one
 20 industry, and any out-of-context approval
 21 of this proposed project that will erode
 22 the esthetic and the cultural appeal of our
 23 shared historic corridor that includes the
 24 National Register of Historic Places, such
 25 as including Deepwells, St. James General

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1 **HIS-2** | Store, the William Sydney Mount House, and
 2 | the Stony Brook Grist Mill is shortsighted
 3 | in the extreme.

4 | The DEIS does not adequately address
 5 | the negative impact that the proposed
 6 | project will have upon these historic and
 7 | conservation-based resources.

8 | Similarly, there are other concerns
 9 | relating to neighboring townships,
 10 | including the reality that every elected
 11 | official who represents the northwest
 12 | corner of Brookhaven Town is opposed to
 13 | Gyrodyne's massive proposal due to its
 14 | outsized impacts on our intermunicipal
 15 | transportation network, Stony Brook Harbor,
 16 | and our shared historical and cultural
 17 | character.

18 **SEQRA-1** | And although this is a
 19 | regional-scale project and the quality of
 20 | life of Brookhaven residents would be
 21 | impacted by the project, the DEIS does not
 22 | speak to or take a hard look at regional
 23 | impacts.

24 **SEQRA-2** | Also of great concern is the absence
 25 | of the collective impact from proximal

SMITHTOWN PLANNING BOARD

SEQRA-2
cont.

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development proposals, including the Bull Run Farm property on Mill Ponds Road, and its proposed build-out into an assisted-living facility, the International Bible Church property on Route 25-A west of Stony Brook Road, and its contract vendee, who intends to build housing, and the uncertainty of the future of the BB&GG Farm.

What we need is a comprehensive Environmental Impact Statement rather than the fragmented parcel-by-parcel approach as evidenced in this site-limited DEIS.

Other shortcomings of the DEIS include the project's growth inducing cumulative impacts upon Stony Brook Harbor.

GW-3

Questions such as: Will the onsite plant become a regional sewer district? And what specific type of sewer system will be purchased and installed? And will it predictively remove nitrate? And what is the specific anticipated time travel for sewer effluent to the harbor?

It should also be noted that unlike any other north shore harbor, New York

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GW-3
cont.

State owns the bottom of Stony Brook Harbor. This almost 900 acres of State ownership is a public trust that the Gyrodyne project and its sewer effluent must not be allowed to violate. What will be done to protect the water chemistry of the harbor?

These and other related meaningful, unanswered questions need to be answered and resolved before the project is allowed as proposed to seal the fate of Stony Brook Harbor.

Similarly, and in a larger sense, the Gyrodyne project as proposed must not be allowed to negatively impact the quality of life of our two towns.

In its present form, I am strongly opposed to this project, urge you to reject the present DEIS as submitted, and ask that you require and pursue with Brookhaven Town a cumulative impact statement that would adequately address the collective regional impact of all the nearby projects, including but not limited to the Gyrodyne proposal before you this evening, and that

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1 are now being simultaneously contemplated
2 or advanced within both towns.

3 Thank you for your consideration.

4 Signed Assemblyman Englebright.

5 **BOARD MEMBER MARCHESI:** Thank you.

6 **CHAIRMAN DeSORBE:** Thank you.

7 **RICHARD MURDOCCO:** Good evening. My
8 name is Richard Murdocco, and I am here to
9 give my comments on the Draft Environmental
10 Impact Statement for the proposed
11 subdivision of the 75-acre Gyrodyne
12 property.

13 **BOARD MEMBER MARCHESI:** We need your
14 address, also, please.

15 **RICHARD MURDOCCO:** Crab Apple Lane,
16 Commack, New York.

17 For those of you who do not know me,
18 I write on land use and real estate
19 development issues. My award-winning
20 published work is regularly seen in
21 publications such as Newsday, the New York
22 Daily News, Crain's New York Business, the
23 Real Deal, and my site, the
24 TheFoggiestIdea.org. I also teach
25 graduate-level courses on economic

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1 development and urban planning.

2 As part of its work, the Foggiest
3 Idea regularly reviews the policy actions
4 taken by local, state, and federal
5 governments that impact communities
6 throughout Nassau and Suffolk Counties.

7 Being that Gyrodyne is one of the
8 last large tracts of developable land in
9 western Suffolk County, any development at
10 this particular site is regionally
11 significant.

12 **SEQRA-9**

In my formally submitted remarks to
13 the Planning Board earlier this week, which
14 are available both online and has been
15 published by local media outlets, I noted
16 that the DEIS is an extensive document, but
17 outsized concerns remain unaddressed by the
18 draft, including the impacts to neighboring
19 municipalities and on local watersheds.

20 Tonight I would like to focus on two
21 distinct elements: One, comprehensive
22 impacts; and two, the intermunicipal
23 relations between the towns of Brookhaven
24 and Smithtown.

25 In recent years, local governments

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SEQRA-9
cont.

1 have been isolating the discourse around
 2 proposals of regional significance. In
 3 Huntington, policymakers found their
 4 concerns regarding Heartland Town Square
 5 unaddressed by their neighbors in Islip.
 6 Smithtown must not follow Islip's example
 7 by failing to listen to Brookhaven's
 8 concerns. In the coming years,
 9 intermunicipal cooperation will help
 10 determine our regional successes.

11 Moving forward, I urge policymakers
 12 in both towns to work with the Gyrodyne
 13 company in crafting a proposal that meets
 14 our region's collective goals of
 15 environmental and economic sustainability.
 16 Development actions on our island are not
 17 isolated for they resonate far beyond
 18 municipal borders.

19 As such, the comprehensive impact of
 20 any development effort at Gyrodyne in
 21 relation to other proposed projects must be
 22 more fully understood. Given
 23 infrastructural, environmental limitations
 24 that exist around the parcel in question,
 25 the DEIS fails to give us a comprehensive

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SEQRA-9

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understanding of the regional implications of the applicant's proposal.

In closing, I am confident that all interested parties can come together to craft a vision that works for these 75 acres. As I argued on the pages of Newsday in 2018, quote, Long Island's municipalities must stop pursuing patchwork economic development strategies and come together to properly chart the region's future.

Thank you.

CURT CROLEY: Good evening. My name is Curt Croley. I reside at 49 Quaker Path, Stony Brook with my wife.

Ladies and gentlemen of the

GW-40

Smithtown Planning commission, I stand before you and the rest of these good people here as a concerned homeowner. I have no association with any -- affiliation with any group or anything like that.

I stand before you as a homeowner, and I have no doubt in my mind that while

ECON-10

there is great diligence done by the engineering team and your group, I stand

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1 ECON-10
cont.

before you concerned that if this project goes forward, my property values go down. All right. There is absolutely no doubt about it in my mind.

5 SEQRA-1

All right. So I spent 20 years, my wife and I, building a property, living in peace, investing in our community. And I am deeply concerned that it does not seem to me that there is a collaboration between Smithtown and Brookhaven in something that is the right on the border between our two municipalities.

Now, if you Google map Gyrodyne to the center of St. James, the St. James Post Office is 1.8 miles. If you Google map Gyrodyne to Smithtown -- to the Stony Brook Post Office, it's 2 miles.

18 GW-40
cont.

So it seems to me this proposal is opportunistic based on available property, but you are placing a sewage treatment plant equidistant, smack dab in between the two centers of two beautiful towns.

And I can't help but wonder if there has been enough diligence about the sewage treatment plant, the runoff, the -- all of

SMITHTOWN PLANNING BOARD

1 **OPEN-8** | the impacts are going to happen so close to
 2 | these municipalities' downtown centers.
 3 | Right.

4 | So I appreciate you giving me the

5 **TR-2** | opportunity to speak here. I know that
 6 | there is a lot of people talking about
 7 | traffic, which is also a concern of mine,
 8 | but I won't speak about that.

9 | I just know that this is going to
 10 | adversely affect me and my family. And I
 11 | urge you -- I urge to rethink this
 12 | proposal.

13 | Thank you.

14 | **JOYANN CIRGLIANNO:** Good evening.
 15 | My name is Joyann Ciriglianno. I am a
 16 | Smithtown resident. 9 Attridge Drive,
 17 | Kings Park, New York.

18 | Thank you for this opportunity to
 19 | comment. I'm also a representative for the
 20 | Audubon Society. And I am a half-century
 21 | resident of Smithtown. I am the product of
 22 | the Smithtown School District. And I have
 23 | resided the past 27 years in Kings Park.

24 | I'm chapter president for the Four
 25 | Harbors Audubon Society, into which

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1 territory Gyrodyne falls. Additionally, I
 2 sit on the Board for the Audubon Council of
 3 New York State. And I am an ecosystems
 4 specialist, and a consultant for Audubon
 5 New York.

OPEN-4

6 Lately, I seem to be speaking out
 7 quite a bit on the last few large parcels
 8 of our Long Island open spaces. I'm not
 9 going to say that the site should not be
 10 developed. I am going to say that if this
 11 site is developed, the planning of the
 12 development must be considered extremely
 13 carefully.

14 This is one of the last remaining
 15 large open spaces in Smithtown, and its
 16 zoning was put into place before the area
 17 was so heavily developed and our waterways,
 18 groundwater, soil and open spaces were put
 19 under so much pressure, to continue
 20 functioning properly with dwindling
 21 environmental resources.

22 We are running out of open spaces.
 23 Open spaces does not mean people's yards,
 24 parking lots, farm lands, which is not
 25 really good for wild habitat. Open spaces

OPEN-4
cont.

SMITHTOWN PLANNING BOARD

1 are functioning ecosystems which provide
2 habitat and environmental services that
3 most of us so rarely think about like clean
4 air, clean water, food, and other things
5 that we can't do without.

6 The last remaining true open spaces
7 are necessary, and should be developed with
8 care. We are reaching a number of tipping
9 points with soil, water, and our ecosystems
10 and wildlife.

11 As for the Gyrodyne property, 41
12 percent of the existing site is already
13 developed. The plan allows for 51 percent
14 of the total area to be developed with 49
15 percent left as open space. This leaves
16 only an additional 10 percent currently
17 available to be developed. In looking at
18 the proposed site plan, it certainly
19 appears that less than 49 percent is
20 designed true open space.

21 In order for open space to be most
22 viable for habitat services, it needs to be
23 contiguous or connected. In the plan, the
24 open space is not contiguous. It is
25 fragmented. This is not conducive to

SMITHTOWN PLANNING BOARD

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OPEN-4
cont.

wildlife and biodiversity.
In the report, land bank parking, landscaping, and lawn are considered open space. Land bank parking in this calculation of open space should not be considered, as land bank parking can be open and looked upon in the future.

In addition, landscapes and lawns are fertilizer and irrigation dependant, and are not functioning ecosystems. Non-native, ornamental plantings do not provide biodiversity. They are managed landscapes, and they cannot be counted as open space. They are not a habitat for wildlife and diversity.

GW-37

In one area, the proposed plan shows a doubling of the amount of lawn from 6 percent to 12 percent. The study also states that there is going to be a 4 percent increase in nitrogen load. This may not sound like a lot, but the doubling managed turf, which takes both fertilizer and water, it may become an environmental issue as the Long Island north shore is approaching a nitrogen load tipping point.

SMITHTOWN PLANNING BOARD

GW-37
cont.

1 Basically, what this means is if we get any
 2 more nitrogen in our water, we will get
 3 even more algae blooms than we already
 4 have, which suck up the oxygen in the water
 5 and cause our marine life to die because
 6 they can't breathe.

7 I love clams on the half shell. As
 8 a little girl, we used to go out clamming,
 9 and I used to take the boat out and dig as
 10 many clams as we wanted. Then we had water
 11 quality issues in the Smithtown boat basin
 12 and Stony Brook Harbor, and the Town closed
 13 the shellfish beds.

14 We are finally allowed to dig clams
 15 again. As a resident, I really don't want
 16 to lose clamming. In addition to the green
 17 and brown algae loads, we call them brown
 18 tides, which would only kill the marine
 19 life, there is also the possibility of
 20 serious human health issues because of the
 21 possibility of blue, green, or red algae
 22 blooms, also known as red tides.

23 Back to the issue at hand, we
 24 already have water issues in Stony Brook
 25 Harbor and Smithtown Bay with E. Coli and

SMITHTOWN PLANNING BOARD

GW-37
cont.

hypoxia, which is the scientific term for oxygen depletion in the water. Adding more nitrogen to this is not useful. The bad thing about most useable forms of nitrogen is that they don't stick to soil particles, they actually push away from them, and unless a plant's roots are there and ready to use them, the nitrogen compounds push down through the soil and move through the water table and out into the waterways and the Long Island Sound.

Lawns have very shallow root systems, which don't have much time to uptake nitrogen before it flows deeper into the soils. The lawns, if they are going to be maintained, should just have grass clippings be put back down on them and not have any additional nitrogen or any kind of fertilizer put on them as the plan states.

The fertilizer is not necessary. Adding 4 percent more nitrogen to the harbor is significant. The DEIS is encyclopedic, but not analytic. The applicant must analyze these impacts and their repercussions and ramifications

SMITHTOWN PLANNING BOARD

GW-37
cont.

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before proceeding with this plan.

Speaking against nitrogen and open space, the proposed leaching fields around the STP is also not open space. It's an area that will be disturbed to build the leaching field and then covered with lawn. Again, lawn is not a viable ecosystem. It's actually the least productive of land usage because there is little wildlife able to use it, and it also helps very little with flood control, unlike native grasses and forbs, which have deep root systems to help channel groundwater down from parking lots, roads, and improved land.

In addition to the sewage treatment plant's effluvia, the medical buildings and assisted-living facilities will contain pharmaceuticals and chemicals. A sewage treatment plant would eliminate some, but not all these contaminants, which will cause additional environmental impacts to Stony Brook Harbor.

A bioswale would be a better solution than a leaching field for the STP. As the native plant roots would uptake

SMITHTOWN PLANNING BOARD

GW-37
cont.

chemicals and water in addition to nitrogen as a natural filtration system, instead allowing the water to flow out from the shallow rooted lawns to the Stony Brook Harbor and other embayments and estuaries north of the property.

In addition, the STP should be relocated to a more western end of the property. We have heard this before. It is currently sited where the effluvia for the plant would reach Stony Brook Harbor within 10 to 25 years. By moving it west, the water would be estimated to reach the harbor in 25 to 50 years.

By adding bioswales, it could hoped that the water issues would be even further mitigated. Office buildings or less biohazardous buildings should be considered in place of the medical and assisted-living facilities, as geriatric care and medical facilities generate many more chemicals in addition to the effluvia than the average office building does.

ECOL-7

As for the birds, which is what Audubon is actually known for, the language

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ECOL-7
cont.

in the DEIS states that eastern meadowlarks and kestrels are onsite and it's inconsistent with the lack of healthy functioning native areas provided. These birds need large contiguous areas, and the areas provided onsite are insufficient.

Assuming that these grassland birds are still present, what is the mitigation measures for the loss of this habitat? If the project is to move forward, the lot should be configured to provide 50 percent of uninterrupted contiguous meaningful protected open space. That is separate from the developed area and invasive species on the property especially in the buffer area should be mitigated.

When I say mitigated, I don't mean that they should be removed to let new invasives grow in their place, it means a considered plan to remove invasive species and replant with native vegetation, and also a three-year monitoring plan so that the invasive plants do not reinfest the area.

ECOL-2

There is also no plan in place for

SMITHTOWN PLANNING BOARD

1 **ECOL-2** | sustainable power sources to help offset
 2 **cont.** | climate issues. Solar panels over the
 3 | parking lots should be considered. The
 4 | possibility of geothermal pumps and/or wind
 5 | turbines might also be considered.

6 **ECOL-7** | Bird-friendly building design and
 7 **cont.** | elements should be used on the medical
 8 | buildings if they are to be put in, and the
 9 | assisted-living facilities and any other
 10 | buildings.

11 **SOIL-11** | Also, this site has a history of
 12 | contaminate -- of soil contamination. I
 13 | don't know if anybody has assessed the of a
 14 | potential Superfund site.

15 **ECOL-7** | Since this proposal, Four Harbors
 16 **cont.** | Audubon would be pleased to offer its
 17 | assistance in the continued development of
 18 | the plan for the Gyrodyne site.

19 **TR-32** | And now I am going to go back to
 20 | being a resident. I drive these roads
 21 | frequently. The roads surrounding Gyrodyne
 22 | site are considered a historic corridor
 23 | comprising North Country Road, New York
 24 | State Route 25-A Heritage Trail, and the
 25 | Mills Pond Road.

SMITHTOWN PLANNING BOARD

TR-32
cont.

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Historically, the Washington Spy Trail ran along 25-A. In addition, our narrow, winding, two-lane roads are not equipped to handle the increased traffic flow.

I don't know how many of you use the ferry to Bridgeport. I do all the time. And sometimes getting there with time to spare is a real challenge. With increased traffic, it will be even more difficult to gauge what time I would have to leave to catch the boat. That should only take 20 minutes for me to reach with normal traffic. 25-A is what I would consider at/or above capacity for the kind of road that it is; however, improving it would destroy both the historic and the intrinsic character of the less developed areas. Sometimes more is not better.

Thank you very much.

BOARD MEMBER MARCHESI: Thank you.

Excuse me one second. We want to try get to everybody, and we don't want to set a time limit on how long you can talk. But please, we don't need to hear the same

SMITHTOWN PLANNING BOARD

1 subject a hundred times. And please,
2 police yourself at the mic. We would like
3 to get everybody to speak. All right?
4 Thank you.

5 **CHAIRMAN DeSORBE:** And also, please
6 always address the Board. It is important
7 that you address the Board with your
8 comments.

9 Thank you.

10 **NATALIE WEINSTEIN:** I promise I will
11 respect that.

12 **BOARD MEMBER MARCHESI:** Thank you
13 very much.

14 **NATALIE WEINSTEIN:** My name is
15 Natalie Weinstein, and I have lived in
16 St. James for over 40 years.

17 In 1985 my husband and I purchased
18 two properties: One on Lake Avenue, and
19 one around the corner. One I knew to be
20 the old Calderone Theater, and the other I
21 later discovered was the location where the
22 first silent movies were shown.

23 Our home was nestled in the woods of
24 Nissequogue. We shopped at the Waldbaums
25 in Smithtown, pumpkin picked at the Giant

SMITHTOWN PLANNING BOARD

1 Witch, and walked our two little boys on
 2 Lake Avenue. There was a Suffolk store
 3 among others, shoe stores, and even an ice
 4 cream parlor and luncheonette. I know that
 5 for a fact because it was in our building
 6 at 455 Lake Avenue. They are all gone now.

7 When my husband passed away in 2010,
 8 I decided to move my design firm out of the
 9 Calderone Theater and onto our Lake Avenue
 10 EXEC-3 location. Now, I actually saw the visible
 11 deterioration of our downtown. Stores were
 12 empty. Absentee landlords were raising
 13 rents as businesses opened and closed.
 14 Cars were whizzing by with very little to
 15 stop for, and a vape shop and massage
 16 parlors found a home.

17 Those who stayed and invested in
 18 St. James looked to the then-supervisor for
 19 help in revitalizing the town. His answer
 20 was always the same. I'm not raising the
 21 taxes. Without sewers, the business
 22 district will have to manage, and people
 23 can shop at the mall.

24 Then something wonderful happened.
 25 A new supervisor was elected, a civic

SMITHTOWN PLANNING BOARD

EXEC-3
cont.

1 association was born, and the organization
2 of which I am the president, Celebrate
3 St. James Past-Present-Future was founded.

4 People had had enough. While our
5 hamlet was woefully short on
6 infrastructure, it still had the community
7 pride generations of families who lived
8 here and loved St. James had nurtured.
9 Along with that old spirit came something
10 new, progress.

11 Roads were getting fixed. Parks
12 were being upgraded. A master plan was
13 finally being developed for the Smithtown
14 communities based on our input.

15 The town officials were actually
16 listening to us: Homeowners, business
17 owners, families, seniors. Along with the
18 town officials, St. James found a new
19 friend and neighbor at Gyrodyne.

20 The Gyrodyne Corporation, whose
21 board has many St. James'ers on it, has
22 spent years developing their plan taking
23 into account existing environmental and
24 traffic issues, as well as the profile of
25 our small town community. They have

SMITHTOWN PLANNING BOARD

EXEC-3
cont.

1 consistently exhibited the good-neighbor
2 policy to St. James. They are involved and
3 caring partners in our community, our
4 community. And their development can only
5 benefit us and the entire surrounding
6 community.

7 St. James has waited a very long
8 time for such a neighbor who can help turn
9 our dreams of a charming town with art
10 galleries, and restaurants, an ice cream
11 parlor, and boutique shops a place where we
12 and visitors can once again stroll down an
13 avenue with things to see and do, and where
14 our rich heritage in the arts can be
15 celebrated.

16 I speak for my organization and the
17 countless others who have reached out to me
18 who live and work in St. James. Gyrodyne
19 has done their due diligence. You have
20 heard the facts tonight; not distortions,
21 not exaggerations.

22 The development of that property
23 will only enhance us and will allow us to
24 grow, not into a monster city, but become
25 the microcosm of small-town life we yearn

SMITHTOWN PLANNING BOARD

1 EXEC-3
2 cont.

to be again, and for which we almost lost
hope.

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Please let us be good neighbors, and
endorse the progress that means so much to
St. James and truly all of our communities.

6

Thank you.

7

BOARD MEMBER MARCHESI: Thank you.

8

CHAIRMAN DeSORBE: Thank you,

9

Mrs. Weinstein.

10

KATHLEEN VIZE: My name is Kathleen

11

Vize. I live at Dale Road, Stony Brook.

12

PN-20

My husband has a business at the Gyrodyne

13

Industrial Park, so I'm straddling both

14

sides of the line. Thank you for the

15

opportunity to be heard this evening.

16

Some of Long Island's most beautiful

17

places are right here in the Town of

18

Smithtown. A few years ago as part of a

19

club that I belong to with antique cars, I

20

was in charge of putting together a rally.

21

My first thought was along the water, north

22

shore, Smithtown. I brought the cars

23

through Nissequogue, I brought the cars

24

through Head of the Harbor, Stony Brook,

25

Old Field, and we settled back at -- pardon

Old Field, and we settled back at -- pardon

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PN-20
cont.

me. I have a cold -- at Deepwells Estate.
It was fabulous.

Many good things I heard. Everyone
who thinks of Long Island, they think of
Levittown and congestion and whatnot.

Almost 100 years ago, development
was also threatening to change St. James.
South of 25-A was developing at such a
rapid pace, that they referred to it as
Boomertown. Some community members foresaw
that this would cause irreversible damage.
So as stewards of the land, they realized
whatever they allowed to happen would
become their legacy.

At that point in time Nissequogue
and Head of the Harbor incorporated. Each
area developed stringent zoning codes to
preserve older homes, maintain a pleasing
esthetic, and country setting. They did
that, but they didn't stop progress.

PN-21

Today, we are at a crossroads. Our
population has increased, more homes were
built, and what was supposed to be a small
teachers' college, morphed into an
internationally recognized state

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PN-21
cont.

university.
The University seized Gyrodyne property by eminent domain in order to build more, it has given no consideration to its impact on the surrounding area. I ask that Gyrodyne please be a good neighbor in their quest to create a project with synergy and connectivity to Stony Brook University.

TR-2

St. James and Stony Brook residents contend with roads which are overburdened. Traffic grows worse with each new commercial building erected and with each home illegally converted into multiple-family dwellings and student boarding houses by absentee landlords.

Traffic on the historic 25-A corridor and Stony Brook Road has increased substantially without this project. The projected full scope of the proposed project will add more vehicles to the already overburdened local roads.

The DEIS suggested fewer than 500 external trips during any peak hour. That's eight vehicles per minute, one

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TR-2
cont.

vehicle every 7.5 seconds, give or take.
Peak traffic currently creeps along 25-A.
The proposed solution to add traffic lights
at Mills Pond Road and either a traffic
light or roundabout at Stony Brook Road
will not alleviate congestion brought about
by this project.

TR-34

The intersection of Mills Pond and
25-A is targeted for a traffic light and
turn lanes; this is needed. Since there is
a plan for traffic control at that
intersection, why not shift the entrance to
the project to Mills Pond Road to help
create a smoother traffic flow along that
stretch of 25-A rather than break it up by
putting this in the middle after a traffic
light before the next traffic light, and
then side roads which come in.

TR-35

What also can be done to try and
keep some of this traffic off of
Stony Brook Road? How can you go about
encouraging the use of Mills Pond Road and
Moriches Road as an access between Gyrodyne
property and Route 347? There are more
commercial properties and fewer private

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TR-35
cont.

homes or driveways on these two roads other than on Stony Brook Road.

SW-1

Also what in this surrounding environment will be done to control the flooding issue at the corner of Mills Pond Road and 25-A? The pond floods after substantial rainfall, and there have been times when the flooding covers a large percentage of Mills Pond Road. This does also add to the traffic problem.

SOIL-3

Then I know the EIS -- DEIS, rather, has looked into, you know, pollutants onsite and what have you. Going back to the historical uses of the property, one of the former owners of the property -- this is what, I am a little nervous, I apologize -- given that the property was once used for manufacturing at a time when contaminated waste was dumped into bodies of water or buried into onsite picks, what assurances do we have that the pond bottoms and the soil below or along the immediate surrounding areas were thoroughly tested for contaminates or toxins outside of the legacy toxins in the soil associated with

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SOIL-3
cont.

past horticultural use.

Have those ponds been tested? One of the previous owners was a company named Bendex. So when I Googled Bendex, they come up with a Superfund site in another state. And in that state, they were dumping toxins into a pond.

SEQRA-14

The DEIS states that the applicant intends to sell one or more parcels such that future buyers would undertake any future redevelopment applications.

So if that is the case, what covenants can be put into place today to prevent a future developer from buying an open lot and proposing high-density housing development on it? If the sewage is there, I think we do need to concern ourselves with that if these lots are going to be sold off and somebody else is going to approach you in the future.

HIS-2

Without judicious planning, this project will forever change the historic Washington Spy Trail. Smithtown can direct the applicant in their project planning in a way which would preserve the corridor's

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HIS-2
cont.

bucolic appearance.

What you allow to happen will be your legacy. Thank you for hearing me today.

LEN GOMBERT: Good evening. Len Gombert, 15 Great Oak Road, St. James.

I am no fluent speaker like these people. I got a little scrappy paper here.

CHAIRMAN DeSORBE: That's good.

BOARD MEMBER UNVERZAGT: Good man.

MISC-4

LEN GOMBERT: But this corridor that you are talking about, this trail and everything else, did the State of New York when they took the 275 acres and built those three enormous buildings in there, did they worry about that? Did Brookhaven Town say, hey, what the heck are you doing to me? I don't know. I wasn't interested then, but I'm interested now.

The buildings, I went in there today just to see. So I will tell you, if you have never been in there, do yourself a favor and drive in and see the size of the buildings the State of New York put in there for -- I know it's a good purpose,

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MISC-4
cont.

and all this future scientific stuff and everything else, but wow. I will bet you in the one building alone, there was at least 100 cars in the parking lot. Yeah, I'm not kidding you. Take a ride over there. You will see during the day.

CHAIRMAN DeSORBE: Please address the Board.

LEN GOMBERT: I'm sorry. You will see. It's crazy.

TR-3

The other thing is, everybody has concerns about traffic, and I do too. I live out here. I have been here over 50 years. But in the same token, does anybody care from Brookhaven who comes down Edgewood Avenue all the time to go home and go to work, and see that congestion on the St. James/Smithtown roads? I think we are going up the -- barking up the wrong tree, to tell you the truth.

Let's move forward and go with what is going become -- the man is allowed to do this stuff. And I don't care if it comes from 1910 or whatever the law was. He is entitled to do it.

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ECON-12

I personally believe that St. James needs this project to stabilize taxes, Number 1. We have no tax base with industry. This could help us. Number 2,

GROW-2

with the Town working with the owner, maybe we can use their sewer thing, and we are going to put in \$5,000 worth of sewers this summer in town, right through the middle of St. James to help it grow a little bit.

I think the Town should embrace this proposal and go forward.

Thank you.

BOARD MEMBER MARCHESI: Thank you.

CHAIRMAN DeSORBE: Thank you.

JOHN TURNER: Good evening. My name is John Turner, and I serve as conservation chair of the Four Harbors Audubon Society. And it's a pleasure to speak before the Smithtown Planning Board. I don't think I've had that opportunity. Also, I will do a shoutout to David Barnes, who up until recently was a colleague of mine working in the Town of Brookhaven working on land use matters.

We are going to be providing much

SMITHTOWN PLANNING BOARD

1 more extensive comments, written comments
 2 before the deadline. So I am not going to
 3 really go into great detail now, but I want
 4 to do something a little bit unusual focus,
 5 and that is actually focus on the DEIS and
 6 to just provide a few comments or concerns
 7 that the organization has.

8 To make it clear to you, Four
 9 Harbors Audubon is involved because we are
 10 a chapter of the National Audubon Society,
 11 and our charter area consists of the Town
 12 of Smithtown and the western/northwestern
 13 part of the Town of Brookhaven. Basically,
 14 above the Long Island Expressway east of
 15 the Riverhead line. So that is the area
 16 that we have concerns about, and, of
 17 course, this project is situated pretty
 18 much smack in the middle of that.

19 I have two primary concerns about
 20 the DEIS. One really relates to a full
 21 lack of alternatives that we would hope
 22 that you would consider. I am asking for
 23 some supplemental information on. And the
 24 other is just some undisclosed adverse
 25 impacts. We think that should also be

SMITHTOWN PLANNING BOARD

1 addressed.

2 ALT-6

With regard to the alternatives,
Supervisor Romaine touched upon them a
little bit. From my reading of the DEIS,
all the alternatives are just kind of
shuffling around different uses within each
of these jigsaw puzzle pieces that are
sitting in place.

It's really vital that you think
about shifting the projects around on the
landscape to try to maximize open space.
So you have those nine lots, but in the --
every alternative discussion, those nine
lot lines, the boundaries are frozen.

I started doing that and shifted
things around a bit, and was able to -- I
don't have it with me here, but I will
provide the comments to you -- I was able
to come up with maximizing more open space,
making it contiguous, and open space that
is more meaningful.

So that's with regards to the
alternative section, I think that it's
really important that you not just look
alternatives of uses in each of the lots,

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ALT-6
cont.

but actually shift lots around. Maybe you could do a cluster.

In the Town of Brookhaven where I work, I used to be director of DEP for the Town for a long time. We have actually done some industrial clusters. There's no reason why you can't do it.

You typically think of cluster development for open space preservation when it comes residential uses, but you can certainly do it with the industrial, and I would encourage you to give some thought about that as to an alternative in that regard.

GW-22

I also want to support what Joy Ciriglianno said about the shifting of the sewage treatment plant. It is within -- right now it looks like it's within the 10-to-25 year groundwater contributing area for Stony Brook Harbor, which I believe the DEIS indicates makes it inconsistent with the new Subwatershed Plan that Suffolk County is in the process of developing, and is vetting right now publicly.

We think that it makes sense to

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GW-22
cont.

think about maybe looking to put that STP in a way that will reduce potential impacts to harbor, maybe you move it to the 25-to-50 year groundwater contributing area. That buys us some time.

GW-1

That leads me to the other concerns that we have, and that has to do with a better assessment about some of the impacts. Again, as Joy mentioned to you, the DEIS talks about calculations, nitrogen loading calculations in Stony Brook Harbor that suggest that it will increase by about 4 percent. That sounds like it's minor. If I say that we gained 4 percent eating over the holidays, people will think, well, maybe that's not that big of a deal. 4 percent doesn't sound like a lot.

4 percent can be, from an ecological perspective, catastrophic. And I see in the audience we've got Dr. Larry Swanson here, who can speak more to this about the tipping points in ecosystems.

But you add 4 percent more nitrogen into Stony Brook Harbor, what could be the impacts? They are not just necessarily

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GW-1
cont.

linear, where you might just have a little bit more of an impact. And we'd strongly encourage you and ask that you assess in the DEIS specifically what that 4 percent increase might mean ecologically to the ecosystem of Stony Brook Harbor.

ECOL-2

The last thing I will say has to do with, again, something that Joy brought up, and that is windows bird collisions. We know, and it's something that's unassessed in the DEIS. We know that one of the major wildlife impacts of development is birds flying into windows. It's been getting a lot of attention nationally. You may be aware of it.

We know that upwards of a billion birds a year die in the United States flying into windows of commercial buildings and residential buildings. It makes it the second largest cause of mortality behind cat predation, and it's something that we would like to work on.

It is not something that's discussed in the DEIS, nor is the idea of incorporating bird-friendly building design

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ECOL-2
cont.

into the buildings and the windows
assessed. We think it really should be,
because the nature of the buildings that
are proposed here -- we are not just
talking about homes where they are rather
small, you know, windows -- we are talking
about a campus setting with -- presumably
with facilities that have fairly large
windows that could pose a problem. So we
would ask that that be incorporated as
well.

Again, we will be providing comments
to you, and I appreciate the opportunity to
share some initial thoughts with you.

CHAIRMAN DeSORBE: Thank you.

BOARD MEMBER UNVERZAGT: Thank you.

HERB MONES: Good evening, and thank
you for this opportunity to speak to you.
My name is Herb Mones. I live at 37 Quaker
Path in Stony Brook.

I am speaking to you tonight on
behalf of the Three Village Civic
Association and its affiliate associations,
the Stony Brook Civic Association, the
Civic Association of the Setaukets, and the

SMITHTOWN PLANNING BOARD

1 Civic Association of the Setaukets and
2 Stony Brook.

3 I have served on the Three Village
4 Civic Association's board of directors for
5 nearly 30 years. I am the past president
6 of the association. I am now the land use
7 chairman of our association, and after
8 three decades of civic engagement, I am
9 well versed in planning issues and draft
10 impact statements.

11 On a personal note and as a local
12 resident and a frequent visitor to
13 St. James and Smithtown, I am deeply
14 committed to preserving, protecting, and
15 enhancing our shared historic north shore.

HIS-1

16 It is a remarkable beauty -- it is the
17 remarkable beauty and the historic setting
18 of this area that is both a blessing and a
19 curse. It is a blessing in making
20 residents deeply proud of their homes and
21 our colonial heritage; but it's a curse in
22 that we need to fend off continually
23 high-density, maximum-profit projects that
24 see this area only as an area to exploit.

25 Yes, our communities are constantly

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HIS-1
cont.

subject to some change, especially in an area as attractive and desirable as ours, but hopefully these changes fit the area and the unique character of our community.

GROW-3

The Gyrodyne project is the first step in a pivotal, high-density, multi-land division that is the epicenter of a new, expansive corporate build-out slated for this area.

In a very real sense, Gyrodyne's project is the template, the model for a new north shore complete with office parks, hotels, and assisted-living facilities. And with that in mind, the Gyrodyne Corporation, the Smithtown Planning Board has an obligation to its residents to give a hard, careful examination of any build-out of the properties under review and other properties subject to future build-outs.

And they cannot, under the rules and standards of a Draft Impact Statement, be myopic and superficial. To that end, the Environmental Impact Statement needs to weigh all future potential developments of

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GROW-3
cont.

surrounding properties:

One, to the immediate east, the build-out of the Stony Brook Wireless Research Park, which sits on former Gyrodyne property. This build-out may very likely add eight buildings in the near future.

To the immediate east, Bull Run Farm. Potentially an assisted-living center.

Three, to the immediate northeast, the International Baptist Church, the contract vendee now proposing 100 to 120 senior living resort.

And to the immediate north, BB&GG Farm land, future unknown.

The important point here is that any evaluation, assessment, review of the Gyrodyne property cannot take place in isolation, not in a vacuum. It cannot ignore what is happening in the surrounding communities and neighborhoods and properties.

Put it another way, knowing the proposed build-out of the Gyrodyne

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GROW-3
cont.

property, and knowing the potential build-out of the surrounding properties to the east, west, north, and south, it is not credible to think that the Smithtown and Brookhaven neighborhoods on the north shore will not experience severe and lasting negative changes.

TR-32

Of the many adverse impacts, which have been spoken to by many of the other speakers, is the impact on the North Shore Heritage Trail, the Washington Spy Trail. It was 30 years ago that the New York State DOT proposed four-laning Route 25-A in our community. It was only the civic engagement that voiced such objections that turned that back. 30 years ago, the traffic in this community paled in comparison to what it is today, and is nothing like what it can be tomorrow; that has to be considered.

MISC-2

Lastly, let me say this, I say this with no malice, with love of the Smithtown community, and the Smithtown Board and Smithtown Planning, but I am aware that the -- there is a Planning Board Member

SMITHTOWN PLANNING BOARD

MISC-2
cont.

that needed to recuse themselves from the hearing today due to a conflict.

This recusal only seemingly occurred when confronted by press reports. It appears that the Smithtown Planning Board process leading to this hearing has been tainted by one member that was part of every decision that brings us to this point.

It would be now appropriate for the Town Board and the Planning Board to reexamine the application, not only because of everything that is discussed tonight, but knowing how this application advanced to this particular point.

Thank you for your time.

GEORGE HOFFMAN: Good evening,

everyone. My name is George Hoffman, and I live in Setauket, and I'm the cochair of the Town of Brookhaven's 25-A corridor Citizen's Advisory Committee. I also served as chief of staff in several towns on Long Island, the Town of Huntington, the Town of Islip, and the Town of Brookhaven, and I have been involved in many major

SMITHTOWN PLANNING BOARD

1 planning initiatives.

2 I am here tonight to point out

3 some --

4 **BOARD MEMBER MARCHESI:** Your

5 address, please.

6 **GEORGE HOFFMAN:** My address is 143

7 Main Street in Setauket. I did say I was a

8 Setauket resident.

9 **SEQRA-10** I am here tonight to point out some

10 of the deficiencies, as I see it, in the

11 Draft EIS for Gyrodyne. My major concern

12 tonight is that the DEIS fails to take a

13 hard look at significant impacts of the

14 proposed build-out of the 75-acre parcel.

15 There is no -- there was no intermunicipal

16 review. Given that the property is

17 directly adjacent to the border of two

18 towns and one incorporated village, this is

19 a significant failure of this DEIS.

20 It fails to consider significant

21 development being proposed directly

22 adjacent to the Gyrodyne property that

23 hasn't been looked at in the DEIS. We have

24 heard that mentioned tonight, the 18-acre

25 Bible school that will be potentially 118

SMITHTOWN PLANNING BOARD

SEQRA-10
cont.

1 55-and-older housing; the Bull Run Farm,
 2 which we heard now will become an
 3 assisted-living, which is directly across
 4 the street from the Flowerfields entrance;
 5 and of course, BB&GG Farm. Right now the
 6 county is looking at it for potential
 7 acquisition, and what happens to the
 8 Gyrodyne properties will impact our ability
 9 to be able to purchase that.

10 To do a DEIS in isolation of these
 11 proposed developments is classic
 12 segmentation, and it's a failure of this
 13 DEIS. As co-chairman of the Town of
 14 Brookhaven's 25-A Citizen's Advisory
 15 Committee, I worked with two dozen
 16 community residents and spent two years
 17 studying the development patterns on
 18 Route 25-A, and compiled a visioning report
 19 that was adopted by the Town Board in 2018.

20 The recommendation of that two-year
 21 study was to preserve and protect the rural
 22 and small town character of the historic
 23 25-A corridor by limiting development, by
 24 standardizing the signage along the way,
 25 and improving pedestrian safety on

SMITHTOWN PLANNING BOARD

SEQRA-10
cont.

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Route 25-A. This proposed project will have a direct impact on those goals, and it is being proposed without any consideration of adjacent impacts and regional planning.

HIS-2

There are other omissions that need to be addressed. The importance of maintaining and preserving a shared historic corridor, we have heard that it is the George Washington Spy Trail. Our President in 1790 actually travelled that road when he visited Long Island, and to see it squandered into a commercial development on the road just makes no sense to us.

TR-36

The impacts of the development on Head of the Harbor, nothing in the DEIS talks about cut-through traffic when people find they can't traverse 25-A anymore. And

OPEN-9

finally the impacts to the significant agriculture and parklands located on the same 25-A corridor.

GW-3

Finally, the DEIS fails to take a hard look at the impacts associated with building a regional sewage treatment facility on the Stony Brook Harbor, one of

SMITHTOWN PLANNING BOARD

GW-3
cont.

1 the most threatened inland harbors on the
2 north shore.

3 The build-out plan being proposed in
4 this plan is not environmentally sound or
5 creative; it's a throwback to land use in
6 the 1960s, and hopefully the Smithtown
7 Planning Board will force the developer to
8 go back to the planning boards and start
9 over.

10 Thank you.

11 **CHAIRMAN DeSORBE:** Thank you.

12 Excuse me. We are going to take a short
13 break now. The stenographer has to
14 regroup, and we will probably take about
15 ten minutes and thank you very much.

16 (Whereupon, the Planning Board took
17 a short recess.)

18 **CHAIRMAN DeSORBE:** We would like to
19 resume the meeting. Please have a seat.

20 **CHRISTOPHER McNAMARA:** Thank you.

21 **CHAIRMAN DeSORBE:** Thank you. Your
22 name and address, please.

23 **CHRISTOPHER McNAMARA:** Sure. My
24 name is Christopher McNamara. 17 Dorian
25 Lane, Commack, New York.

SMITHTOWN PLANNING BOARD

1 I want to thank the Smithtown
 2 Planning Board on the opportunity to speak.
 3 I'm a life-long resident of Smithtown, for
 4 the most part, excluding my time in the
 5 U.S. Air Force. I am also currently the
 6 president of the Smithtown Chamber of
 7 Commerce.

PN-22

8 We are in economic development as
 9 well as the sewage treatment plants, we're
 10 for. Smithtown hamlets, they need this. I
 11 mean, you have Kings Park, Smithtown, we
 12 both got grants for about \$20 million. In
 13 Smithtown, it costs \$70 million just to put
 14 our sewage treatment in that we need.

GW-11

15 If St. James is able to capitalize
 16 on this, with what goes on, more power to
 17 them, and I'm happy for them. It's better
 18 for the environment, the aquifers having
 19 severe problems we all live -- you know, we
 20 don't have sewers, and it's a severe
 21 problem and it's just getting worse.

ECON-12

22 Besides what the project is going to
 23 do, they are going to provide 1,500
 24 construction jobs. They are going to
 25 provide over a thousand permanent jobs.

SMITHTOWN PLANNING BOARD

ECON-12
cont.

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It's a huge tax base for Smithtown.

Young and old people, many people I know, went to school with, parents, as well as siblings are moving out of the state or have moved out of the state due to the fact of the cost base of taxes here.

The majority of the land is not developed, and their due diligence has been done. We as the Smithtown Chamber hope you will vote yes on this matter, and I won't take up any more of your time, because I know it's getting late.

Thank you very much.

CHAIRMAN DeSORBE: Thank you.

CAROLE ANNE WOLF: Thank you to the

Planning Board for letting us all speak.

My name is Carole Anne Wolf. I live on

Mills Pond Road in St. James. I have lived

there since 1980. Before that, I lived on

Cambon Avenue. My aunt lived up in

Stony Brook right in the middle of the

historic district.

I am part of the Stony Brook Yacht

Club. I help seed clam at the Stony Brook

Yacht Club, and seed oyster for Stony Brook

SMITHTOWN PLANNING BOARD

1 Yacht Club. My concern, yes, of course,
 2 **ECOL-1** | I'm concerned about the wildlife, and I'm
 3 **TR-2** | concerned about the congestion on the
 4 | roads. It is going to be horrible. But my
 5 **GW-1** | biggest concern is Smithtown Bay, St. James
 6 | Bay, and Stony Brook Harbor.

7 **GW-42** | Gyrodyne, that site, has been
 8 | contaminated for decades. It's been
 9 | contaminated since the '20s. It has never
 10 | been a potato farm. This area, when you
 11 | dig up that earth, is going to immediately
 12 | contaminate our ponds, our brooks, and our
 13 | waterways. It's going to kill the
 14 | shellfish. It's going to contaminate the
 15 | fish to the point where you are not going
 16 | want to use them.

17 **SOIL-3** | There is even possibility of nuclear
 18 | material there. And I'm saying this
 19 | because in the '80s I saw munitions being
 20 | brought out of there. I saw missiles under
 21 | tarps being taken out in the wee hours of
 22 | the morning. That came out of Gyrodyne.
 23 | Maybe they were going out to the Rocky
 24 | Point missile site. I don't know where
 25 | they were going, but they came out of

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SOIL-3
cont.

there.
We don't know what's in that soil,
but when you turn it over, all of that
contamination is going to go right into our
waterways. If you enjoy boating, fishing,
clamming, if you enjoy swimming, you are
going to be swimming in the Hudson River.

I don't understand why this project
is necessary, but I do understand that it's
going to destroy the area that we live in,
and I'm very very unhappy with it. Just
letting you know.

Thank you.

CHAIRMAN DeSORBE: Thank you.

BOARD MEMBER MARCHESI: Thank you.

COUNCILWOMAN CARTRIGHT: Good

evening. My name is Valerie Cartright.
I'm a councilwoman for the Town of
Brookhaven representing Council District 1,
which encompasses the Stony Brook and
Setauket area. My address is 9 Hancock
Street, Port Jefferson Station, New York
11776.

First, I'd like to again say good
evening to the Planning Board members.

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EXEC-12

Thank you for giving me this opportunity to speak. I echo the sentiment of our Town Board Supervisor Ed Romaine. His comments were detailed, comprehensive, and spot on, so I will try to keep my comments fairly brief.

As the Brookhaven Town Council District Representative of Council District 1, I had to come here tonight to convey my deep concern about this proposal. I too have previously submitted comments on this to the Board throughout this years' long process for this application.

I have voiced my concerns and those of the neighboring residents in Brookhaven's Council District 1, who will be directly affected by any development at the Gyrodyne site. I am here today to reenforce these concerns.

TR-2

As you are aware, as the cochair of my 25-A visioning committee, George Hoffman has previously spoken, the Town recently completed a corridor study of Route 25-A from the Smithtown line to the Poquott Village line.

SMITHTOWN PLANNING BOARD

1 TR-2
2 cont.

3 Among the many topics that were
4 considered, traffic was a major and
5 consistent concern. This project will
6 greatly magnify this problem.
7 Additionally, as evidenced by our corridor
8 study, the surrounding residents are
9 staunchly opposed to road widening to
10 reduce traffic congestion.

11 TR-29

12 Many feel that the growth of
13 Stony Brook University, though positive in
14 many ways, has changed and shifted the
15 rural character of the adjacent
16 communities. Adding additional
17 high-density in traffic generating projects
18 would increase the major changes to our
19 community. The DEIS that has been
20 submitted does not properly address this
21 concern.

22 GW-1

23 I have spoken to many of my
24 colleagues that represent this area, some
25 of which have submitted letters this
evening, and some that have been
represented here today regarding the
infrastructure needed to support such a
project. Specifically the potential of a

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GW-1
cont.

regional STP that will discharge into our groundwater.

I too share these concerns, particularly considering the sensitive nature of Stony Brook Harbor and the recent Suffolk County Subwatershed Plan that indicates sanitary wastewater from the site will absolutely affect the harbor.

Should there be any approval on this application the proposed development as laid out in the DEIS should be significantly reduced in density to avoid negative impacts to Stony Brook Harbor. The DEIS does not appropriately address this issue, but rather gives variations and iterations of the same project. That was stated by Supervisor Romaine.

TR-2

Furthermore, most of the potential uses of the site as stated in the DEIS are unnecessary to support the surrounding community and will serve only to add traffic impacts to overburdened roadways.

My constituents in Stony Brook and the immediately surrounding area would be significantly affected by the development

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TR-2
cont.

of the proposed application. The neighboring community I represent is already saturated with infrastructure concerns surrounding Stony Brook University.

PN-23

The University, however, offers numerous, positive community benefits, academic, scientific, economic, that helps mitigate these impacts to some degree. However, none of the proposed project ideas that have been put in the DEIS would offer the same benefits to our community, and have the very real potential to irreparably harm the community.

OPEN-4

To be clear, the Town of Brookhaven is not completely opposed to growth; however we strive for strategic controlled growth. A potential alternative option for a significant portion of the Gyrodyne property which would benefit both Smithtown and Brookhaven Town is public acquisition for preservation as open space. This option would alleviate these community concerns.

I am supportive of preserving the

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OPEN-4
cont.

Gyrodyne property as open space, and I urge that this alternative option will be taken into real consideration.

I am well aware of the County's efforts to move in this direction; however, there has been no cooperation from the owners. Additionally, I would like to note our disappointment at the lack of communication with the Town about this application. As a neighboring municipality that would be directly and immediately impacted by such a project, we would have expected a response to the Town's previously expressed concerns.

SEQRA-1

As I mentioned, the DEIS proposes direct impacts on our Brookhaven residents, not the least of which are changes to the infrastructure of roads within the Town of Brookhaven, and there has been no collaboration.

Rather than to continue to reiterate in detail more of the same points made by my supervisor, I will close by mentioning that our board -- your Board should expect extensive detailed comment on the DEIS from

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SEQRA-1
cont.

our Brookhaven Town Planning Department
before the conclusion of the written
comment period on January 24th.

I again state my objections to the
proposed project in any of the currently
proposed forms on the record. And I truly
hope that you take a hard look at all of
the credible comments that have been put
forth before you today by the community and
various elected officials, and say thank
you again for giving me this time, and have
a great evening.

CHAIRMAN DeSORBE: Thank you.

ALYSSA TURANO: Good evening. My
name is Alyssa Turano. I am the chief of
staff for Suffolk County Legislator Kara
Hahn. The legislator was unable to make it
this evening, so she asked me to come here
to read the following statement on her
behalf:

TR-32

The Gyrodyne, LLC property on which
this application has been made is bounded
by a historic corridor comprised of the
North Country Road, New York State
Route 25-A Heritage Trail, and Mills Pond

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TR-32
cont.

Road. These narrow, winding, two-lane roads have not been engineered for the inevitable influx of vehicles that will emanate from this site upon completion and, in fact, are currently at or nearing capacity from the residential, commercial, and economic centers they already serve.

While the project's potential traffic consequences alone are of great concern, the environmental impacts are

GW-15

equally profound. In its current design, the Gyrodyne development provides no consideration of stormwater runoff from the proposed project's impervious surfaces along the Route 25-A corridor. Nearby woodlands, wetlands, Mills Pond, Stony Brook Harbor, and the Long Island Sound could be exposed to contaminate runoff.

GW-17

Just as concerning is the proposed onsite sewer treatment facility designed to discharge into groundwater. In as little as ten years from now, sewage effluent will infiltrate into and pollute the surface waters of Stony Brook Harbor.

SMITHTOWN PLANNING BOARD

1 GW-17

For more than a generation we, as a region, have been fighting to protect the natural ecological function of the Long Island Sound and its embayments. If approved, and especially if it's expended later into a regional sewer, this facility will confound these efforts.

8

Because it will be constructed too -- much too close to important educational, historical, and cultural sites, the Gyrodyne project will diminish the character, livability, and property values now associated with the residential homes that are near the proposed location.

15 SEQRA-9

The Draft Environmental Impact Statement is incomplete in its current form, and it must answer certain fundamental questions about this project, including its impact on local watersheds of significance, the Long Island Sound, and its impacts on our area's limited transportation network, and must also contain a detailed assessment of impacts it would have to neighboring municipalities.

25

The Gyrodyne property is one of the

SMITHTOWN PLANNING BOARD

SEQRA-9
cont.

1 largest undeveloped tracts remaining in
 2 western Suffolk. This project, if built,
 3 will fundamentally change the character of
 4 this community through increased traffic, a
 5 deteriorated environment, and a diminished
 6 quality of life. For all of these reasons,
 7 I have grave concerns on the size and scope
 8 of this project.

9 I urge you on behalf of the
 10 residents living in both the Town of
 11 Smithtown and Town of Brookhaven to
 12 postpone any further consideration of this
 13 project unless and until the applicant
 14 submits an analysis that fully considers
 15 these concerns and addresses each in a more
 16 thorough and comprehensive Environmental
 17 Impact Statement.

18 I also have a written copy to
 19 submit, as well.

20 Thank you.

21 **CHAIRMAN DeSORBE:** Thank you.

22 **BOARD MEMBER MARCHESI:** Thank you.

23 **TROY ROSASCO:** Good evening. My
 24 name is Troy Rosasco. I live at 19
 25 Highland Avenue in Village of Head of the

SMITHTOWN PLANNING BOARD

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HIS-4

Harbor.
I think I have a unique perspective on the 25-A Historical Corridor. It happens that Highland Avenue intersects on 25-A, as it does on the other end Moriches Road. I am in walking distance within three minutes of both the general store, of Deepwells Farm, and of the St. James Episcopal Church.

On my block, it's is probably one of the most historic blocks in all of the Head of the Harbor. I live in a circa-1895 home that was built by George Hodgkinson, and he was one of the builders of Boxhill, Stanford White's home.

The home is also in the book, Images of St. James, and it is known as the A.D. Carlisle summer home. He was a Vaudeville performer and actually travelled the country with a dog-and-pony show. And I've got the barn in the back with the stalls. And so, I myself am particularly interested in preserving the historic 25-A corridor.

In addition to my block being one of to most historic blocks in Head of the

SMITHTOWN PLANNING BOARD

HIS-4
cont.

1 Harbor, a couple of years ago you probably
2 know that there was a threat to the closure
3 and the sale of the historic St. James Fire
4 Department building on 25-A.

5 It was my honor along with many
6 other St. James residents to fight that
7 sale and preserve that historic firehouse;
8 and thankfully, we were successful. And
9 that's right on 25-A.

10 So please understand if I thought
11 for one minute that the Gyrodyne
12 development was going to have a severe
13 impact on the 25-A historic, cultural
14 esthetic, I would oppose that, and not only
15 would I oppose it, I would mobilize the
16 same forces and the same people who fought
17 to preserve the historic firehouse to see
18 that this development would fail.

19 However, I'm not here to argue
20 against the Gyrodyne development. Instead,
21 I'm here to argue that it should be
22 developed as proposed.

23 I have investigated this, as I have
24 many other developments along 25-A. Okay.
25 And the potential for development along

SMITHTOWN PLANNING BOARD

HIS-4
cont.

1 25-A. I moved here into the Village of
 2 Head of the Harbor because I loved the
 3 historic, rural nature of it. I don't want
 4 the see that threatened at all, but I do
 5 know that there will be a substantial, as
 6 indicated in the program before, buffer on
 7 25-A. I believe it's at a minimum 200 feet
 8 with new plantings that will protect the
 9 esthetic, cultural, rural nature of 25-A,
 10 and certainly I, as a resident of the
 11 Village of the Head of the Harbor, want to
 12 protect that.

13 Our village covers a good distance
 14 of that, so I simply am here today to
 15 solely comment on the historical impacts of
 16 the development, and no other issues
 17 regarding this development, but to ask you
 18 to strongly consider supporting this
 19 development proposal by Gyrodyne.

20 Thank you.

21 **CHAIRMAN DeSORBE:** Thank you.

ECON-8

22 **BOARD MEMBER MARCHESI:** Thank you.

23 **WARREN STRUGATCH:** My name is Warren
 24 Strugatch, and I'm publisher of Select Long
 25 Island, an economic development

SMITHTOWN PLANNING BOARD

ECON-8
cont.

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publication. And prior to that, I have been a business journalist on Long Island covering and profiling figures in business, in economics, and in economic development for many years.

And that has, I believe, given me a perspective on what true economic development actually is. And I have to say, I really see little to none of it in the project that we are here to discuss today.

There is very little of it in terms of what the traditional and contemporary requirements of what economic development are. Looking at planning is a big part of it. I think the comments from the leaders across the border, it almost sounds like there is a kind of wall that has been put up, and it's unfortunate because the whole purpose of planning -- planning is at the essence of economic development.

If planners aren't talking to other planners, if planners aren't talking to economic leaders, if the people who create the programs and the projects and design

SMITHTOWN PLANNING BOARD

ECON-8
cont.

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them and plan them are not talking to their counterparts across the border, then there is no planning going on. It doesn't exist; it's phantasmagorical.

Earlier today, in order to try to get a sense of what economic benefits might be and to question what I have heard from many people, which is that maybe the environmental issues may be questionable, but there is going be economic growth. We are going to create a thousand jobs. I have heard that bandied about, and possibly that's so. But I looked into what those jobs are going to be. And anybody in this room who has a connection to the internet can do the same thing I did. You can find these statistics on the U.S. Labor Department and in the Census Bureau, and they are all freshly cooked today. I just found them this morning.

For example, in terms of looking at the occupational wages and profiles, what the jobs that are going be based on the industries that are going to be in this project, it is not a pretty picture. We

SMITHTOWN PLANNING BOARD

1 ECON-8
2 cont.

are looking at hotel, we are looking at nursing homes, and we are looking at medical offices.

These are industries that hire predominantly in the lowest third of occupations, as ranked by wages. Again, according to the U.S. Labor Department.

Let's start with proposed hotel. Hotels hire predominantly housekeepers, desk clerks, porters, security people, maintenance people. Hotel and motel employees last year earned just over \$27,000. These are median annual earnings. They were the 17th lowest wage earners in the United States. Maids and housekeepers came in just over \$24,000. They ranked 7th lowest. Food prep workers, dining room attendants, food service workers, earn between \$23,000 and \$24,000. I repeat, \$23,000 and \$24,000. They held the second lowest ranked salary jobs in the economy. Okay. Everybody remembers Letterman's top ten list. Who do you think is the lowest ranked in the United States by occupation? It's laundry workers. They earn \$22,500.

SMITHTOWN PLANNING BOARD

ECON-8
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Let's take a quick look at the nursing home assisted-living center. Nursing and health aides earn just under \$27,000 a year. They are ranked 16th lowest.

The situation is a little brighter over in the medical offices. Doctors do well. My mother would say they are comfortable. They earned the median salary of just under \$300,000 last year. And this is national. Locally, on Long Island, it's a little higher, but Long Island wages have not kept pace with the growth around the country. We earn pretty close to what we earned ten years ago. Most of the country has gone higher. We've lagged that growth occupationally.

Most people who work in medical offices, though, are not doctors. It's about maybe one out of ten employees of a typical healthcare facility are doctors. Nursing and health aides last year earned less than \$27,000. That's not a tenth of what physicians earned. The average medical records clerk's salary in the

SMITHTOWN PLANNING BOARD

1 ECON-8
cont.

United States \$35,000.

2 Over at the sewage treatment plant,
3 uncomfortably near all of these -- the
4 average salary there, assuming that the
5 plant is run by a private operator --
6 governments pay more, will be about
7 \$46,000.

8 Let's stop for a moment and compare
9 that to what it takes to live in Smithtown.
10 Median household income in Smithtown last
11 year was \$126,000. And that's about double
12 the amount of the national median wage.
13 It's about double. And this is coming from
14 the Census Bureau. I don't make it up.

15 Keep in mind, that if you are family
16 of four and you are earning \$81,000 a year
17 here, you qualify for public benefits.
18 That's \$81,000. So most of those jobs that
19 we are talking about here would require two
20 people, and they still would not be
21 getting -- they would still be on public
22 benefits.

23 So to summarize my research, it
24 appears that most of the jobs that would be
25 created on this site will be low-income

SMITHTOWN PLANNING BOARD

ECON-8
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jobs, among the lowest on the books in the United States. Most of the jobs will not pay enough to keep a Smithtown household above the property line.

I would also say that the highest paying jobs of the new jobs will probably represent jobs that are transferred from other locations fairly nearby, as distinct from jobs that will be created. Meaning, there will be expansions, there will be companies that exist or operations that will expand and create a new program, and they will transfer people here. They will not be literally creating new jobs, and that is an important distinction. Why is it important? Because if the person is now working in Commack, they may be offered the chance to take the job in Smithtown. And they very possibly will accept it, and that will be called a new job. It will not genuinely be a new job. It will not have that economic impact.

So I have also heard it said that the project will bolster the communities' tax rolls. I see that as highly unlikely,

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ECON-8
cont.

and that's based on recent experience. The reason, as everybody knows about the 2005 land grab -- excuse me, eminent domain seizure of the University, Stony Brook acquired either 246 acres or 278 acres, it depends how you -- I guess on who you ask, of Gyrodyne.

Since then, the University has constructed and continued to construct buildings for academic research purposes, which we have discussed at length, ad nauseam, here today.

The important thing to note is that on Brookhaven's experience, and if Ed Romaine were still here I think he could attest to it, that's been a negative loss for Brookhaven. They took the property off the tax rolls, it was bought by a government entity, and it pays no taxes, but consumes public services including emergency services.

So what we are saying is that the experience in Brookhaven -- and by the way, I don't have to tell anybody in this room that this property is adjacent to the

SMITHTOWN PLANNING BOARD

ECON-8
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Stony Brook campus. Gyrodyne has not yet allowed who will be the -- definitively say who will be the customers of this property or who will be actually developing the property. If the planners know, maybe they can tell us. It would be interesting to hear.

But the point is, is that they have already asked for an abatement so that this campus would be easily adjoinable and accessible to Stony Brook University. So you can take a guess at who might be the ultimate customer for that lot. If Stony Brook buys it, it goes off the tax rolls. It is not going to be a tax -- a tax positive gain for Smithtown.

Thank you.

CHAIRMAN DeSORBE: Thank you.

MICHAEL KAUFMAN: Members of the Board, first off, I have a little bit of laryngitis. Thank God for microphones. My name is Michael Kaufman. I live on -- well, it's actually my driveway -- River Hollow Lane in St. James. I have been here for about 40 years. So compared to Richard

SMITHTOWN PLANNING BOARD

1 Smith, I'm a rookie. His family has been
2 here for about ten generations or so.

3 First off, I just want to bring up
4 one or two quick points, and also identify
5 myself a little bit more, because of what
6 I'm going to be talking about. I am going
7 to actually try to focus on the EIS and
8 talk about hard looks, because that's my
9 job with the County.

10 **GW-38** I, among others, sit on the Council
11 for Environmental Quality, and we run the
12 EIS process for the County. We are in the
13 middle of one right now called the
14 Subwatershed Management Plan, which is part
15 of LINAP, which John Cameron at the
16 regional board is one of the people in
17 charge of. So as I'd like to say, I've got
18 a piece of the action.

19 **HIS-2** Okay. First off, everyone is
20 talking about this State designated
21 heritage trail. Yes, it is a heritage
22 trail, that's 25-A. But it was in 1974 it
23 was designated as a State designated
24 historic highway with certain rights and
25 responsibilities, and it was named after

SMITHTOWN PLANNING BOARD

HIS-2
cont.

1 Governor who? The guy after Rockefeller.
2 Nobody remembers his name; it's Malcolm
3 Wilson. Okay. And it was intended to
4 preserve the rural corridor that existed in
5 1974.

6 There was also, I believe, at that
7 time the 200-foot setback that encumbers
8 Gyrodyne at this particular point in time
9 on the south side of Route 25-A, again to
10 preserve the rural vistas and
11 characteristics.

12 Let's see, you should also know that
13 I, for about 27 years, worked with and
14 helped form and was a leading member of the
15 Nissequogue Head of the Harbor Local
16 Waterfront Revitalization Commission, the
17 LWRP. We had responsibilities for
18 Stony Brook Harbor since the two villages
19 surround 95 percent of it. I helped run
20 the dredging operations in the harbor, and
21 also in the Nissequogue River, and I fought
22 for many years to keep the area as pristine
23 as environmentally clean as we possibly
24 could. I will get into one other point on
25 that in a second.

SMITHTOWN PLANNING BOARD

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SEQRA-11

GW-38 cont.

First off, I have to ask a question:
Was there any coordinated review set forth
in this? I mean, we keep on hearing
Brookhaven Town didn't know anything about
this and hasn't been brought into the
process. Normally at the County,
coordinated review is undertaken, and
because this is Type 1, you've got an EIS
in front of you, obviously there was a
pos-dec given. Yet, nobody responded.

So I find it kind of odd that, you
know, all this opposition from the local
municipalities, Head of the Harbor, for
example, I know is dropping a letter with
major concerns. Town of Brookhaven, we've
heard from several representatives. I'm
very curious about why they are only
showing up now, whether they got the proper
notice when this process all started a
couple of years ago. I'm not challenging
it. I'm just very very curious about it.

Okay. What I am going to speak
about is -- are some facts regarding
nitrogen and coastal waters. I'm one of
the people who's working on the Suffolk

SMITHTOWN PLANNING BOARD

GW-38
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County Subwatershed Management Plan, which again, is part of LINAP. We are doing the primary review of it at CEQ.

Suffolk County has almost completed its analysis of why our coastlines are collapsing and why our natural resources are disappearing. It's Public Enemy Number 1 according to the county executive, and that's basically nitrogen; and that's why a couple of million dollars has been expended by the County to form the Subwatershed Plan, or SWP as everyone calls it.

So again, nitrogen is Public Enemy Number 1. It causes, eutrophication, excessive algae harmful blooms, hypoxia, and things like that. But I have more evidence than a lot of the people have talked about. They have all been spot on with what they are talking about, but I as a necessity had to dig into it.

Suffolk County has mapped 191 watersheds in Suffolk County, north shore, south shore, the Peconic, and established basically where the nitrogen is coming

SMITHTOWN PLANNING BOARD

GW-38
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from, which is primarily septic discharge, how it flows to the coastline in those 191 watersheds, how long it takes to get to each of the costal areas from the inland discharge points; for example, Gyrodyne we are hearing, you know, 10 to 25 years.

I have the maps, but I am not going to bother with them right now. We also are finding out how much of nitrogen is actually coming out, and a number of other scientific parameters with a general baseline of 1987.

To our horror, we have found the following facts: Since 1987, there has been a 41 percent increase in nitrogen in the Upper Glacial Aquifers on Long Island in Suffolk County, mostly from septic discharge even with STPs.

There has been a 45 percent increase in nitrogen in the enclosed Long Island Sound harbors, such as Stony Brook Harbor, such as Nissequogue River, Mount Sinai, Port Jeff, places like that. Again, this is an average, but all of them are suffering from nitrogen contamination.

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GW-38
cont.

And again, that's over the last ten years. So that 45 percent increase has been popping up quite nicely, and it's mostly attributable to watersheds.

The Nissequogue River, for example, had a study done recently, and the amount of nitrogen that's popping out from Smithtown is incredible.

The nitrogen in Long Island Sound has increased by 10 percent. Okay. And the only reason that's a lower number is because the Sound is well flushed; the interior bays are not. We've also found out that the nitrogen is collapsing our wetlands. And we are seeing that damage, for example, in Nissequogue River, and more particularly, Stony Brook Harbor.

Stony Brook Harbor is one of the worst flushing harbors on the north shore. It takes up to around 12.4 days to get rid of the daily load of nitrogen that's popping in there from the watershed, which does include the Gyrodyne property. It goes in at about a mile and-a-half.

Head of the Harbor and Nissequogue

SMITHTOWN PLANNING BOARD

GW-38
cont.

1 contribute quite a lot of it, parts of
2 Smithtown and Gyrodyne contribute a fair
3 amount of it, but it takes 12 -- up to, not
4 exactly, but up to 12.4 days to get rid of
5 the daily load.

6 It turns out Stony Brook Harbor on
7 the southern portion is not as well flushed
8 as anybody thought. Now, Port Jeff as a
9 comparison takes 2.9 to 4.3 days to flush.
10 Mount Sinai, 4.5 days, and that has a much
11 larger entrance channel. If you know
12 Stony Brook Harbor, we don't have an
13 entrance channel, we've got about 100 feet
14 deep, 100 wide dredged; that's about it.

15 We also have noted that Stony Brook
16 Harbor wetlands have already been damaged
17 by the nitrogen that's coming in.

18 Okay. Now, there is another
19 problem: We have got a giant gyre or
20 whirlpool existing in Smithtown Bay that
21 actually traps the water leaving Stony
22 Brook Harbor for up to about four days or
23 so, and prevents the mixing of water in
24 Smithtown Bay with the rest of Long Island
25 Sound, even though they are literally

SMITHTOWN PLANNING BOARD

GW-38
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contiguous with each other. It literally traps the water in there. And that's why Smithtown Bay is hypoxic during the summer, no oxygen, it's a dead zone.

Okay. It is, in fact, the deadest zone in the entire Sound east of New York City during the summer below 10 feet in depth. The benthic life is gone at below 10 feet, according to Long Island Sound studies.

I have the maps in my car. I will be presenting them eventually before the 24th. But this is according to the Long Island Sound study, I am not making the data up. It is a black zone. That's how they qualify it. If you look at New York City through the East River, it's black. If you look at Smithtown Bay, it's a black zone; nothing over there. Okay. The rest of the Long Island Sound is relatively well flushed.

I mean, this is a problem over here. The nitrogen from the Stony Brook Harbor watershed and also the Nissequogue River basically takes forever to leave the area.

SMITHTOWN PLANNING BOARD

GW-38
cont.

1 It's not well flushed. We have the gyre
2 out there. So Stony Brook Harbor is
3 suffering, and the hypoxia is caused by the
4 retained nitrogen, which is showing the
5 problem.

6 Now, in the Suffolk County
7 Subwatershed Management Plan, Stony Brook
8 Harbor ranks as a priority water body for
9 reduction of nitrogen input. You have this
10 STP here, which normally would be okay if
11 it was just for Gyrodyne, and that's my
12 particular position.

13 It's becoming a regional STP, and
14 that's what a lot of people have been
15 talking about. And it's in the Stony Brook
16 Harbor watershed.

17 An STP for Gyrodyne will produce
18 nitrogen even after treatment. And some of
19 the numbers were shown up there by
20 Mr. Cameron, although they were on the low
21 numbers as opposed to some of the higher
22 numbers that were on same data sheet.

23 On a daily basis, the overall
24 nitrogen only of Gyrodyne might be
25 tolerable compared to the overall nitrogen

SMITHTOWN PLANNING BOARD

GW-38
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that's coming in from the rest of Nissequogue and Head of the Harbor and parts of Smithtown.

By making the plant regional, going from 71,000 gallons per day, which is the Gyrodyne number, to 171,000 gallons per day, which is Gyrodyne plus Lake Avenue, okay, creates a regional aspect, and that was not, for example, reviewed at the Suffolk County Planning Commission. That's 59 percent increase more nitrogen coming in, and the transit times to the coast is about the fastest in Suffolk County that I have been able to identify.

There is also talk about possibly increasing the capacity to 342,000 gallons a day. That's just talk. Nonetheless, that is a regional aspect that's being created from out of nowhere. Obviously, Gyrodyne, as a private company, would have to agree to certain of these aspects. But as somebody who has some oversight over sewage treatment in the County as part of the Council on Environmental Quality, you see these things happen.

SMITHTOWN PLANNING BOARD

GW-38
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The DEIS fails to take a hard look at these particular issues and the fact that we are getting a regional plant when Stony Brook Harbor is already under stress, and that the Subwatershed plants would be looking to reduce nitrogen, not add to it. So going to potentially 171,000 or maybe 342,000 gallons per day -- again, the 342,000 is an upper number that's not fully confirmed -- is a problem that you should be looking at.

A hard look at the regional impact of adding sewage treatment -- of adding sewage flow needs to be performed. Again, the statistical data that's in the plan right now is focussed basically upon Gyrodyne, which again is tolerable. They have the right to divide. Okay. There is no doubt about that, under governmental supervision, and they have possibly, you know, as an agreement with the Town of Smithtown and others, to put Lake Avenue into the mix.

But you are starting to get numbers that have never been looked at, okay, and

SMITHTOWN PLANNING BOARD

GW-38
cont.

1 that are going to affect a water body
2 that's shared by several other
3 municipalities. And there is a transit
4 zone over there. It's not like this thing
5 is in isolation.

6 So in that sense, a harder look
7 needs to be taken. That is the job of the
8 EIS at this point in time when you are
9 starting to go into final. You have done
10 the initial craft, and you are going to
11 final; and I am advising you that there are
12 some issues that need to be looked at. If
13 I'm wrong on the 342, so be it.
14 Nonetheless, it's 171,000 gallons. That is
15 going to have an impact. We already see
16 that. We have the maps. We know what
17 potentially is happening.

18 And, yes, an STP removes 90 percent
19 of the nitrogen. You are still going to
20 have the max loads. There is still a lot
21 of nitrogen out there. Okay. And that's
22 what you have to guard against, okay, with
23 a water body that's already under stress.

24 Thank you.

25 **CHAIRMAN DeSORBE:** Thank you.

SMITHTOWN PLANNING BOARD

1 **LEE KRAUER:** Okay. I am going to
2 wake everybody up. I'm not going to talk
3 about water. Okay. I'm going to talk
4 about synergy.

5 My name is Lee Krauer, and I'm the
6 past president of the Three Village Civics
7 Association, and I'm the chairperson of the
8 Friends of Stony Brook Road.

9 Synergy, according to the
10 dictionary --

11 **CHAIRMAN DeSORBE:** Excuse me, please
12 state your address.

13 **LEE KRAUER:** I'm sorry. 33
14 Fraternity Lane, Stony Brook.

15 **CHAIRMAN DeSORBE:** Thank you.

16 **LEE KRAUER:** I said to someone here
17 before I consider myself bi coastal. I
18 live in Stony Brook. I go to church in
19 St. James. I shop in King Kullen. I take
20 my dog to Terry's PAW-fection. I get my
21 nails done on Lake Avenue. And I eat at
22 Basil's. So I have a foot in both worlds.

23 Synergy, according to the
24 dictionary, is the combined power of a
25 group of things that when they are working

SMITHTOWN PLANNING BOARD

1 together is greater than the sum of the
2 individual elements.

3 The University -- and we know which
4 university I'm talking about -- their
5 definition of synergy is we work with no
6 one, and when we tell you what we want, you
7 do it.

8 A gentleman before raised the issue
9 of -- he took a ride today on Gyrodyne.
10 What a shock. There's University buildings
11 there. What a shock. How did they get
12 there? I don't know. They appeared
13 overnight. Everybody in Brookhaven
14 objected to those buildings. They did not
15 have to do a water study. They did not
16 have to do an environmental study. They
17 are the State.

18 ECON-6

18 Okay. Why do I mention synergy?
19 Because the proposal before us tonight goes
20 to great lengths to stress the importance
21 of synergy. It's mentioned many many
22 times. Synergy between the developer and
23 the University. Why is that?

24 The fact is the DEIS over and over
25 again discusses synergy. I question this.

SMITHTOWN PLANNING BOARD

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ECON-6
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Why is it mentioned? Were there ever other types of uses discussed for this land that might have had less density and might be more appealing to the community; were any of them ever considered?

I question since these parcels will be single and separate, is Stony Brook considering purchasing any of these lots, maybe directly for the medical staff, which would result in having the buildings taken off the tax rolls?

CHAIRMAN DeSORBE: Excuse me for a minute. Would everybody in the room be silent, and let the speaker have her say.

And our court reporter -- our stenographer is having a little problem with the noise in the background. So please maintain your silence.

Thank you.

ECON-6
cont.

LEE KRAUER: If Stony Brook takes these buildings, they come off the tax rolls. There goes your \$300 million in property taxes.

BOARD MEMBER MARCHESI: Do you want to speak to the Board, please.

SMITHTOWN PLANNING BOARD

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LEE KRAUER: I'm sorry. I will absolutely do that.

BOARD MEMBER MARCHESI: Thank you.

SEQRA-1

LEE KRAUER: We question why there was no inter-municipality planning. Someone raised the question before: Did Brookhaven know about this? I can tell you for a fact that the Brookhaven supervisor got notification 24 hours before they had to reply. I know that for a fact.

TR-25

I was here in this room when this whole proposal first came up, and those of you who were also here will remember I didn't plead for Stony Brook. I pled for Mills Pond Road. I know what is happening on Stony Brook Road. I know what is happening on 25-A. Mills Pond Road is about to be destroyed, and you people here are going let it happen.

I want to know does Kevin Law, who is the head of the Stony Brook President's Council, the head of the Long Island Association, chair of the Long Island Regional Economic Council, did he include all stakeholders and all local residents as

SMITHTOWN PLANNING BOARD

1 to the best use of this property, or did he
2 solely focus on Stony Brook University?

3 Here is the latest example of the
4 University's definition of synergy:
5 Representatives from various civic
6 associations and the Chamber of Commerce
7 spoke with a woman named Judy Greiman --
8 you all know her, I'm sure -- she is the
9 chief deputy to the president of the
10 University. All these people told her they
11 were opposed to the University granting an
12 easement over the railroad tracks to
13 Gyrodyne. You heard me right. They were
14 all in agreement.

15 Okay. So what do you think
16 happened? The easement was granted. Those
17 of you who think that dealing with the
18 University is an honest and straightforward
19 process -- synergy -- think again.

20 Given the fact that Stony Brook
21 University is a major science campus, one
22 would think that environmental issues would
23 be of great concern to them. Again, you
24 would be wrong. If it benefits the
25 University, the environment be damned.

SMITHTOWN PLANNING BOARD

1 **ECON-13** When they used eminent domain to
 2 claim the property -- we all spoke about it
 3 tonight -- it cost us taxpayers \$167
 4 million; but a million here, a million
 5 there, sooner or later it adds up to real
 6 money. Okay.

7 **ECOL-11** They destroyed animal habitats.
 8 They had a massive demolition of the trees
 9 there. That's why most of us are now
 10 eating dinner with deer every night. Their
 11 habitats have been destroyed. They are on
 12 my front lawn. I think I win the prize
 13 here tonight. I have lived in Stony Brook
 14 for 53 years. In 53 years, only in the
 15 past five years have I seen deer every
 16 single day. They're coming from somewhere.
 17 Their habitats are being destroyed at
 18 Gyrodyne, and the University is
 19 responsible.

20 **MISC-1** As an aside, I have some questions
 21 for this Board. How many of you knew of
 22 Mr. Ryan's very clear conflict of interest
 23 when he made decisions affecting this
 24 property? Are you going to go forward when
 25 every vote, every decision, and every

SMITHTOWN PLANNING BOARD

MISC-1
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comment he made is tainted?

Mr. Ryan did the right thing in
recusing himself. This action was
absolutely necessary; however, it is not
enough. Mr. Ryan has been involved and
vocal about his support for Gyrodyne
development on multiple occasions. As far
as back 2003 when he made his first quote
that ended up in local newspapers.

Mr. Ryan has been privy to this
proposal before it became public. He voted
on the acceptance of the current DEIS prior
to recusing himself. He has been part and
parcel of the Planning Board's actions
since the inception of the current
proposal.

Recusing himself underscores his own
awareness of the conflicts, and his
presence before and after taints the
process.

His actions warrant scrutiny as to
the validity of the December 11th vote.

His presence --

CHAIRMAN DeSORBE: Excuse me.

Please let the speaker finish.

SMITHTOWN PLANNING BOARD

MISC-1
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LEE KRAUER: His presence from the beginning undermines public trust that the proposal was developed with no special interest and in good faith without any one individual being in concert with the developers either in formal or informal meetings. I would add the University to that list.

We ask the Board for denial of this proposal based on the internal conflict of interest since its inception.

CHAIRMAN DeSORBE: Thank you.

BOARD MEMBER MARCHESI: Thank you.

LEE KRAUER: I have one other comment. Okay. I have one other question: Did anyone follow the money? How many people involved with this very tainted process have profited in some way from this disaster in the making?

OPEN-7

And finally, I'm sure you all realize this is the very last parcel of open space in St. James and Stony Brook, and you people are about to destroy it, and the living style of your friends and neighbors.

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SEQRA-12

BOARD MEMBER MARCHESI: Thank you.

CHAIRMAN DeSORBE: Your name and address? And also, have you signed the sheet there?

CINDY SMITH: Yes. Cindy Smith, One Olney Way in Stony Brook.

And let me start by saying thank you for having me this evening. Actually, I'm a 50-year resident of Smithtown, born and bred. I went to Smithtown West, second year graduating. Lived over by Blydenburgh Park. So I actually drove the Five-Star mothers around for the Memorial Day parade and part of the bicentennial celebration. I can tell you where the time capsule on the Smithtown Library lawn is, in case you are interested at some point you want to dig it up.

But recently we moved over slightly over to Stony Brook, because we wanted to be close to the scenic roads, the preservation of history, the wonderful water views, which we really feel, my husband and I, that the proposed development can easily destroy.

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SEQRA-
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The fact is, this plan was done in isolation. No consideration was done to the effects of this plan on the residents primarily adjacent to the development, specifically in the Town of Brookhaven or the Head of the Harbor would have.

The DEIS did not take into consideration the eight new buildings Stony Brook University has on the plans at the Wireless center. And those -- by the way, those eight buildings would be 800,000 square feet of space. Something to really think about. Not that we are not happy about it, by the way. Just want you to know. The Stony Brook residents in the area are not happy with the University. Okay.

It also doesn't take into consideration the redevelopment of the old Baptist church, which is pretty much right next door to the Gyrodyne property, or yet another nursing home. How many nursing homes can we possibly use in one area? Another facility at Bull Run, which is really directly across from Gyrodyne. And

SMITHTOWN PLANNING BOARD

1 TR-33

of course, I am here to talk about traffic,
and traffic with numbers, because I've got
the numbers.

You know, this potentially impacts
the current traffic and traffic of the
future. So here are the facts:

As one of our other speakers said,
25-A is a historical trail. It's really
our brand, really. You talk about
Smithtown, St. James, Stony Brook, it's
really our brand; it's our brand of
tourism. It's what brings visitors here.
It's why people come here to visit. It's
why people come to live. We love this
area. It's who we are. It's what makes
our community. It's our sense of place, if
you will.

And if you listen to Kristen
Jarnagin, by the way, over at Discover Long
Island, and what the IDAs are doing, it's
all about where you belong and creating
that sense of place, both for residents and
businesses, by the way. So you really need
to maintain that to attract these people
here.

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TR-33
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Not only is the type of building that Gyrodyne is proposing out of character to the area, according to New York State DOT, historic Route 25-A, which is a two-lane road, is already at capacity, and it can't take any more traffic.

Now, here is where it really gets interesting, because we over by the University. I work with the Friends of Stony Brook Road. In 2007 -- sorry. In 2017 in response to residential complaints about the massive traffic on Stony Brook Road, the Town of Brookhaven commissioned a traffic study from Nelson and Pope.

Now keep in mind, Stony Brook Road will be the major north/south road visitors will use to come to the Gyrodyne development, because it's really easily accessed from Route 347. It's a straight shot right down 347.

According to Nelson and Pope, in 2017, almost three years ago by the way, Stony Brook Road is already 60 percent overcapacity. Let me repeat, 60 percent overcapacity, and that was in 2017.

SMITHTOWN PLANNING BOARD

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In days when Stony Brook University is in session, there is upward of 22,000 cars on that road daily. That's 22,000 cars, trucks, and 18-wheelers that residents already have to contend with. There is total gridlock at peak travel times. And if you don't believe me, I've got the video to show you.

According to the Suffolk County Police Department -- who were kind enough to provided me with numbers yesterday -- there were 124 accidents on Stony Brook Road in 2019. That's a 10 percent increase over the prior year.

The more traffic we have, the more accidents we are having. Stony Brook Road is an incredibly heavy, dense, horrible road to come down, and it's a residential road.

The Stony Brook Fire Department or the Stony Brook Fire District, the EMTs and the fire trucks, already have trouble navigating through gridlock. It's a safety hazard for anyone who needs emergency services. And if you look at the video,

SMITHTOWN PLANNING BOARD

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you would see there is nowhere for those fire trucks to go, and nowhere for the EMTs to go. And I don't know about you, but I don't want to be either in the back of one of those ambulances, and I don't want to have my loved one in there, because they can't make a call, or my house is burning down.

Yet the DEIS mentions Stony Brook Road numerous times -- which, by the way, happens to be in the Town of Brookhaven -- as a place to basically dump all the extra traffic from the Gyrodyne development.

By the DEIS recommending Stony Brook University provide the Gyrodyne development an easement through the property the way it is configured right now, they would actually give them an easement that would go from the property in question, right through and come right out on Stony Brook Road to help ease that traffic; or possibly reopen one of the roads that's there behind some homes on University Heights Drive. That was closed a long time ago to stop people from cutting through. So they are

SMITHTOWN PLANNING BOARD

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thinking about opening that one up for us too over in the Town of Brookhaven.

That would already put more cars on an already overburdened, overcapacity, and dangerous Stony Brook Road. And what about Mills Pond Road? That's another small residential road. It's the only other north/south road with access to Gyrodyne. If you have been on that road, you know it's a small road. It's already overcapacity, and with the new build-out there, there's no way that road is going to be able to manage that.

The DEIS needs to take a hard look at the arterial roads that link up to those three roads immediately surrounding Gyrodyne's development, which it has not done. There are a lot of other feeder roads into that that we know of, and the DEIS didn't even look at those. It barely looked at Stony Brook University, and I can tell you they don't have the numbers that I have as far as traffic, and I will be happy to provide you with the information I have from Nelson and Pope and the Suffolk County

SMITHTOWN PLANNING BOARD

TR-33
cont.

1 Police Department. The traffic study
2 portion of the DEIS, we believe, is
3 deficient.

4 As a long-standing resident of the
5 area, I think it's almost abusive that the
6 DEIS does not take into the concerns of
7 Gyrodyne's immediate neighbors, the Town of
8 Brookhaven and the Head of the Harbor. How
9 can you plan in a vacuum? How could you
10 even think it's okay to dump more traffic
11 on a road that's already oversaturated?

12 Oh, and by the way, expect another
13 municipality to foot the financial bill for
14 road maintenance, safety, and the
15 quality-of-life issues that this is going
16 to generate.

17 You are dumping all that traffic on
18 the Town of Brookhaven and we have
19 absolutely no say in this, and it was
20 developed in a vacuum. And I'm sorry.
21 That's just not acceptable. We will be
22 providing you some written comments later.

23 **CHAIRMAN DeSORBE:** Thank you.

24 **BOARD MEMBER MARCHESI:** Thank you.

25 **DIANE SANDER:** Thank you. I think

SMITHTOWN PLANNING BOARD

1 I'm the last speaker.

2 **CHAIRMAN DeSORBE:** Would you put
3 your name and address on that sheet.

4 **DIANE SANDER:** I will, as soon as I
5 finish speaking. Diane Sander, 45 Malvern
6 Lane, Stony Brook Road.

7 I'm here tonight to read a statement
8 from Carl Safina. I think you know who he
9 might be. For those of you who may be
10 unfamiliar, Carl Safina's work has been
11 recognized with McArthur, Pew and
12 Guggenheim fellowships; and his writing has
13 won Orion and Lannan literary awards, and
14 the John Burroughs, James Beard, and George
15 Rabb medals. He has a Ph.D. in Ecology
16 from Rutgers. Safina is the inaugural
17 holder of the Endowed Chair for Nature and
18 Humanity at Stony Brook University; and is
19 the founding president of the
20 non-for-profit organization Safina Center.
21 He hosted the ten-part PBS series, Saving
22 the Ocean with Carl Safina.

23 The subject is: My Opposition to
24 Gyrodyne Proposal at Flowerfield's
25 Property.

SMITHTOWN PLANNING BOARD

1 PN-3

I am a resident of Setauket.

2 Professionally, I am an ecologist, author
3 of various books on human impacts on the
4 living world, and I hold the Endowed Chair
5 For Nature and Humanity at Stony Brook
6 University's School of Marine and
7 Atmospheric Science.

8 Gyrodyne's proposal for a hotel and
9 conference center, medical offices, and
10 assisted-living facility with a major
11 sewage plants strikes me as it strikes so
12 many others as entirely wrong for its
13 location.

14 This is a light industrial
15 development in a non-industrial area, and
16 would mar the nature of our community, and
17 create a prior condition for other
18 industrial developments along the corridor.

19 Gyrodyne's proposed development
20 would be more suited to the light
21 industrial corridor along the Long Island
22 Expressway. It would be extremely out of
23 place at Flowerfields. It is simply wrong
24 for our area. The concerns raised about
25 crowding traffic, additional nitrogen, and

SMITHTOWN PLANNING BOARD

1 PN-3 | groundwater going into Stony Brook's
 2 cont. | Harbor, shellfish beds and the Sound strike
 3 | me as serious and sufficient to stop this
 4 | project.

5 TR-2 | The added noise and congestions of
 6 | tens of thousands of vehicle trips daily on
 7 | already overburdened roads in our area

8 GROW-4 | strike me as unacceptable. So does the
 9 | future copycat development that would near
 10 | certainly follow if this proposed misplaced
 11 | development goes forward.

12 | Route 25-A must not be allowed to
 13 | morph into another Jericho Turnpike, nor
 14 | can St. James be allowed to become the new

15 VIS-1 | Commack. This project, if built, would
 16 | help ruin the character of our communities,
 17 | make our roads more hazardous than they
 18 | already are, and encourage even more

19 GROW-4 | inappropriate development.
cont.

20 | I strongly oppose Gyrodyne's
 21 | proposed Flowerfield development plans.

22 | Thank you.

23 | **CHAIRMAN DeSORBE:** Thank you.

24 | **BOARD MEMBER MARCHESI:** Thank you.

25 | **JUSTIN BRYANT:** My name is Justin

SMITHTOWN PLANNING BOARD

1 Bryant. What do I have to give you, my
2 address? Stony Brook, New York.

3 **CHAIRMAN DeSORBE:** Also on
4 the street address.

5 **BOARD MEMBER MARCHESI:** How about a
6 street?

7 **JUSTIN BRYANT:** Seabrook. I live on
8 a street.

9 **BOARD MEMBER MARCHESI:** And a
10 number?

11 **JUSTIN BRYANT:** 26.

12 **BOARD MEMBER MARCHESI:** There we go.

13 **JUSTIN BRYANT:** Just don't show up
14 at my house, please. I will keep my
15 remarks short, because I know everyone
16 wants to leave.

17 I just wanted to sincerely thank the
18 Board for allowing us to be here to express
19 our concerns, both for and against this
20 proposal. I would also like to thank you
21 all for taking the time out of a busy
22 holiday season to read a 3,000-page
23 document. I don't know many people that
24 would want to do that. So I thank you for
25 doing your job as public servants.

SMITHTOWN PLANNING BOARD

1 I have lived in the area my entire
 2 life. I was born in Smithtown. I went to
 3 school at St. Pats. I was confirmed at
 4 St. Pats. I made my Holy Communion at
 5 Saints Philip and James. So I have a
 6 strong connection to this place.

7 I don't see it as an issue that
 8 should divide our two towns. In fact, I
 9 think it's something that should unit us.

10 SOIL-14 I have some serious concerns. I
 11 read through a bulk of the EIS. Most of it
 12 pertaining to some of the lack of thorough
 13 environmental study that should have taken
 14 place.

15 There are a lot of legacy pollutants
 16 that persist at that site. I have
 17 documentation that I would be happy to
 18 provide to you that is from when it was
 19 owned by Flowerfields back in the early --
 20 prior to 1950.

21 Some of these contaminants include,
 22 led, arsenic, naphthalene, calcium cyanid,
 23 methylbromide, and a number of other
 24 substances that are not recognized. At the
 25 time they were not; but now they are. They

SMITHTOWN PLANNING BOARD

SOIL-14
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have the potential to cause grave impact to health -- human health and our environment.

Some of these chemicals are persistent and stay in the ground for many generations. And unfortunately, they have the potential to leach into our sole source aquifer. That will be incredibly problematic if we want to continue to live cancer-free and free of neurological and developmental disabilities for our children.

I think that for the Town not to conduct a rigorous and more thorough evaluation and remediation of this proposed subdivision, in my opinion, would be negligence.

I just wanted to also point out one other detail, the Environmental Impact Statement prepared for SUNY, Stony Brook back in, I believe, it was 2004, they interviewed -- this was performed by KTR -- I believe they interviewed Clint Borgstrom (phonetic). He was an employee of Gyrodyne, and the questions pertained to whether there were any hazardous materials

SMITHTOWN PLANNING BOARD

SOIL-14
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known at that facility at that site. He answered affirmatively to just about every question.

That stands in stark contrast to the EIS that was provided this year. Marigene Galluccio, she answered this on behalf of Gyrodyne. She worked there for 13 years, and she answered no to every single question that Mr. Borgstrom answered yes to.

That's concerning to me, that there is such a contradiction between the two EIS's considering they pertain to the same exact property. I think it raises some fundamental questions of validity of some of the statements and conclusions reached in this report, and I would encourage you to do stronger and more thorough due diligence before you approve this subdivision.

Thank you.

CHAIRMAN DeSORBE: Thank you.

BOARD MEMBER MARCHESI: Thank you.

CHAIRMAN DeSORBE: I want to thank you for your comments, and to remind you

SMITHTOWN PLANNING BOARD

1 that there will be no vote tonight, and
2 that the DEIS is on the Smithtown website
3 for your review again, and also for your
4 public comments. Please feel free to
5 comment, and also the comment period will
6 end at 5:00 p.m. January 24, 2020.

7 And thank you again for coming out
8 this evening. We are now going to close
9 the hearing.

10 I'd like to make a motion in the
11 matter of Number 1178 Draft Environmental
12 Impact Statement for Gyrodyne LLC,
13 St. James. I move that we close the public
14 hearing.

15 **BOARD MEMBER MARCHESI:** I second.

16 **CHAIRMAN DeSORBE:** All in favor?

17 (Whereupon, all Board Members said,
18 "aye.")

19 **CHAIRMAN DeSORBE:** Any opposed?

20 Seeing none, the meeting is closed.

21 Thank you.

22 **BOARD MEMBER UNVERZAGT:** I move that
23 we adopt the minutes from the December 11th
24 and November 28th meetings.

25 **BOARD MEMBER MARCHESI:** Second.

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CHAIRMAN DeSORBE: All in favor?

(Whereupon, all Board Members said,
"aye.")

CHAIRMAN DeSORBE: Any opposed?

Seeing none.

Motion to adjourn?

BOARD MEMBER UNVERZAGT: Motion to
adjourn.

BOARD MEMBER MARCHESI: Second.

CHAIRMAN DeSORBE: Meeting is
adjourned.

(Continued on the following page to
include the court reporter's
certification.)

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C E R T I F I C A T I O N

I, Darlene M. Devlin, a Senior Court Reporter for the Supreme Court of the State of New York, County of Suffolk, do hereby certify that the foregoing transcription of the within proceedings held on January 8, 2020 is a true and accurate transcription of my stenographic notes.

Furthermore, photocopies made of this transcript by any party cannot be certified by me to be true and accurate.

Darlene M. Devlin, Senior Court Reporter
County of Suffolk, State of New York

#	<p>103 [1] - 4:21 106 [1] - 4:22 11 [1] - 2:19 114 [1] - 4:23 11776 [1] - 94:23 1178 [1] - 150:11 11787 [2] - 1:12, 2:24 118 [1] - 87:25 11th [2] - 133:22, 150:23 12 [2] - 55:18, 121:3 12.4 [2] - 120:20, 121:4 120 [1] - 84:13 124 [1] - 139:12 127 [1] - 5:4 13 [4] - 4:5, 15:18, 15:20, 149:7 141 [1] - 36:22 142 [1] - 5:5 143 [1] - 87:6 145 [1] - 5:6 15 [3] - 19:15, 35:19, 73:6 150 [1] - 3:7 151 [1] - 3:8 152 [1] - 3:9 16th [1] - 110:4 17 [1] - 90:24 171,000 [3] - 124:6, 125:7, 126:14 177 [1] - 13:15 1790 [1] - 89:10 17th [1] - 109:14 18-acre [1] - 87:24 18-wheelers [1] - 139:4 19 [1] - 103:24 191 [2] - 118:22, 119:2 1910 [1] - 74:24 1950 [1] - 147:20 1960s [1] - 90:6 1974 [2] - 115:22, 116:5 1980 [1] - 92:19 1985 [1] - 63:17 1987 [2] - 119:12, 119:14</p>	<p>2019 [3] - 2:18, 2:19, 139:13 2020 [6] - 1:6, 2:2, 6:3, 9:9, 150:6, 152:8 2023 [1] - 21:13 22,000 [2] - 139:2, 139:3 23 [1] - 4:6 24 [4] - 9:8, 15:23, 130:9, 150:6 245 [1] - 38:12 246 [1] - 113:5 24th [2] - 100:3, 122:13 25 [8] - 12:12, 15:16, 16:2, 18:7, 59:12, 59:14, 119:6 25-A [52] - 20:15, 20:16, 21:7, 21:15, 22:6, 22:8, 22:14, 22:18, 28:6, 31:24, 35:12, 41:7, 41:23, 42:23, 45:5, 61:24, 62:2, 62:14, 68:8, 69:17, 70:2, 70:9, 70:15, 71:6, 85:13, 86:20, 88:14, 88:18, 88:23, 89:1, 89:18, 89:21, 95:21, 95:23, 100:25, 101:15, 104:3, 104:5, 104:23, 105:4, 105:9, 105:13, 105:24, 106:1, 106:7, 106:9, 115:22, 116:9, 130:17, 137:8, 138:4, 145:12 25-to-50 [1] - 79:4 26 [1] - 146:11 27 [2] - 52:23, 116:13 275 [1] - 73:14 278 [1] - 113:5 28th [1] - 150:24 29 [1] - 4:7</p>	<p>57:21, 79:13, 79:14, 79:16, 79:18, 79:23, 80:4 4.3 [1] - 121:9 4.5 [1] - 121:10 40 [7] - 4:8, 16:23, 16:24, 22:1, 24:15, 63:16, 114:25 41 [2] - 54:11, 119:15 420 [1] - 1:12 45 [4] - 35:19, 119:19, 120:2, 143:5 455 [1] - 64:6 47 [1] - 4:9 48.7 [1] - 36:4 49 [3] - 50:14, 54:14, 54:19 4th [1] - 41:2</p>	
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*Final Environmental Impact Statement
Map of Flowerfield Subdivision Application*

December 2020

Appendix D: Conservation Board Hearing Transcript

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TOWN OF SMITHTOWN
MEETING OF THE CONSERVATION BOARD

GYRODYNE LLC, SUBDIVISION #1178,
APPLICANT.

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January 30, 2020
3:30 P.M.

A P P E A R A N C E S:

- ARMOND DEROSE, CHAIRMAN
- LAWRENCE LIQUORI, MEMBER
- JACK HESSLE, MEMBER
- THOMAS D. McCARTHY, MEMBER
- KATHLEEN ALBRECHT, MEMBER

1 A L S O P R E S E N T :

2

3 D E P A R T M E N T O F E N V I R O N M E N T & W A T E R W A Y S

4 H O W A R D B A R T O N I I I , A S S I S T A N T E N V I R O N M E N T A L

5 P R O T E C T I O N D I R E C T O R

6 S T E P H A N I E H U R D , S E N I O R E N V I R O N M E N T A L A N A L Y S T

7

8 A P P E A R A N C E S

9 C E R T I L M A N B A L I N , L L P

10 1 0 0 M o t o r P a r k w a y , S u i t e 5 6 0

11 H a u p p a u g e , N e w Y o r k 1 1 7 8 8

12 B Y : J . T I M O T H Y S H E A , E S Q .

13 A t t o r n e y f o r A p p l i c a n t

14

15 C A M E R O N E N G I N E E R I N G & A S S O C I A T E S , L L P

16 1 7 7 C r o s s w a y s P a r k D r i v e

17 W o o d b u r y , N e w Y o r k 1 1 7 9 7

18 B Y : K E V I N M . M c A N D R E W , R L A , A I C P

19 A L S O P R E S E N T F R O M C A M E R O N E N G I N E E R I N G

20 M A R K W A G N E R , C E P

21 R E B E C C A G O L D B E R G ,

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24

25

1 [HEARING CALLED TO ORDER AT
2 P.M.]

3 CHAIRMAN DEROSE: I'd like to call the
4 meeting to order, the Town of Smithtown
5 Conservation Board, on Thursday January 30th,
6 2020. I'd like to first introduce and
7 welcome new people here. Mr. David Barnes,
8 who is now the Director of Environment &
9 Waterways. Welcome David.

10 MR. BARNES: Thank you.

11 CHAIRMAN DEROSE: We have two new Board
12 members, Mr. Thomas D. McCarthy, as
13 differentiated by Thomas J, and Kathy
14 Albrecht. Welcome.

15 MS. ALBRECHT: Thank you.

16 CHAIRMAN DEROSE: I don't know if you've
17 received minutes of the previous meeting,
18 but it wouldn't matter because you weren't
19 here. So, the older members, you've all
20 received copies of the minutes -

21 MR. LIQUORI: Motion to accept.

22 CHAIRMAN DEROSE: Second?

23 MR. HESSEL: Second.

24 CHAIRMAN DEROSE: All in favor?

25 MR. HESSEL: Aye.

1 MR. LIQUORI: Aye.

2 [ALL IN FAVOR]

3 CHAIRMAN DEROSE: Minutes are
4 accepted. Do we have correspondence?

5 [CORRESPONDANCE DISTRITUBED
6 TO BOARD MEMBERS FOR REVIEW]

7 There are completed SEQRA Reviews.

8 MR. LIQUORI: The Sweetbriar
9 attendance sheets, when are we supposed
10 to receive them?

11 MS. HURD: We're supposed to
12 receive them monthly. Unfortunately,
13 they are a little behind.

14 MR. LIQUORI: I just see that July
15 was received in January.

16 MS. HURD: Yes, that's fairly typical.

17 CHAIRMAN DEROSE: Any comments on
18 the incoming mail?

19 MR. HESSEL: None.

20 CHAIRMAN DEROSE: If not, we can move on
21 to the first item on the agenda. Application
22 for the subdivision approval for Gyrodyne
23 Subdivision #1178.

24 MR. BARTON: Gyrodyne #1178. Southeast
25 corner of New York State Route 25A and Mills

1 Pond Road, St. James.

2 Application for subdivision
3 approval to create eight industrial
4 lots (up to six new industrial building
5 lots) and a ninth lot consisting of open
6 space and a sewage treatment plant,
7 with miscellaneous facilities including
8 roadways and recharge facilities, from a
9 partially developed 74.98 acre parcel of
10 land zoned LI and R-43.

11 On December 11th, 2019 the Smithtown
12 Planning Board, the SEQRA lead agency for
13 the review of this subdivision application,
14 accepted as complete for purposes of SEQRA
15 review, the Draft Environmental Impact
16 Statement regarding this matter. A SEQRA
17 Public Hearing was held by the Planning
18 Board on Wednesday, January 8th, 2020 and
19 the Public Comment Period on the subject
20 DEIS closed on Friday, January 24th, 2020.
21 All questions and comments received at the
22 public hearing, as well as all written
23 questions or comments received by the lead
24 agency during the public comment period,
25 will be addressed in the Final Environmental

1 Impact Statement.

2 As the public comment period of the
3 Draft Environmental Impact Statement is now
4 closed, the Conservation Board is not
5 able to accept any further public comments
6 for inclusion or consideration in the Final
7 Environmental Impact Statement. Rather,
8 the purpose of this meeting is to provide
9 the members of the Conservation Board with
10 the opportunity to identify and discuss
11 among themselves any additional questions or
12 issues which the Board members may
13 wish to see addressed in the Final
14 Environmental Impact Statement.

15 Representatives of the applicant are
16 present to provide a brief overview of the
17 project, following which the Conservation
18 Board members will have the opportunity to
19 identify any additional questions, concerns
20 or issues to be addressed in the Final
21 Environmental Impact Statement.

22 CHAIRMAN DEROSE: We have a number of
23 people here. Maybe you want to introduce
24 each one of yourselves for the record.
25 You're all in connection with the applicant.

1 MR. SHEA: J. Timothy Shea, Jr.
2 Certilman Balin, 100 Motor Parkway,
3 Hauppauge, New York, for the Applicant.

4 MR. WAGNER: Mark Wagner,
5 Cameron Engineering, 177 Crossways Park
6 Drive, Woodbury, New York.

7 MS. GOLDBERG: Rebecca Goldberg,
8 Cameron Engineering, 177 Crossways Park
9 Drive, Woodbury, New York.

10 MR. McANDREW: Kevin McAndrew,
11 Cameron Engineering.

12 MR. ZOPOLSKI: Richard Zopolski,
13 with Cameron Engineering, Civil Engineering.

14 CHAIRMAN DEROSE: Mr. Shea.

15 MR. SHEA: Chairman DeRose, members of
16 the Board, it's a pleasure to be here.

17 I'm going to give a brief overview of the
18 overall prospective of Gyrodyne bringing
19 this subdivision application and will hand
20 over the presentation to Mr. McAndrew with
21 regard to some more specific elements of
22 the project.

23 Gyrodyne has occupied the overall
24 site of Gyrodyne Properites for over
25 sixty years. The Board of Directors, and

1 it's a publically traded company, the
2 Board of Directors is largely populated
3 with people who either currently reside
4 within the immediate area, or who earlier in
5 their life lived in the immediate area.

6 The Board of Directors has given us
7 a direction to balance the need of the
8 shareholders with those of the community in
9 making this subdivision application. In
10 doing so we are attempting to balance
11 potential impacts of traffic, sanitary use,
12 esthetics, and other potential environmental
13 impacts of the subdivision.

14 In doing so we limited some of the
15 alternatives in trying to ensure that any
16 potential future land- owners of the
17 subdivision would be bound by the
18 limitations of the subdivision
19 application and the DEIS, whatever ends
20 up as the planning statement. It's also
21 to give the community at large a clear
22 expectation of what would be developed on
23 this site.

24 We could have come in with a blank
25 slate and allow for any and all of the

1 industrial uses but that doesn't give
2 anybody any kind of expectation as to
3 what the final product is going to look like
4 and would lead to be open to all sorts of
5 types of development.

6 Part of our charge, myself and the
7 Cameron team was to make sure that we had
8 certain limitation around those uses so that
9 they would fit within the scope of the
10 property itself. As such, even though
11 the reference is a nine lot subdivision,
12 really we are contemplating up to three new
13 users on the lot.

14 Currently, there is on the lot the
15 Flowerfield Celebrations Catering Hall, and
16 it's got accessory uses, and the Gyrodyne
17 Industrial office building, which are
18 located along here, along the southwest
19 corner of the site. The new uses that we
20 do contemplate are an assisted living
21 facility, hotel and office in R&D space.
22 They are on various versions of the
23 alternative maps that were provided in
24 our DEIS filings.

25 We don't envision any substantial

1 changes to the existing buildings other than
2 façade changes, upgrades to the site,
3 upgrades to the infrastructure to
4 meet today's standards and eventually, if
5 we are granted the application, a hook up
6 to the sewage treatment plant that is
7 currently, all current users, are on
8 cesspools. So, we'll be taking those off
9 cesspools and outside of having direct
10 discharge into the ground, and
11 into a treatment plant located on that
12 site.

13 As I said, we're trying to take a
14 balanced approach with regard to the
15 potential impact and in furtherance of
16 of the same in negotiations and discussions
17 with the Town, we have held a 200 foot
18 buffer area along 25A, which would be
19 largely undisturbed and or replanted in
20 certain sections, and the site is going to
21 be utilizing an interior road configuration
22 which will not be dedicated to the Town.
23 So, the interior road configuration here is
24 going to be the responsibility of a property
25 owner's association so the Town won't be

1 responsible for maintenance of any of those
2 roads after they're constructed.

3 The property owners association will be
4 responsible for all of the inside
5 improvements of the community areas,
6 including all of the roadways, walkways,
7 etc.

8 So, I'm going to turn the presentation
9 over to Mr. McAndrew and he can give you
10 a little bit more detail, and then Mr.
11 Wagner on sanitary issues.

12 MR. McANDREW: Thank you, Tim. Good
13 afternoon. Again, Kevin McAndrew, Partner
14 with Cameron Engineers, the planners and
15 architects for Gyrodyne. I'm going to pass
16 out a hand out to the Board members.

17 What I'm passing out is the Power Point
18 presentation that was made to the Planning
19 Board on January 8th. I have those slides
20 here mounted on the board. I'm going to
21 look to take you through this as briefly as
22 I can, and I'll use my notes also. I'll
23 keep my presentation as brief as possible.

24 As Mr. Shea has eluded to, I think
25 there are important supplemental points that

1 I'd like to touch upon specifically because
2 I think it's relevant to this Board,
3 starting with the distinctive planning and
4 engineering approach to the subdivision,
5 and then highlighting several of the
6 relevant environmental and engineering
7 details associated with this.

8 MR. LIQUORI: Mr. McAndrew?

9 MR. McANDREW: Yes.

10 MR. LIQUORI: Is that part of the site?

11 MR. McANDREW: It is.

12 MR. LIQUORI: That photo right there?

13 MR. McANDREW: It is. That is one of
14 the two freshwater ponds on the subject
15 property.

16 MR. LIQUORI: Thank you.

17 MR. McANDREW: If everybody can see
18 the exhibit boards here. The DEIS, which
19 is one of the volumes of three on the table.
20 It's assigned space. It's analytical. It's
21 an engineering based document. One of the
22 distinctive attributes of the approach we've
23 taken, which Mr. Shea also touched upon, is
24 the number of alternatives that the study,
25 and this was prefaced because the approach

1 that we've taken with this application is
2 that there were preferred land uses.
3 While this is a subdivision, the
4 preferred land uses were studied to
5 demonstrate that in combination with
6 preferred land uses could be built out in
7 a sustainable manner, complimentary to one
8 another, minimizing impacts and also
9 introducing a number of important benefits.

10 I wanted to just briefly expand upon
11 one of the points Mr. Shea said, that
12 75 acres is not being developed. As
13 Mr. Shea pointed out, 13 acres is the
14 caterer, approximately 13 acres is the
15 Gyrodyne Industrial buildings and
16 surrounding site.

17 Each of the alternatives are in the
18 document other than as of right light
19 industrial have a commonly owned parcel,
20 which is approximately 24 acres. So,
21 that would be 13 and 13, and 26 and 24.
22 It's approximately 50 acres. That means
23 Approximately 25 acres of the property,
24 which would be developed and it's more
25 specifically laid out on alternate ten,

1 which I'll touch upon briefly, which would
2 be three new users on the campus.

3 The approach that we've taken to the
4 property with open space preservation,
5 natural buffer retention, is referred to
6 as a conservation subdivision approach.
7 Gyrodyne did not opt to look to maximize
8 the development yield in the form of
9 development lots for the overall density
10 on this property. They've taken a balanced
11 approach again which regulates the intensity
12 of the development.

13 Each of the alternatives studied, other
14 than the as of right light industrial,
15 demonstrate that over 30 acres ,of
16 approximately 40% of the property, would be
17 a combination of green space and open space,
18 natural buffers and contiguous managed
19 landscape.

20 Probably the most distinctive planning
21 approach associated with this application
22 are what we call development thresholds,
23 and development thresholds associated with
24 limiting the total of volume of traffic
25 during the peak hour that would be generated

1 as well as the total sanitary flow.
2 By doing so, it is effectively regulating
3 the intensity of the development and as an
4 effect there's a mitigating measure and to
5 minimize any external impacts associated
6 with the development. I'll touch upon that
7 just a little bit more in a couple of
8 slides.

9 One of the important outcomes of
10 this project is the significant
11 economic benefits that are generated.
12 The land uses that were studied: Office,
13 medical office, research and development,
14 hotel and assisted living. These weren't
15 arbitrary. There were extensive market
16 studies performed by Gyrodyne that
17 identified these uses to not only be
18 complimentary to the caterer and the uses
19 on the property, but to be complimentary
20 and synergistic with Stony Brook Medical,
21 Stony Brook University, as well as Downtown
22 St. James, which is an important
23 consideration.

24 The market study, with those
25 various land uses was modeled in a fiscal

1 impact, which really yielded some very
2 compelling numbers and I'm just going to
3 give you two. The project at full build out
4 is three uses, and combination which is on
5 the property which generates over 3.5
6 million dollars and net new annual property
7 taxes. At full build out, the proposed
8 project is estimated to generate
9 approximately 90 million dollars
10 from direct economic output on an annual
11 basis with hundreds of high quality jobs.
12 There's no increase to school children
13 associated with this and also important,
14 as Mr. Shea noted, there's no burden or
15 minimal burden I should say, on Town
16 resources. The infrastructure, the roads,
17 the wastewater, water supply, storm water
18 management, and open space improvements
19 which are walking trails, which I'll touch
20 upon those, will all be the responsibility
21 of the property owner's association.
22 So, the net result of the economics are
23 really significant to the Town and the
24 various recipients of this project.

25 I'll share with you what I stated to

1 the Planning Board when we had the hearing
2 two years ago, as well as earlier this month
3 and the issue with traffic concerning
4 impact, we completely understand.

5 Cameron Engineering has studied traffic
6 around the Gyrodyne area for the last 15
7 years. Gyrodyne has been a client of ours
8 for over 20 years. The Traffic Impact Study
9 that we prepared confirmed that traffic
10 conditions concern is valid. There's
11 longstanding issues around the area,
12 primarily near 25A and Stony Brook Road.

13 The approach that Gyrodyne has
14 taken is a combination of mixed uses,
15 which have offsetting peak uses and one
16 of the perfect examples of the mixed
17 uses is the assisted living. It's one of
18 the lowest intensity uses. In fact,
19 Benchmark Assisted Living has entered
20 into a contract for 10 acres of this
21 property. So, it's a substantial portion
22 of that 25 acres that I spoke about.
23 That in combination with R&D office or
24 regular office and the hotel, these uses
25 do not have peak traffic all at the same

1 time in the morning and afternoon, which
2 is where the greatest concern is for
3 the traffic impact.

4 That in combination with assigning a
5 cap or a limit to the overall trips that
6 can be generated provides the degree of
7 mitigation that will effectively manage
8 the new traffic that will be generated
9 in conjunction with the proposed mitigation.
10 The traffic impact study spells out a set
11 of mitigation measures. The primary
12 intersections that would be improved
13 will be Mills Pond Road and 25A, there
14 will be a new signal at that location,
15 there will be new turn lanes.

16 The other primary intersection
17 is Stony Brook Road and 25A. Cameron
18 Engineering, myself and Rebecca and
19 Rich we met with DOT over the holidays and
20 they're currently studying two alternatives
21 to rework that intersection, one is a
22 roundabout and the other is a signalized
23 T-Intersection. They're projecting the
24 new intersection to actually build
25 out that new intersection in 2023, which

1 really kind of syncs well with the build out
2 of this application.

3 Also, I think a nice update is that
4 the intersections that were identified on
5 Stony Brook Road, the Town of Brookhaven
6 actually completed those improvements last
7 year. That is at the South Drive, the main
8 University Drive from the west side and
9 then Oxhead Lane.

10 The other intersections where there's
11 turn lanes and signalization timing are at
12 347 and Stony Brook Road. Gyrodyne took a
13 very expansive look at this and goes all
14 the way out to making some minor
15 signalization in St. James and changes over
16 at Moriches Road and 347.

17 I want to just quickly take you
18 through this slide that talks about
19 the significance of open space preservation
20 and natural buffers, contiguous landscape.
21 It's important that the sensitivity that's
22 been provided for and planned to preserving
23 the road-scape of 25A. It's a significant
24 historic corridor. There'll be no
25 disturbance on 25A other than the single

1 right in and right out access in the middle
2 of the property.

3 So, there's 2,700 linear feet, and
4 there's going to be one opening, right in
5 and right out. There will be an existing
6 buffer maintained, minimum 200 hundred
7 feet of depth. Importantly, that entire
8 buffer as well as Mills Pond will be
9 augmented with significant planting.
10 Gyrodyne has detailed out in the DEIS
11 approximately 330 new indigenous species
12 trees to be planted to further reinforce
13 that buffer along 25A, Mills Pond Road as
14 well as the interior of the property.
15 There will be 2 miles of walking and nature
16 trails within the property that will be open
17 to the public. There will be a permanent
18 bicycle lane on the interior road that
19 Mr. Shea pointed out and will have an
20 external connection with bike routes on
21 25A.

22 The storm water management
23 approach which is very much what's
24 considered green infrastructure,
25 ecologically based, instead of traditional

1 curbs and catch basins. There's going
2 to be roadside bias where runoff from the
3 road will be conveyed to naturalized
4 retention areas and rain gardens if and when
5 those area reach capacity they will
6 then overflow into existing ponds on the
7 property so that we're mimicking a pre-
8 development run off to those freshwater
9 ponds.

10 We'd coordinate this deployment with
11 the DEC and they've endorsed this
12 approach. It's just one of the many
13 green approaches to the application.
14 The other of course is related to
15 ground water recharge in addition to
16 storm water and waste water. I'll take
17 you through the slide. If I need the
18 support of Mr. Wagner, I'll call him up.
19 I think I'm good to go here, just to
20 take you through a few final comments.
21 The waste water treatment is a key
22 infrastructure investment associated
23 with the build out. It's also a
24 significant environmental benefit
25 associated with this application.

1 That particularly pronounced when you
2 look at the comparison of the results
3 of on-site septic systems compared to
4 this proposed waste water treatment
5 plan.

6 If you look at this chart or
7 graph that's on the table here, the
8 waste water treatment plant will reduce
9 nitrogen levels by 80-90% compared to
10 current on-site septic systems. What
11 the graph is showing is that the proposed
12 waste water treatment plant will not only
13 be 75% less than the as of right industrial
14 build out, which would not necessitate
15 a waste water treatment plant, but the
16 waste water treatment plant full build
17 out will actually be less than what is on
18 the property today, just with the caterer
19 and the Gyrodyne industrial buildings.
20 So, it's a very significant improvement.

21 The final point that I wanted to
22 make is that it's an important
23 development issue is that of the viability
24 of the St. James Business District or Lake
25 Avenue Corridor having the ability to

1 connect to the Gyrodyne waste water
2 treatment plant. In the event that the
3 sewer district is established and the Town
4 is desiring such a connection, Gyrodyne
5 would have the ability to treat the
6 St. James Business District waste water and
7 would be willing to discuss such a
8 relationship with the Town.

9 CHAIRMAN DEROSE: Does the Board have
10 any questions?

11 MR. LIQUORI: Not yet.

12 CHAIRMAN DEROSE: Mr. Hessel?

13 MR. HESSEL: I'm just absorbing
14 everything.

HIS-3

15 MS. ALBRECHT: I have a question.
16 You talked about the light at Mills Pond and
17 25A, why not a roundabout seeing that it's
18 a historical district there?

19 MR. McANDREW: One of the important
20 considerations with limiting the trip
21 generation was to not have traffic
22 mitigation improvements change the physical
23 character of that corridor. Really what
24 that means is we did not want to have
25 development require land acquisition,

1 property takings, the ability to
 2 changing that historic character.
 3 So, a roundabout at that particular
 4 location would entail going into the
 5 farm on the opposite side and then
 6 potentially go into the left lane and
 7 historic Mill Pond House.

8 So, it's an improvement, again,
 9 where the roundabout could be
 10 accommodated as an example down in
 11 Stony Brook Road, there's enough room
 12 within that intersection but not
 13 nearly enough room at Mills Pond and
 14 it would result in a lot of trees
 15 coming down and changing that whole
 16 character to that area.

17 CHAIRMAN DEROSE: Mr. McCarthy?

18 MR. McCARTHY: Nothing.

19 CHAIRMAN DEROSE: I noticed in these
 20 different alternatives that you have, one
 21 through ten, that no one is really looking
 22 at one. We're looking at one of them or
 23 a combination of some development of one
 24 or another perhaps. We're looking at what
 25 it might be and not what it will be. Is

ALT-7

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that correct? _____

ALT-7
cont.

MR. McANDREW: That is correct with the clarification that, with Benchmark Senior Living in contract Gyrodyne for 10 acres, that effectively puts focus on alternate ten within the DEIS because that plan that was developed had contemplated that Gyrodyne might in fact at the time that they filed this entered into an agreement with Benchmark.

So, the high likelihood is that in the FEIS, Final Environmental Impact Statement, actually showing alternate ten or a version similar to that being the preliminary subdivision map and the sole alternative, the sole plan that Gyrodyne wants to see within the subdivision.

ALT-7
cont.

CHAIRMAN DEROSE: In any event the whole project, taking different possibilities, assuming that contract doesn't go in but glad to hear that it might be done before the final. You're going to limit the project to the amount of sewage generated or traffic

ALT-7
cont. 26

1 trips generated, is that what you're
2 saying?

3 MR. McANDREW: That's exactly right.

4 CHAIRMAN DEROSE: So, there's always
5 going to be a maximum.

6 MR. McANDREW: There's a self-
7 regulating threshold that unless -- Tim?

8 MR. SHEA: It's arrived at by the
9 nature of the uses and the square footage
10 attributable to each of the uses.

11 So, by taking those three types of
12 uses that we envision here, each one is
13 a sanitary generator or a traffic at a
14 particular formulaic or flow -- Traffic flow
15 let's say or waste water flow. So, what
16 we've done is to try to make sure
17 that the uses all fit within parameters
18 where by the square footage of each
19 individual part of overall subdivision,
20 a maximum amount of traffic at peak
21 hour would be generated, or a maximum amount
22 of sewage flow would be generated by each
23 one of those. So, that's how those
24 alternatives kind of came around. Each
25 one had those maximum thresholds. It's

1 based on square footage and use but it's
2 also based upon the science of looking at
3 traffic generation, trip generation, for
4 each one of those uses and sanitary
5 generation based on each one of those uses.
6 So, you kind of back your way into it. GW-39

7 CHAIRMAN DEROSE: So, am I correct in
8 thinking that the waste water treatment
9 plant would be built to carry the maximum,
10 even though the maximum would never be
11 built?

12 MR. SHEA: Well, the waste water
13 plant will be a little bit larger than
14 currently, a little bit larger than what
15 we finally envision, when we pick the plan,
16 as the final sanitary flow. Suffolk
17 County Department of Health Services
18 requires us to have room to increase that
19 capacity.

20 CHAIRMAN DEROSE: So, you're going ALT-8
21 to pick the plan before final approval?

22 MR. SHEA: We're going to propose,
23 between now and the submission of the
24 Final Environmental Impact Statement,
25 a form of alternate ten which was

1 presented in the diagram.

SEQRA-14

2 CHAIRMAN DEROSE: Right now we
3 have latitude to change but after this
4 is finally approved you won't have
5 latitude to change anymore, is that
6 what you're saying?

7 MR. SHEA: It would be in our
8 best interest not to change. Once we
9 are at this point in time, again, make
10 the shift to a plan that's going to be
11 very much aligned with alternate ten,
12 we're not looking to change at that
13 point. We're looking to get through
14 SEQRA, having preliminary approval by the
15 Planning Board and then go through
16 the final subdivision for this.

17 CHAIRMAN DEROSE: Pardon me for not
18 understanding. If we have, let's say
19 a hotel of a hundred units, are you
20 prevented from coming back in 5 years
21 and saying, well we didn't get the
22 assisted living over here so we want to
23 get 150 rooms over there instead, or
24 would you be locked in?

25 MR. SHEA: It really depends on

SEQRA-14
cont.

1 what the final finding statement
2 says with regard to the limitations,
3 and whether it's a significant deviation
4 from those findings. So, if we came in with
5 150 room hotel and then we came in
6 with a 300 room hotel, and assuming none
7 of the other parts changed, that would
8 be considered a substantial deviation.
9 We would then have to go back and study
10 that impact of that particular item.

11 If it's a change of five rooms here
12 or slightly lower square footage on the
13 office it may be deemed insignificant and
14 not of any significant impact. So,
15 basically, the plan that we end up
16 finalizing when we go through the FEIS and
17 get to the findings and preliminary
18 subdivision will be the plan that is
19 ultimately approved and you would have an
20 expectation that it would be, if not
21 exactly that plan, very substantially the
22 same.

23 MR. McANDREW: I'm going to supplement
24 that by saying that we coordinated with DEW
25 staff. This is a subdivision so the hotel,

1 the assisted living whether it's an office
2 building, those uses will still be subject
3 to site plan approval. So, this is the
4 creation of the lots.

5 One of the reasons we have specifically
6 wanted these land uses is because it is very
7 much contemplated that the uses we studied
8 will in fact be brought forward as site
9 plans and since they've been studied to this
10 degree under the SEQRA process, as long as
11 they are within the parameters of what
12 Mr. Shea is talking about, they will not be
13 subject to further SEQRA review at the time
14 of site plan because it will have been
15 studied and analyzed as part of the process.

16 So, that's one of the distinct
17 approaches that we've taken to where a
18 traditional subdivision would normally
19 come in not even showing anything on the lot
20 per se and just an undeveloped piece of
21 property. We've combined the analysis of
22 preferred land uses at the time of
23 subdivision again to look at this at
24 this point in time in conjunction
25 with the subdivision application.

1 MR. SHEA: We did that, again, the
 2 Board of Directors wanted to make sure
 3 that it was understood they had a
 4 vision of what they wanted for the
 5 overall parcel. So like, if somebody
 6 came in and said we want something
 7 completely different, that would open
 8 up new environmental review.

9 So, if they came in with a 200,000
 10 square foot warehouse, that's not what was
 11 studied and they would have to reopen
 12 SEQRA and would have to go back and have
 13 new studies done. That's why we pigeon-
 14 holed these type of uses because we want
 15 those type of uses, and they fit
 16 together. It's like putting pieces of a
 17 puzzle together.

18 CHAIRMAN DEROSE: Okay. So, what
 19 you're saying and skirting around, and
 20 I'm looking for an explicit statement.
 21 When you come in close to the final and
 22 you have one particular client which is
 23 the assisted living client and the other
 24 clients are hypothetical, and so if in
 25 the future you wanted to change that use

SEQRA-14
 cont.

1 from a medical office building to something
 2 else, you are going to want to have that
 3 right to do so?

SEQRA-14
 cont.

4 MR. SHEA: We'd have to come back in
 5 and have that use studied.

6 CHAIRMAN DEROSE: So, the drawing will
 7 never represent what is going to be the
 8 final development here. It's conceptual.

9 MR. SHEA: I think it's kind of an
 10 over-characterization I would say. If we
 11 wanted to deviate from this, we'd have
 12 to go back and start the process again.

13 CHAIRMAN DEROSE: But you could?

14 MR. SHEA: We'd have to start from
 15 the beginning.

16 CHAIRMAN DEROSE: But you could?

17 MR. SHEA: The goal here is not
 18 to go back.

19 CHAIRMAN DEROSE: If you subdivided a
 20 piece of property into one-acre residences,
 21 you normally would not expect that
 22 somebody could come back later on to
 23 change that. Here, you're retaining the
 24 right to come back and change this at
 25 some time.

SEQRA-14
 cont.

1 MR. McANDREW: Not even the subdivision,
2 the lot line will be fixed at the time
3 we come back for the Final Environmental
4 Impact Statement. It's only at the time
5 post subdivision approval that if there's
6 a different use that would have been
7 studied and it is of such a magnitude that
8 it deviates from the land uses that we
9 studied, that particular use would be
10 subject to further environmental study at
11 the time of site plan.

12 CHAIRMAN DEROSE: So, you're retaining
13 the right to change things?

14 MR. SHEA: If I may. I think what
15 you're -- We're not retaining the right to
16 change things so to speak in those terms.
17 So, we could have come in with simple lot
18 lines and we would have had to study all the
19 uses and then if we studied those uses
20 within the context of the subdivision,
21 that anybody could basically come in and
22 if DEW asked us in their scoping to study
23 all of the potential outlined uses, then
24 there would be no control and anybody
25 could come in.

1 CHAIRMAN DEROSE: I'm not saying
2 that what you're doing is not good.

3 MR. SHEA: I wanted to make sure that
4 you understand that so if you come in
5 with any piece of property and you have
6 it built for whatever, and 20 years goes
7 by and the market has changed and let's
8 just say your average commercial center,
9 you have 5 acres of property, you got
10 a strip mall maybe something like that.
11 The market is not there anymore for that
12 type of use and now you come back in
13 and you want to put in apartments or you
14 want to put in drive through restaurant,
15 you always have that ability to come
16 back in and ask the Town for permission to
17 do that as long as it meets with the Zoning
18 Code. This application is no different
19 than that in that we're asking for a
20 subdivision and we have certain parameters
21 that are studied within the subdivision.
22 If somebody wants to deviate from the
23 subdivision, either with regard to the
24 lot lines or with regard to the uses,
25 they have to make a new application,

1 just like any other land owner. So, yes.
 2 The only approval we are seeking is for
 3 the subdivision with the uses that were
 4 studied. That's all that can be built out
 5 as of right, assuming that an approval is
 6 granted. If something deviates, you're
 7 starting over from the beginning.

8 CHAIRMAN DEROSE: Okay.
 9 So, really what we're approving now is
 10 a subdivision of the probability of
 11 the first -

12 MR. SHEA: Well, the subdivision
 13 because even after the subdivision the
 14 end user has to come in and do a site
 15 plan. When they do the site plan, then
 16 they do the more complex engineering,
 17 the lighting, building renderings, all of
 18 those very site specific type of things
 19 and that is going to be subject to
 20 review by DEW and by Planning.

21 CHAIRMAN DEROSE: Okay. Another
 22 question I have is ,what is going to
 23 be the sequence of building the roads
 24 and the waste water treatment plant,
 25 and the hooking up of that waste

MISC-3

1 water treatment plant to the existing
2 industrial buildings as well as the
3 catering facility? Is that going to
4 be hooked up first?

MISC-3
cont.

5 MR. SHEA: I think it would have to
6 be. Again, we're in the contract with
7 the assisted living facility and it's
8 going to be a decent flow generator,
9 and Suffolk County DPW is going to
10 require the treatment plant be
11 built prior to opening up that facility.

12 CHAIRMAN DEROSE: And?

13 MR. SHEA: We would have to hook up
14 the - Suffolk County Department of Public
15 Works and Suffolk County Health Department
16 require that you hook up all of the parcels
17 on the site when you put in a treatment
18 plant. So, they're going to have to
19 hook them all up at that time. There
20 could be a slight deviation but normally
21 they want all of the existing users to be
22 hooked up at the time that they issue the
23 Certificate of Occupancy for the new
24 building. So, you can't open up the
25 assisted living until you get the other

1 buildings hooked up.

2 CHAIRMAN DEROSE: Okay. Thank you.

3 MR. SHEA: Thank you.

4 CHAIRMAN DEROSE: Any other Cameron
5 presentations? No.

6 [INDICATING NO OTHER
7 PRESENTATIONS]

8 Okay. Mr. Liquori?

9 MR. LIQUORI: No questions.

10 CHAIRMAN DEROSE: Mr. Hessel?

11 MR. HESSEL: No.

12 MS. ALBRECHT: No.

13 MR. McCARTHY: No questions.

14 MR. LIQUORI: Very thorough
15 presentation.

16 CHAIRMAN DEROSE: That is a terrific
17 idea and doing this for the overall
18 parcel instead of coming in piecemeal.
19 It's impressive. I have one request,
20 in looking at the tree clearing and
21 planting plan, the trees that are cut
22 in that plan appear to me to be the trees
23 that are necessary to be cut for the sewage
24 treatment plant, as well as the interior
25 roads but none of the other trees on the

ECOL-8

ECOL-8
cont.

38

1 lots are showing as cut, and know that
2 some are going to be cut at some time in
3 the future. This Board gets involved
4 in tree clearing on lots of 5 acres or
5 more. So, I'd like to request that
6 the client concede to letting us review
7 the individual lots that are less
8 than 5 acres for tree clearing and
9 planting purposes.

10 MR. McANDREW: At the time of
11 site plan?

12 CHAIRMAN DEROSE: At the time of
13 site plan. Yes, sir.

ECOL-8
cont.

14 MR. SHEA: I don't think they'd have
15 issue with that. I believe there is a
16 required tree clearing preservation survey
17 to DEW, which would then go to the
18 conservation Board because it would be
19 greater than 5 acres.

20 CHAIRMAN DEROSE: Well, some of them
21 are less.

22 MR. SHEA: I think the only one that
23 is less is the hotel. We can agree to that.

24 CHAIRMAN DEROSE: I have no further
25 questions. Okay.

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MR. SHEA: Thank you very much for your time. I appreciate it.

[HEARING CONCLUDED]

CHAIRMAN DEROSE: One more thing, you received a copy of the proposed lease agreement. Do we need a resolution for this?

MR. BARTON: No.

CHAIRMAN DEROSE: Okay. Thank you.

[TIME NOTED: 4:47 P.M.]

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C E R T I F I C A T E
STATE OF NEW YORK)
SS:
COUNTY OF SUFFOLK)

I, CHARMAINE DEROSA, shorthand reporter
and Notary Public in and for the State of
New York, do hereby certify:

THAT, the foregoing is a true and accurate
transcript of my stenographic notes.

IN WITNESS WHEREOF, I have set my hand
on this 30th day of January, 2020.

Charmaine DeRosa



CHARMAINE DeROSA, CSR

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*Final Environmental Impact Statement
Map of Flowerfield Subdivision Application*

December 2020

Appendix E: Municipal Comments from 2017 and 2018

Submitted by
at public hearing for SEQRA

**STATEMENT FROM BROOKHAVEN TOWN SUPERVISOR
EDWARD P. ROMAINE REGARDING THE PROPOSED
GYRODYNE DEVELOPMENT**

Dear Smithtown Planning Board Chairman Chayes and Members:

I want to thank the members of the Smithtown Planning Board for allowing me to comment on the preliminary subdivision proposed for the Gyrodyne property.

As you know, the Gyrodyne property, one of the largest properties in Smithtown to be fully developed, directly abuts the Town of Brookhaven. The proposed development has the potential for significant impacts on the environment, traffic, water quality, and air quality in St. James, Head of the Harbor and the Town of Brookhaven.

I am concerned about the traffic impacts on New York State Route 25A (North Country Road), Stony Brook Road, Oxhead Road, and Mills Pond Road. The carrying capacities of all these roads exceed the recommended limits. Additional development of the Gyrodyne property will only exacerbate traffic congestion on these roads and other local roads, such as Main Street in Stony Brook.

Likewise, I am concerned about the proposed construction of Road "C" that would require the opening of a defunct railroad crossing on the Gyrodyne property. Opening this Long Island Rail Road crossing would cause an unnecessary hazard to the motoring public.

Even more ominous, the construction of Road "C" would create access from the Gyrodyne property to Stony Brook Road. This residential road is over utilized as a secondary entrance to the State University and is beyond its carrying capacity. Adding traffic to this road from the proposed Gyrodyne development is unacceptable and Brookhaven is prepared to take whatever step is necessary to prevent this from happening.

Frankly, this proposed development has growth-inducing aspects that will negatively impact the quality of life in both Brookhaven Town and Smithtown; this needs to be seriously considered.

In examining the proposed site plan, there appear to be incompatible uses on several of the lots, including the proposed hotel and medical offices.

Construction of an on-site Sewage Treatment Plant (STP) directly adjacent to private property in Brookhaven Town zoned and used for single-family residences, raises a number of concerns:

Sanitary flow is a concern. STP technology proposed should not just meet the drinking water standard of 10ppm (parts per million) of nitrogen in effluent, it should use Best Available Technology to reduce nitrogen loading to the maximum extent possible. Nitrogen loading rates should be reported in annual loads by quantity, not as concentrated effluent.

The site of the wastewater treatment plant is currently shown as preserved lands. This should be changed to reflect the development of Sewage Treatment Plants.

The buffers needed to protect the extensive wetlands on the site are not currently adequate and should be expanded.

Currently, the site is actively used by grassland species such as American Kestrel and Eastern Meadowlark. Grassland species have declined precipitously on Long Island. Habitat preservation of grassland species should be addressed in any review of this proposed development.

Lastly, impacts to the nature and character of the areas need to be addressed. The proposed development conflicts with the current nature and character on and around the site.

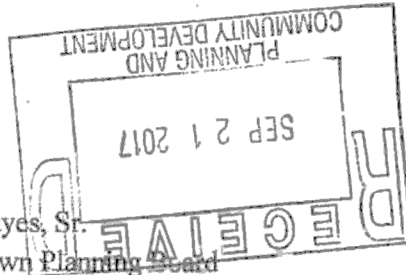
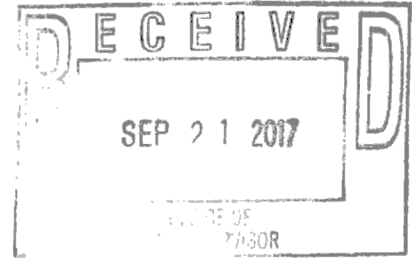
The Town of Brookhaven requests and supports the adoption of a SEQRA Positive Declaration for this project. We also request that a Public Scoping meeting be convened to solicit the concerns of local residents. This should result in the preparation of a full environmental Impact Statement to analyze each of the above concerns and determine if mitigation of the significant impacts is possible or the proposed development is too intensive. The Environmental Impact Statement must comprehensively consider the development in its entirety and not divide or segment the subdivision and future uses.

Again, thank you for allowing me to place my concerns about this proposed development on the record.



Town of Brookhaven Long Island

Edward P. Romaine, Supervisor



September 20, 2017

Mr. Conrad A. Chayes, Sr.
Chairman, Smithtown Planning Board
99 W. Main Street
Smithtown, NY 11787

Dear Chairman Chayes:

I am writing to you regarding my concerns about plans recently submitted to your Town by Gyrodyne for their 62-acre parcel along the Smithtown-Brookhaven border in St. James. While I support the proposal to dedicate approximately 9.5 acres of this property as open space, Brookhaven Town is adamantly opposed to portions of this plan that would cause additional traffic on Route 25A and Stony Brook Road in Brookhaven Town.

The application submitted by Gyrodyne, LLC is to subdivide nearly 75 acres of land into multiple parcels with the conceptual plan that includes the development of a 150-room hotel, two medical office buildings totaling 128,400 feet, and two long-term care buildings that will house 220 assisted units in St. James.

I have also voiced my opposition to this plan to the Suffolk County Planning Commission because of the traffic impact along the Stony Brook Road-Route 25A corridor, as well as the deficiencies in the notice to the Town of Brookhaven on this application.

The Town of Brookhaven is opposed to any traffic created as a result of this proposed subdivision emptying out onto Town roads and, specifically, Stony Brook Road. This road is overcrowded now, particularly because of usage from the University, and does not need additional traffic from a project outside our Town.

I am particularly concerned about the Suffolk County Planning Commission's recommendation, which stated: *"The existing access easement that connects across Long Island Rail Road track*

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right-of-way to the east should also be a separate right-of-way and built to the same specifications of the other roads in the subdivision."

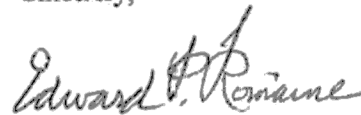
However, this recommendation fails to note that the existing Long Island Rail Road (LIRR) access that connects Gyrodyne to the State University at Stony Brook's property to the east and then to Stony Brook Road is a private easement and currently closed to traffic. Clearly, private Gyrodyne traffic should not use public State University roads to alleviate traffic. Moreover, the LIRR crossing creates more vehicle and pedestrian safety issues than the applicant's proposed curb cut onto NYS Route 25A.

The fourth recommendation of the Suffolk County Planning Commission stated: "*Allowing for the creation of a new ingress or egress along North Country Road (State Route 25A) should only be considered after it has been determined that access along Mills Pond Road, existing point access along SR 25A, and Stony Brook to the east cannot adequately accommodate vehicular trips generated by proposed uses on the property. This could involve a traffic impact study as well as the monitoring of traffic generated (over time) as the proposed lots are developed and occupied.*"

The recommendation makes the erroneous assumption of existing access to Stony Brook Road, which doesn't exist. It also makes the erroneous presumption that the applicant controls property east of the LIRR track. Lastly, it incorrectly assumes that adding a new formalized at-grade LIRR crossing is safer than the existing access to Mills Pond Road or new access to Route 25A.

The Town is disappointed that the Suffolk County Planning Commission and its staff did not provide timely and complete notice of the proposed subdivision. The Town has serious issues regarding potential traffic that would be created on Stony Brook Road. Accordingly, I would appreciate any consideration by the Smithtown Planning Board of the issues that I have raised. Brookhaven Town is committed in its opposition to allow any additional traffic on Stony Brook Road and will take whatever action is necessary to prevent this from happening. The Town likewise has grave concerns about additional traffic being generated on New York State Route 25A.

Sincerely,



Edward P. Romaine
Supervisor

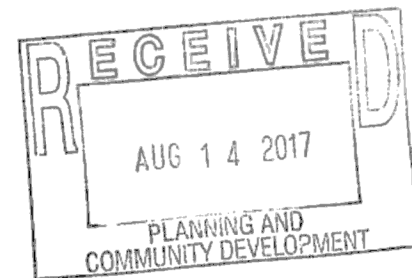
Cc: Hon. Patrick Vecchio, Smithtown Town Supervisor
Hon. Thomas J. McCarthy, Smithtown Town Councilman
Hon. Edward R. Wehrheim, Smithtown Town Councilman
Hon. Lynne C. Nowick, Smithtown Town Councilwoman

Hon. Lisa M. Inzerillo, Smithtown Town Councilwoman
James T. Ehrhardt, Smithtown Planning Board
Barbara DeSorbe, Smithtown Planning Board
William Marchesi, Smithtown Planning Board
Thomas Unverzagt, Smithtown Planning Board
Paul Sabatino II Esq., Attorney to Smithtown Planning Board
Lee Krauer, Chairperson, Friends of Stony Brook Road
James Komosinski, Friends of Stony Brook Road



Town of Brookhaven Long Island

Edward P. Romaine, Supervisor



August 9, 2017

Dear Director Lansdale,

I am in receipt of your letter regarding the proposed Gyrodyne, LLC subdivision in Smithtown that is immediately adjacent to the border with the Town of Brookhaven.

On June 21st, the Suffolk County Planning Commission received a referral concerning the subdivision from Smithtown.

On July 26th, the County Planning Commission mailed a referral letter to Brookhaven, which was received by the Town on July 31st.

On August 2nd, the County Planning Commission held a hearing at 2 p.m. on this subdivision.

The County Planning Commission was not timely in providing a two-day notice for a response. The County Planning Commission has 45 days to consider referral and had been in possession of the application for over a month prior to sending notice to the Town of Brookhaven.

Furthermore, the Notice provided to the Town of Brookhaven only contained the cover letter and the subdivision plat. There was no application, no Environmental Assessment Form, no project description, nor any other material that would typically have been included for referrals. Finally, there was no indication of proposed uses in the Notice, nor in the Suffolk County Planning Commission resolution. For example, if a hotel or ALF (assisted living facility) is proposed, it is not reflected in any of the material the Town has received to date.

We raised these issues to state unequivocally that the Town of Brookhaven is opposed to any traffic created as a result of this proposed subdivision emptying out onto town roads and, specifically, Stony Brook Road. This road is overcrowded now, particularly because of usage from the University, and does not need additional traffic from a project outside our town.

I specifically point to the second recommendation, which stated: "The existing access easement that connects across Long Island Rail Road track right-of-way to the east should also be a separate right-of-way and built to the same specifications of the other roads in the subdivision."

However, this recommendation, fails to note that the existing Long Island Rail Road (LIRR) access that connects Gyrodyne to the State University at Stony Brook's property to the east and then to

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Stony Brook Road is a private easement and currently closed to traffic. Clearly, private Gyrodyne traffic should not use public State University roads to alleviate traffic. Moreover, the LIRR crossing creates more vehicle and pedestrian safety issues than the applicant's proposed curb cut onto NYS Route 25A.

The fourth recommendation of the Suffolk County Planning Commission stated: "Allowing for the creating of a new ingress or egress along North Country Road (State Route 25A) should only be considered after it has been determined that access along Mills Pond Road, existing point access along SR 25A, and Stony Brook to the east cannot adequately accommodate vehicular trips generated by proposed uses on the property. This could involve a traffic impact study as well as the monitoring of traffic generated (over time) as the proposed lots are developed and occupied."

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The Town is disappointed that the Suffolk County Planning Commission and its staff did not provide timely and complete notice of the proposed subdivision. The Town has serious issues regarding potential traffic that would be created on Stony Brook Road. I would appreciate the opportunity to speak to the Town's concerns at the next Planning Commission meeting.

Sincerely,

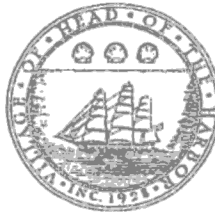


Edward P. Romaine
Supervisor

cc: Jennifer Casey, Chair, Suffolk County Planning Commission
Adrienne Esposito, Vice Chair, Suffolk County Planning Commission
Samuel Chu, Second Vice Chair, Suffolk County Planning Commission
Rodney Anderson, Suffolk County Planning Commission
Michael Kelly, Suffolk County Planning Commission
Samuel E. Kramer, Suffolk County Planning Commission
Matthew Chartrand, Suffolk County Planning Commission
John Condzella, Suffolk County Planning Commission
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Michael Kaufman, Suffolk County Planning Commission
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Kevin G. Gershowitz, Suffolk County Planning Commission
Theresa Ward, Commissioner, Planning and Economic Development, Suffolk County
Louis Bekofsky, Deputy Planning Commissioner, Suffolk County
Andrew Freleng, Chief Planner, Economic Development and Planning, Suffolk County
Theodore R. Klein, Principal Planner, Suffolk County

Tullio Bertoli, Commissioner of Planning, Town of Brookhaven
Ralph Wiebelt, Senior Site Plan Reviewer, Town of Brookhaven
Diane Mazarakis, Principal Planner, Town of Brookhaven

Village of Head-of-the-Harbor



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ANTHONY B. TOHILL
Village Attorney

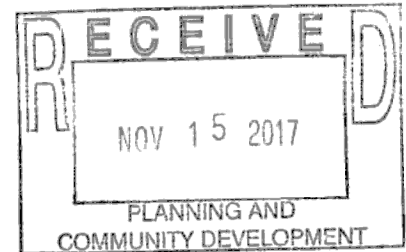
MARGARET O'KEEFE
Village Clerk

PATRICIA A. MULDERIG
Village Treasurer

CHARLES M. LOHMANN
Police Chief

November 8, 2017

VIA FACSIMILE 631-360-7546 AND
US POSTAL SERVICE
Town of Smithtown Planning Board
99 West Main Street
PO Box 575
Smithtown, NY 11787



RE: Preliminary Subdivision
#1178 Gyrodyne LLC
S/s NYS Rt. 25A, E/s Mills Pond Rd. St James
LI & R-43 Zoning Districts
SCTM#: 140-2-4.-13.3,13.4,14&15

Dear Planning Board Chairman and Members:

The Village of Head of the Harbor (the Village) as the neighboring community on the opposite side of North Country Road (25A) from Gyrodyne's property wishes to comment on the Preliminary Subdivision.

As Smithtown's Planning Board's November 15th hearing conflicts with the Village Trustee Meeting, our comments are hereby submitted in writing.

Residents of Head of the Harbor are very proud of their community and have worked hard since the incorporation of our Village to maintain its beauty and rural atmosphere. As Gyrodyne's neighbor, the Village is very concerned that development of the Gyrodyne parcel will increase the volume of car and truck traffic on 25A so as to require new traffic lights and possibly roadside tree removal and widening. We are also concerned about fire protection for a complex that includes two assisted living facilities and a 150-room hotel. In addition, we are concerned about the impact of the new facility to our drinking water and the storm water pollution that may wind up in Stony Brook Harbor from the development site.

The scenic and historic views along the narrow, winding and hilly North Country Road from Setauket thru Stony Brook, Head of the Harbor and Saint James are a major asset that we are tasked to preserve. Historic houses, museums, farms, and parkland line the bucolic route that also is remembered for Revolutionary War history. They represent why we live here and others visit. But they are also fragile and

can easily be destroyed and lost forever. The Village has consistently and successfully worked to protect the quality of Route 25A within its area- it having been well established that widening of this road would destroy local property values and invite heavy traffic with its resultant troubles. Let us not forget there have been threats to 25A as far back as 1945 when New York State proposed to widen the road to four lanes with a mall. The Village successfully opposed this action but it was a close call.

In 1956, and again in 1964, there were proposals to alter the historic road and again the Village persuaded New York State to abandon the idea. In 1960, the Gyrodyne Company petitioned the Town of Smithtown Board to change its zoning from Residential A to Light Industrial. The Village was alarmed over this proposal and filed a protest. Mr. Ward Melville who wished to protect the scenic approach to the museum village of Stony Brook contributed financing to the Village protest. The case was settled by Gyrodyne agreeing to leave historic houses on the northwest corner of the property and to file a restrictive covenant to preserve a buffer zone of 200 feet along the North Country Road frontage in which no building could be erected.

In 1966, the US Department of Commerce described North Country Road through St. James as a relatively unchanged section of the route followed by George Washington in 1790 and commented that "it is one of the few remaining sections of that route which has not been obliterated by road widening and where the character of the adjacent countryside remains virtually unchanged."

In 1969, with the advice of the Suffolk County Planning Director the Village designated Route 25A, to a depth of 500 feet, as the "North Country Road Historic Area". In 1972, the Allstate Insurance Company attempted to build a large office building on the corner of 25A and Shep Jones Lane. In 1973, Allstate withdrew its application in face of opposition by the Village and surrounding community as covered by the local press.

Today when we sit in bumper-to-bumper traffic on the main east west corridors on Long Island, i.e., the Long Island Expressway, Jericho Turnpike and Nesconset Highway we see the impact of over building and the blight of commercial signage that resulted from not developing in a smart way. Once development is done, we cannot go back. We have to live with the mess. The portion of Route 25A that runs through our community must not fall into a similar condition.

County Development Report. The subject parcel was reviewed on August 2, 2017 by the Suffolk County Planning Commission and a Staff Report was issued that included analysis and recommendations. We generally agree with the Staff Report and note that included in their analysis is a belief by Staff that the development of the property should not approach the maximum allowable density allowed by the present zoning. The Staff Report shows concern that the development should not over burden the community visually, environmentally, and with new car and truck traffic. We note that the existing volume of traffic on North Country Road especially during rush hours can be intolerable. In addition, the curves and hills can slow the traffic in the winter weather even further. We just cannot see how the added traffic of a 150-room hotel can be absorbed. In our view, hotels belong in the commercial areas of the LIE with its four-lane capacity plus service roads. We are highly in favor of traffic studies that take into consideration the impact on all of the surrounding communities.

Scenic Buffer. The County report suggests that a "large buffer" be provided to maintain the natural and historic corridors. The County Planning Commission Staff recommended that Gyrodyne should even supplement the 200 foot wide open space/historic vista along North Country Road with indigenous plant materials to help prevent visual intrusion from the development including the lighting out an on to the road and neighboring properties. The Village certainly supports the Staff's suggestion and also asks that the height of any development be limited so as not to be visible and imposing by extending over and above the sight line of the buffer. The Village would also suggest that Gyrodyne remove the large signs

on the Rt. 25A roadside next to both the 25A entrance and at the Mills Pond intersection. These signs advertise current tenants and space for lease. The signs today detract from the scenic vista and should not be permitted in the buffer now or after development. Gyrodyne plans call for a 150-room hotel. Today if visitors to the area need a place to stay there are hotels on Route 347 and the LIE. Hotel signs on the LIE can be seen for miles along with signs for fast food restaurants. Even though the preliminary subdivision application does not include a specific hotel, we must consider the impact of a highly visible hotel with parking lot lighting, signage and staff and travelers coming and going.

Fire Protection. A hotel would require specially designed fire protection and we question whether the St. James Fire Department has this capability. St. James Fire Department's plans call for a consolidation to a newly built station in the south central area of St. James (south of the LIRR tracks) and the closure of the main fire station located down 25A from Gyrodyne. Funding for a new station is not currently available making these plans uncertain.

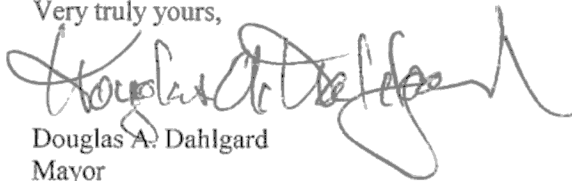
Fire protection for a multi-story hotel cannot be uncertain and is a significant matter that needs to be nailed down. We note that the Stony Brook Fire Department has an investment in expensive firefighting equipment to handle fires at the multi storied University and Medical Center. How will St. James Fire Department finance special firefighting equipment needed to fight fires at a 150-room hotel and at the same time finance a new fire station. These items should be part of the Gyrodyne subdivision discussion.

Water Quality. The Village is concerned with protecting its drinking water from sewage treatment plant discharge. Another concern is stormwater from the site polluting Stony Brook Harbor over time. The land from Gyrodyne's property runs downhill to the water, as does most of the land in Head of the Harbor and Stony Brook. This is a critical area requiring a full environmental study, which should not only deal with the near term but also cover the long-term impacts to our water quality.

Community Cooperation. Another issue deals with working with the surrounding community. Gyrodyne would build a Sewage Treatment Plant to handle the site's sewage. It has been discussed that a revitalization of downtown St. James would benefit from a sewage treatment plant connection. Running a connection from Gyrodyne's STP along the LIRR right of way to St. James sounds possible, but would require State cooperation and financial support. We certainly would be supportive of that effort.

We thank the Smithtown Planning Board for this opportunity to express our concerns with regard to the preliminary subdivision by Gyrodyne LLC, and we remain available should the Board wish to discuss this submission.

Very truly yours,



Douglas A. Dahlgard
Mayor

Cc: Hon. Board of Trustees, Village of Head of the Harbor
Mr. Harlan D. Fischer, Planning Board Chairperson, Village of Head of the Harbor
Mr. Joseph Bollhofer, Chairperson Zoning Board of Appeals, Village of Head of the Harbor
Ms. Robin Herrnstein, Chairperson Joint Coastal Commission, Village of Head the Harbor
Mr. Paul Athineos, Chairperson, Village of Head of the Harbor

COUNTY OF SUFFOLK

KARA HAHN
MAJORITY LEADER
LEGISLATOR, FIFTH DISTRICT

COMMITTEE CHAIR
ENVIRONMENT, PLANNING
& AGRICULTURE
PARKS & RECREATION



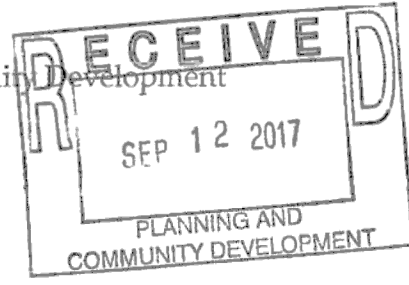
MEMBER
ECONOMIC DEVELOPMENT
EDUCATION & INFORMATION
TECHNOLOGY
GOVERNMENT OPERATIONS,
PERSONNEL & HOUSING
PUBLIC SAFETY

*I am
replying.
I will
reply by
you.
Done*

COUNTY LEGISLATURE

August 9, 2017

David Flynn, Town Planning Director
Town of Smithtown Department of Planning and Community Development
99 West Main Street
PO Box 9090
Smithtown, New York 11787



Dear Mr. Flynn,

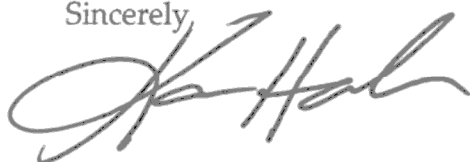
I write in strong opposition to the application submitted by Gyrodyne, LLC to subdivide nearly 75 acres of land into multiple parcels with the conceptual plan that includes the development of a 150 room hotel, two medical office buildings totaling 128,400 ft² and two long-term care buildings that will house 220 assisted living units in St. James. This concept while well intentioned for the future needs of the region's aging population is neither well planned nor well situated for the current site and should be blocked until a more appropriate location is identified.

As of today, the applicant has not submitted a traffic study of the adjacent roadways' current capacities and daily usage and what impact a project of this scale will have on the impacted thoroughfares' functionality and safety. The Gyrodyne, LLC property on which this application has been made is bounded by a historic corridor comprised of the North Country Road/New York State Route 25A Heritage Trail and Mills Pond Road. These narrow, winding two lane roads have not been engineered for the certain influx of vehicles that will emanate from the site upon its completion and, in fact, are currently at or nearing capacity from the residential, commercial and economic centers they already serve. This includes Stony Brook Road in neighboring Brookhaven Town, which bounds Gyrodyne's eastern most perimeter and is already significantly over capacity largely due to Stony Brook University related traffic. Unfortunately, this site is also not serviced by public transportation, so relying upon less impactful options like buses will not even be a possibility for mitigating the anticipated traffic woes.

While the project's potential traffic impacts alone should give planners pause, the proposal's environmental impacts are just as consequential. Current design provides no consideration of run-off from newly created impervious surfaces into adjacent wetlands, the Long Island Sound and Mill Pond in Stony Brook and Stony Brook Harbor, which are frequently the recipients of upland stormwater run-off along the Route 25A corridor. Just as concerning is proposal's site selection for an on-site sewage treatment facility within a 10 to 25 year travel time (zone) to reach the surface water of Stony Brook Harbor and ultimately the Long Island Sound which we have been fighting to protect for more than a generation.

Over the past decade we have watched as the development and redevelopment of the Gyrodyne property has led to increased traffic within neighboring residential communities and diminished qualities of life for their residents. With numerous serious concerns about the plan, at this critical point, I ask for you to rethink the planning approach that has dictated the course of Gyrodyne's future up until this point and begin to weigh this proposal's projected impact to people, the environment and community character. In that context, I urge you to reject the Gyrodyne, LLC application as submitted.

Sincerely,



Kara Hahn, Majority Leader
Suffolk County Legislator
Fifth District

AM

COUNTY OF SUFFOLK

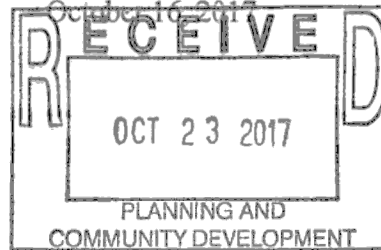


Steven Bellone
SUFFOLK COUNTY EXECUTIVE
Department of
Economic Development and Planning

Theresa Ward
Deputy County Executive and Commissioner

Division of Planning
and Environment

Town of Smithtown
Planning Board
99 West Main Street
P.O. Box 575
Smithtown, NY 11787
Attn: Allyson Murray



Subdivision: "Map of Gyrodyne LLC"
Municipal File No.: #1178
Suffolk County Tax Map No.: 0800 40000 0200 004000 et al
Suffolk County Planning File No.: S-SM-17-01.1

Dear Ms. Murray:

The staff has examined the proposed subdivision map entitled "Gyrodyne, LLC" received on October 4, 2017. Comparing this map with the one that was previously referred to the Commission, the staff did not find any substantial difference between the two maps.

It is the policy of the Commission not to review any map that it has previously considered unless there are substantial differences. Staff noted that the revised map was more in accordance with some of the Commission's prior comments.

A copy of the previous resolution is enclosed for your reference.

Sincerely,
Sarah Lansdale, AICP
Director of Planning

Theodore R. Klein
Principal Planner

TRK/cd

ZSR-17-19

Resolution No. ZSR-17-19 of the Suffolk County Planning Commission
Pursuant to Sections A14-14 to thru A14-25 of the Suffolk County Administrative Code

WHEREAS, pursuant to Sections A14-14 thru A14-25 of the Suffolk County Administrative Code, a Subdivision referral was received on June 30, 2017 at the offices of the Suffolk County Planning Commission with respect to the application of "Gyrodyne, LLC" located in the Town of Smithtown, and

WHEREAS, said referral was considered by the Suffolk County Planning Commission at its meeting on August 2, 2017, now therefore, Be it

RESOLVED, that the Suffolk County Planning Commission hereby approves the staff report, as may be amended, as the report of the Commission, Be it further

RESOLVED, pursuant to Section A14-16 of the Suffolk County Administrative Code and Section 239-m 6 of the General Municipal Law, the referring municipality within thirty (30) days after final action, shall file a report with the Suffolk County Planning Commission, and if said action is contrary to this recommendation, set forth the reasons for such contrary action, Be it further

RESOLVED, that the Suffolk County Planning Commission **Approves** the subdivision application subject to the following comments:

1. The proposed 50' wide access easements should be replaced with a 60' wide right of way, as is required by Town regulation for subdivisions of land in the LI zoning district, but it is recommended that the pavement width be kept to a minimum considered practical for safe vehicular travel within the right-of-way. This type design would allow for drainage swales and sidewalks to the side of the pavement. This design would both reduce and manage stormwater runoff, and also provide an optimal separation of vehicular and pedestrian movement on the subject property.
2. The existing access easement that connects across the Long Island Rail Road track right-of-way to the east should also be a separate right-of-way and built to the same specifications of the other roads in the subdivision.
3. The applicant shall be encouraged to review the Suffolk County Planning Commission publication on Managing Stormwater-Natural Vegetation and Green Methodologies and incorporate into the proposal, where practical, design elements contained therein.
4. Allowing for the creation of a new ingress or egress along North Country Road (State Route 25A) should only be considered after it has been determined that access along Mills Pond Road, existing point of access along SR 25A, and Stony Brook Road to the east cannot adequately accommodate vehicular trips generated by proposed uses on the property. This could involve a traffic impact study as well as the monitoring of traffic generated (over time) as the proposed lots are developed and occupied.
5. The Town and the applicant should communicate with the New York State Department of Transportation to discuss possible signalization at the intersection of Mills Pond Road, as well as the necessary and appropriate level improvements of the subject property's existing point of access at its most easterly frontage along North Country Road (SR 25A), to safely accommodated traffic generated from the proposed subdivision's future development.

6. Consideration should be given to requiring the applicant to supplement the 200' wide open space/historic vista along North Country Road (SR 25A) with indigenous plant materials to help prevent visual intrusion, including lighting, from the future development of the subject property out on to the road and neighboring properties.
 7. The SEQRA process should be completed and any approval should be in accordance with those findings, conditions and recommendations as a result.
 8. The proposed location of a sewage treatment plant should be examined closely and its final placement should be determined in accordance with the SEQRA findings, the mapping of Suffolk County's "Subwatersheds Wastewater Plan of Groundwater Contributing Areas", and recommendations of a qualified hydrologist with knowledge of the subject area.
 9. The applicant should communicate with the Suffolk County DPW Transit Division and discuss possible accommodations for bus riders from the subject development.
 10. The applicant should be encouraged to investigate the viability of offering a 'bike share' program as a way of reducing short distance motor vehicular travel and connecting to nearby surrounding areas such as St. James' downtown and Stony Brook University (which already has an existing 'bike share' program).
 11. The applicant should be encouraged to review the Suffolk County Planning Commission Guidebook particularly with respect to energy efficiency and shall incorporate where practical, applicable elements contained therein.
 12. The applicant should review the Planning Commission guidelines particularly related to public safety and incorporate into the proposal, where practical, design elements contained therein.
 13. The applicant should review the Planning Commission guidelines particularly related to universal design and incorporate into the proposal, where practical, design elements contained therein.
- The Suffolk County Planning Commission Guidebook for policies and guidelines can be found on the internet at the below website address:
<http://www.suffolkcountyny.gov/Portals/0/planning/Publications/SCPCguidebk12r.pdf>



Steven Bellone
SUFFOLK COUNTY EXECUTIVE
Department of
Economic Development and Planning

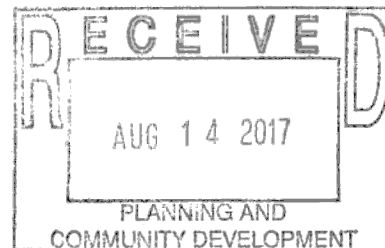
Theresa Ward
Deputy County Executive and Commissioner

Division of Planning
and Environment

August 7, 2017

Town of Smithtown
Planning Board
99 West Main Street
P.O. Box 575
Smithtown, NY 11787
Attn: Agnes J. Vion

Re: Gyrodyne, LLC
Local File No.: 1178
S.C.P.C. File No.: ZSR-17-19



Dear Ms. Vion:

The Suffolk Planning Commission at its regular meeting on August 2, 2017, reviewed the referral from the Town of Islip entitled, "Gyrodyne, LLC" referred to it pursuant to Section A14-14 thru A14-25, Article XIV of the Suffolk County Administrative Code.

The attached Resolution signifies action taken by the Commission relative to this application.

Very Truly Yours,

Sarah Lansdale
Director of Planning

Theodore R. Klein
Principal Planner

TRK/cd

ZSR-17-19

Resolution No. ZSR-17-19 of the Suffolk County Planning Commission
Pursuant to Sections A14-14 to thru A14-25 of the Suffolk County Administrative Code

- WHEREAS, pursuant to Sections A14-14 thru A14-25 of the Suffolk County Administrative Code, a Subdivision referral was received on June 30, 2017 at the offices of the Suffolk County Planning Commission with respect to the application of "**Gyrodyne, LLC**" located in the Town of Smithtown, and
- WHEREAS, said referral was considered by the Suffolk County Planning Commission at its meeting on August 2, 2017, now therefore, Be it
- RESOLVED, that the Suffolk County Planning Commission hereby approves the staff report, as may be amended, as the report of the Commission, Be it further
- RESOLVED, pursuant to Section A14-16 of the Suffolk County Administrative Code and Section 239-m 6 of the General Municipal Law, the referring municipality within thirty (30) days after final action, shall file a report with the Suffolk County Planning Commission, and if said action is contrary to this recommendation, set forth the reasons for such contrary action, Be it further
- RESOLVED, that the Suffolk County Planning Commission **Approves** the subdivision application subject to the following comments:
1. The proposed 50' wide access easements should be replaced with a 60' wide right of way, as is required by Town regulation for subdivisions of land in the LI zoning district, but it is recommended that the pavement width be kept to a minimum considered practical for safe vehicular travel within the right-of-way. This type design would allow for drainage swales and sidewalks to the side of the pavement. This design would both reduce and manage stormwater runoff, and also provide an optimal separation of vehicular and pedestrian movement on the subject property.
 2. The existing access easement that connects across the Long Island Rail Road track right-of-way to the east should also be a separate right-of-way and built to the same specifications of the other roads in the subdivision.
 3. The applicant shall be encouraged to review the Suffolk County Planning Commission publication on Managing Stormwater-Natural Vegetation and Green Methodologies and incorporate into the proposal, where practical, design elements contained therein.
 4. Allowing for the creation of a new ingress or egress along North Country Road (State Route 25A) should only be considered after it has been determined that access along Mills Pond Road, existing point of access along SR 25A, and Stony Brook Road to the east cannot adequately accommodate vehicular trips generated by proposed uses on the property. This could involve a traffic impact study as well as the monitoring of traffic generated (over time) as the proposed lots are developed and occupied.
 5. The Town and the applicant should communicate with the New York State Department of Transportation to discuss possible signalization at the intersection of Mills Pond Road, as well as the necessary and appropriate level improvements of the subject property's existing point of access at its most easterly frontage along North Country Road (SR 25A), to safely accommodated traffic generated from the proposed subdivision's future development.

ZSR-17-19

File No.: S-SM-17-01

Gyrodyne, LLC, Town of Smithtown

8/2/2017

COMMISSION ACTIONS ON ADOPTION OF RESOLUTION

	AYE	NAY	ABSTAIN	ABSENT
ANDERSON, RODNEY – At Large	X			
CASEY, JENNIFER - Town of Huntington	X			
CHARTRAND, MATTHEW - Town of Islip	X			
CHU, SAMUEL – Town of Babylon	X			
CONDZELLA, JOHN – Town of Riverhead	X			
ESPOSITO, ADRIENNE - Villages over 5,000	X			
FINN, JOHN - Town of Smithtown				X
GERSHOWITZ, KEVIN G.- At Large	X			
KAUFMAN, MICHAEL - Villages under 5,000	X			
KELLY, MICHAEL – Town of Brookhaven	X			
KITT, ERROL – At Large	X			
KRAMER, SAMUEL – Town of East Hampton				X
MOREHEAD, NICHOLAS – Town of Shelter Island				X

Motion: Commissioner Kaufman

Present: 10

Seconded: Commissioner Anderson

Absent: 3

Voted: 10

Abstentions: 0

DECISION: Approved



Steven Bellone
SUFFOLK COUNTY EXECUTIVE
Department of
Economic Development and Planning

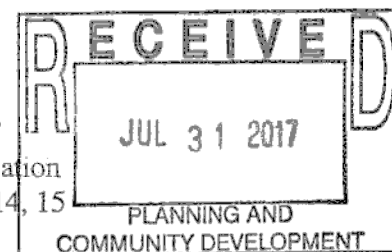
Theresa Ward
Deputy County Executive and Commissioner

Division of Planning
and Environment

July 26, 2017

Town of Smithtown
Planning and Community Development Department
99 West Main Street
P.O. Box 575
Smithtown, New York 11787
Attn: Agnes J. Vion, Clerk to the Planning Board

Re: "Map of Gyrodyne, LLC"
Land Use Action: Major Subdivision Application
S.C.T.M. No.: 0800-40-2-4, 13.3, 13.4, 14, 15
S.C.P.C. File No.: S-SM-17-01



Dear Mr. Vion:

Please be advised that pursuant to Sections A 14-14 to 25 of the Suffolk County Administrative Code, the above captioned application which has been referred to the Suffolk County Planning Commission will be reviewed by the Commission at its regular meeting on **Wednesday, August 2, 2017, at the Rose Caracappa Auditorium, W.H. Rogers Legislative Building, 725 Veterans Memorial Highway, Smithtown, NY.**

The regular meetings of the Suffolk County Planning Commission are open meetings pursuant to the NYS Open Meetings Law; they are not public hearings. Any request to address the Planning Commission on any matter, including subdivision or zoning referrals, must be submitted prior to the meeting. Each request shall be submitted on a card (distributed before the regular meeting) identifying the person and/or organization and topic. During the public portion of the regular meeting, each speaker shall be allotted three (3) minutes. An individual who has requested the opportunity to address the Planning Commission may relinquish his/her time to another speaker. However, no speaker may speak for more than six (6) minutes.

In accordance with Suffolk County Planning Commission Policy, any information to be submitted to the Planning Commission will not be accepted at the meeting unless it has been acknowledged by the local referring municipality. The information must be accompanied with a letter from the local referring municipality, describing such contents, as well as instructions for treating previously submitted material.

This policy was instituted to avoid confusion as to which information is being reviewed by all agencies.

Should you have any questions or comments, please do not hesitate to contact this office.

Sincerely,

A handwritten signature in black ink, appearing to read "Theodore R. Klein" with a stylized flourish at the end.

Theodore R. Klein
Principal Planner

TK/jc

cc: Gyrodyne LLC, c/o Peter Pitsiokos, COO

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*Final Environmental Impact Statement
Map of Flowerfield Subdivision Application*

December 2020

**Appendix F: Final Engineering Plans
(Cover sheet through Sheet C-8)**

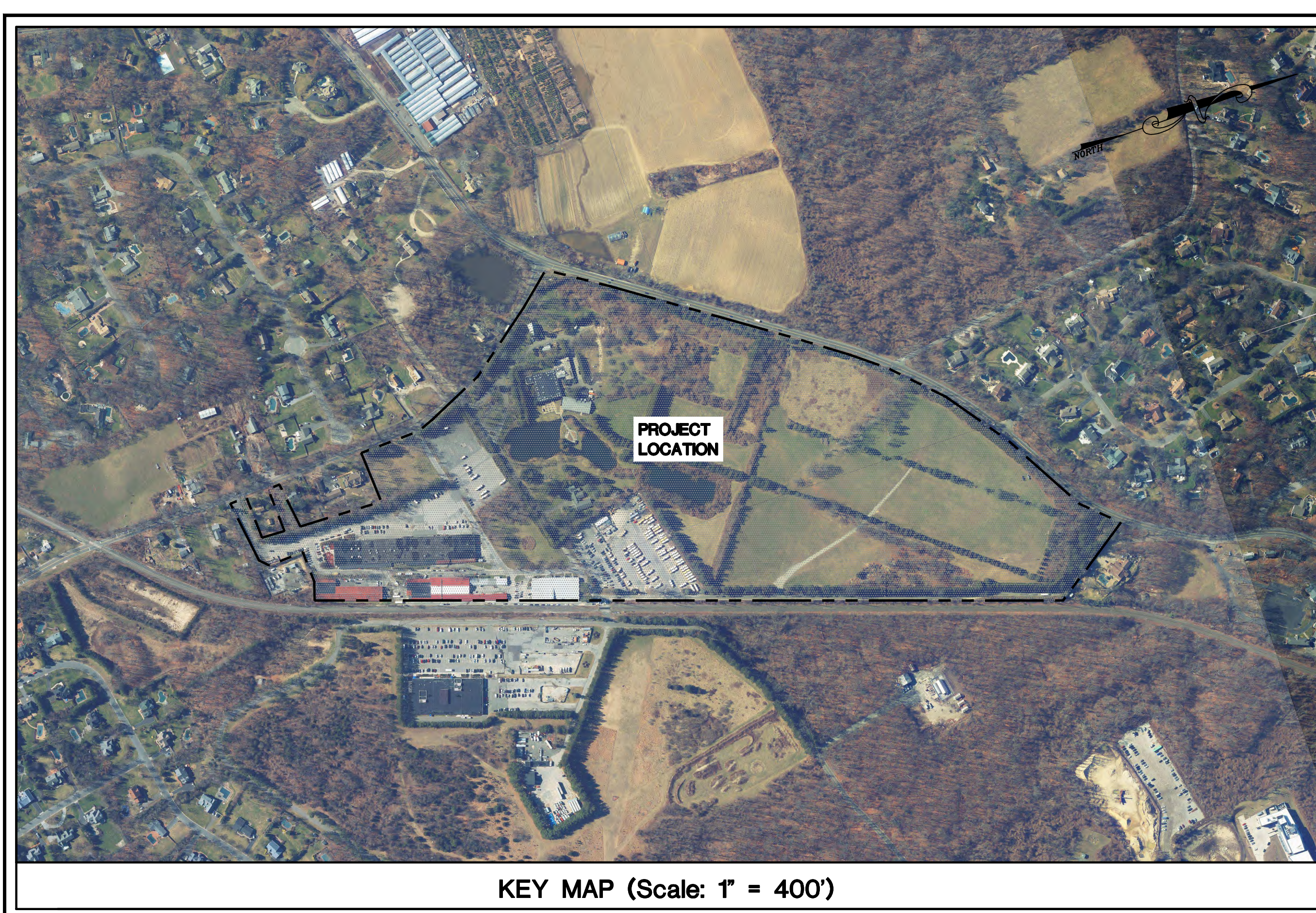
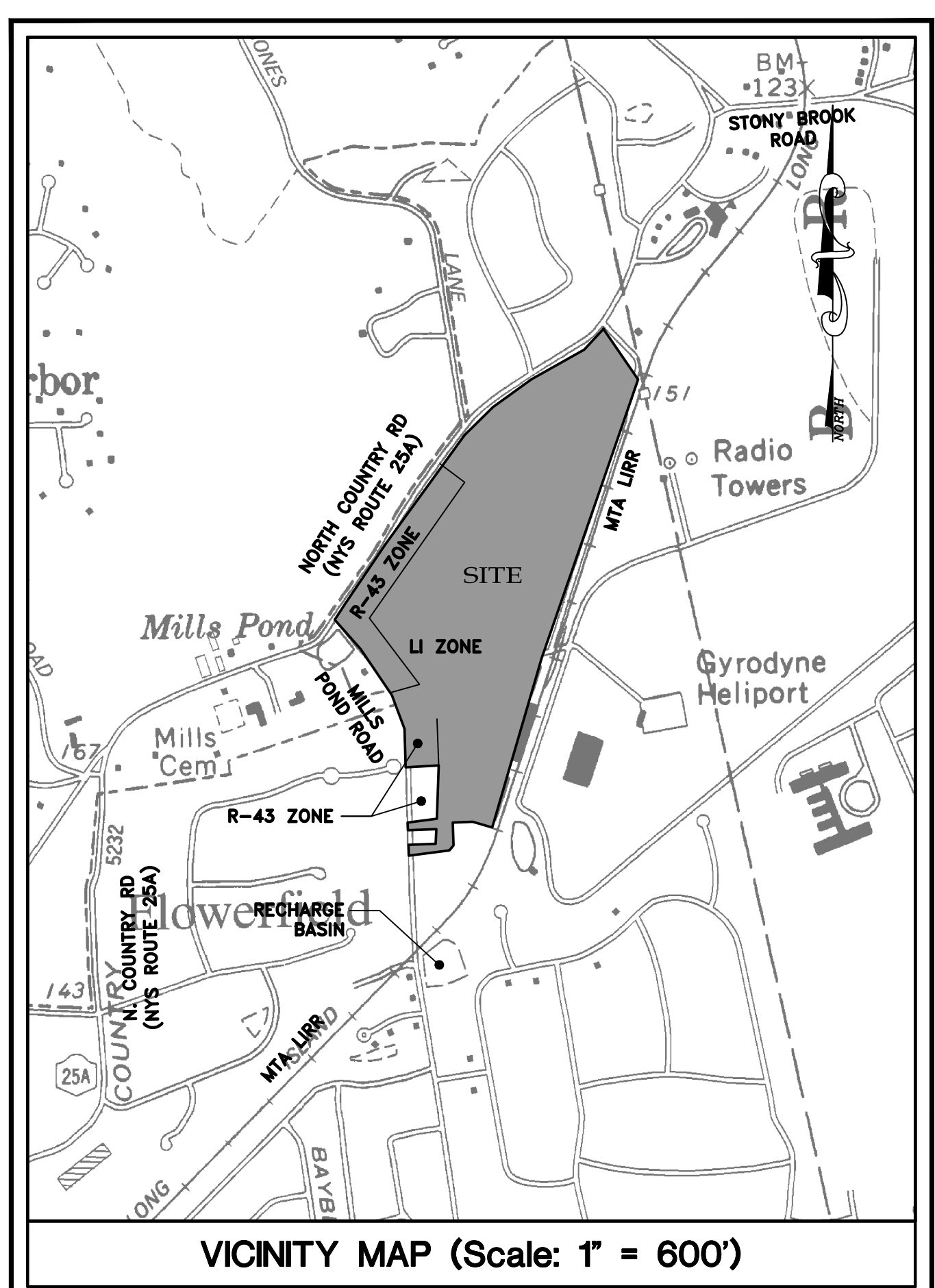
- Cover
- C-0 Overall Streets Plan
- C-1 Existing Conditions and Removals Plan (1)
- C-2 Existing Conditions and Removals Plan (2)
- C-3 Materials and Alignment Plan (1)
- C-4 Materials and Alignment Plan (2)
- C-5 Grading and Drainage Plan (1)
- C-6 Grading and Drainage Plan (2)
- C-7 Grading and Drainage Plan (3)
- C-8 Utility Plan (1)

This Appendix continues in Volume 3 of 4 (Sheets C-9 through C-21)

MAP OF FLOWERFIELD

1 FLOWERFIELD DRIVE
ST. JAMES, NY 11780

Final Engineering Plans



LIST OF DRAWINGS

DRAWING No.	TITLE
--	COVER
C-0	OVERALL STREETS PLAN
C-1	EXISTING CONDITIONS AND REMOVALS (1)
C-2	EXISTING CONDITIONS AND REMOVALS PLAN (2)
C-3	MATERIALS AND ALIGNMENT PLAN (1)
C-4	MATERIALS AND ALIGNMENT PLAN (2)
C-5	GRADING AND DRAINAGE PLAN (1)
C-6	GRADING AND DRAINAGE PLAN (2)
C-7	GRADING AND DRAINAGE PLAN (3)
C-8	UTILITY PLAN (1)
C-9	UTILITY PLAN (2)
C-10	EROSION AND SEDIMENT CONTROL PLAN (1)
C-11	EROSION AND SEDIMENT CONTROL PLAN (2)
C-12	ROAD PLAN & PROFILE ROAD A - STA 10+00.00 TO STA 24+09.00
C-13	ROAD PLAN & PROFILE ROAD A - STA 24+09.00 TO STA 35+78.03
C-14	SANITARY SEWER PROFILES (1)
C-15	SANITARY SEWER PROFILES (2)
C-16	MILLS POND DRIVEWAY PAVEMENT MARKING PLAN
C-17	TREE PRESERVATION AND LAND CLEARING PLAN (1)
C-18	TREE PRESERVATION AND LAND CLEARING PLAN (2)
C-19	LANDSCAPE PLAN
C-20	DETAILS
C-21	DETAILS (2)

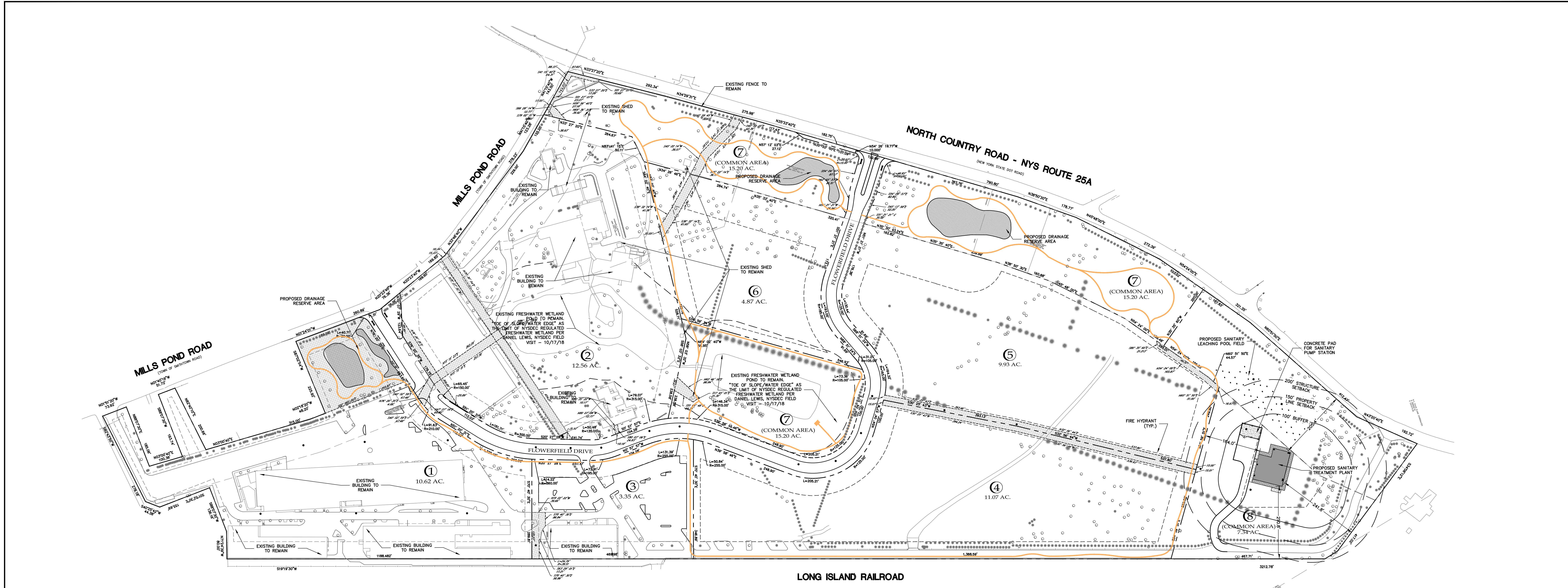


PREPARED BY:
CAMERON ENGINEERING
& ASSOCIATES, LLP

177 Crossways Park Drive, Woodbury, NY 11797
1411 Broadway, Suite 601, New York, NY 10019
303 Farroven Road, 1st Floor, White Plains, NY 10603
Corporate Seal Initiated 1996 State of New York
www.CameronEngineering.com

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T: (212) 324-4000
T: (914) 721-8300
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APRIL 2020
REV. 1 - 8/19/2020



TOWN OF SMITHTOWN SUBDIVISION GENERAL NOTES

1. A TOWN OF SMITHTOWN ENGINEERING INSPECTOR SHALL BE CONTACTED AT (813) 360-7550 TO ARRANGE FOR A PRE-CONSTRUCTION MEETING TO BE HELD TWO (2) WEEKS PRIOR TO THE START OF CONSTRUCTION. ANY WORK PERFORMED PRIOR TO THIS MEETING SHALL BE DEEMED UNACCEPTABLE AND REMOVED AS DIRECTED BY THE TOWN ENGINEER.
2. A TOWN OF SMITHTOWN ENGINEERING INSPECTOR SHALL BE CONTACTED SEVENTY-TWO (72) HOURS PRIOR TO THE START OF CONSTRUCTION TO ARRANGE FOR INSPECTION OF ITEMS SUCH AS CLEARING, GRADING, DRAINAGE, PAVEMENT (INCLUDING THE PREPARATION OF THE SUBBASE AND SUBGRADE), CURBING, AND RETAINING WALLS.
3. UNLESS OTHERWISE DIRECTED BY THE TOWN ENGINEER, NO EXCAVATION SHALL BE MADE BELOW THE LEVEL NECESSARY TO ACHIEVE THE PROPOSED FINISHED GRADES INDICATED ON THE PLANS APPROVED BY THE PLANNING BOARD. AREAS THAT REQUIRE FILL SHALL HAVE THE TOPSOIL STRIPPED, COLLECTED AND STORED FOR REUSE TO THE EXTENT POSSIBLE AND ALL OTHER UNSUITABLE SOILS SHALL BE REMOVED AND DISPOSED OF PROPERLY PRIOR TO FILLING. ALL FILL MATERIAL SHALL BE APPROVED BY A TOWN OF SMITHTOWN ENGINEERING INSPECTOR PRIOR TO DELIVERY. AFTER COMPLETION OF GENERAL REGRADING OF THE SITE, ALL LANDSCAPED AREAS SHALL BE FINISH GRADED WITH A SURFACE LAYER OF SIX (6) INCHES OF TOPSOIL OF A QUALITY NATIVE TO THE SITE.
4. AN AS-CONSTRUCTED PUBLIC IMPROVEMENTS SURVEY PREPARED BY A LAND SURVEYOR IS REQUIRED TO BE SUBMITTED TO AND APPROVED BY THE TOWN OF SMITHTOWN ENGINEERING DEPARTMENT PRIOR TO THE RELEASE OF THE PERFORMANCE BOND. THE SURVEY SHALL PROVIDE AS-CONSTRUCTED INFORMATION OF ALL PUBLIC IMPROVEMENTS INSTALLED AS PART OF THE SUBDIVISION.
5. ANY SUBDIVISION LOTS CONTAINING STORMWATER MANAGEMENT THAT IS ESSENTIAL TO THE OVERALL DRAINAGE COLLECTION SYSTEM (E.G., ROOF DRYWELLS, AREA STORM POOLS, ETC.) OF THE DEVELOPMENT SHALL HAVE THEIR DEED OF RECORD MODIFIED TO INCLUDE A DEED COVENANT THAT REQUIRES CONTINUED OPERATION AND MAINTENANCE OF THE STORMWATER MANAGEMENT SYSTEM.
6. EACH DWELLING OF A SUBDIVISION SHALL PROVIDE AN ADEQUATE DRAINAGE COLLECTION SYSTEM FOR STORMWATER ROOF RUNOFF. THE SYSTEM(S) SHALL BE DESIGNED TO CONTAIN A TOTAL RAINFALL OF THREE (3) INCHES. ALL OUTLETS AND LEADERS FOR EACH DWELLING SHALL BE INTERCONNECTED TO THE SYSTEM VIA AN UNDERGROUND PIPING SYSTEM UNLESS OTHERWISE DIRECTED BY THE TOWN ENGINEER.
7. EACH SUBDIVISION LOT SHALL PROVIDE DRIVEWAY ACCESS FROM THE STREET TO THE GARAGE, CARPORT OR PARKING AREA AT A SLOPE NOT TO EXCEED EIGHT-POINT-SEVEN (8.7) PERCENT AND A MINIMUM SLOPE OF ONE-PERCENT (1%) IN GRADE. ANY FINAL DRIVEWAY CONSTRUCTION EXCEEDING EIGHT-POINT-SEVEN (8.7) PERCENT WILL REQUIRE A VARIANCE FROM THE BOARD OF ZONING APPEALS. DRIVEWAY BACK-PITCHED TOWARDS THE DWELLING MUST BE PROVIDED WITH SUFFICIENT DRAINAGE STRUCTURE CAPACITY AS REQUIRED AND, WHERE POSSIBLE, PROVIDED WITH A SUITABLE MEANS OF OVERFLOW.
8. ALL DRAINAGE LEACHING STRUCTURES SHALL BE INSTALLED INTO A WELL-DRAINING SOIL STRATUM. IF POOR-DRAINING SOIL CONDITIONS EXIST, THE CONTRACTOR SHALL EXCAVATE DOWN TO WELL-DRAINING SOIL, BACKFILL UP TO AND AROUND THE LEACHING STRUCTURE WITH A CLEAN SAND/GRAVEL COLLAR NO LESS THAN THREE (3) FEET WIDE, AND PROVIDE FILTER FABRIC AROUND THE STRUCTURE. ALL LEACHING STRUCTURES MUST BE INSPECTED BY A TOWN OF SMITHTOWN ENGINEERING INSPECTOR PRIOR TO BACKFILL.
9. ALL ROADS TO BE DEDICATED TO THE TOWN OF SMITHTOWN MUST CONFORM TO THE STREET IMPROVEMENT REQUIREMENTS OF CHAPTER 24B, ARTICLE XII, SECTIONS 1-10. ALL ROADS TO REMAIN UNDER PRIVATE OWNERSHIP AFTER CONSTRUCTION THAT DO NOT CONFORM TO THE STREET IMPROVEMENT REQUIREMENTS OF THE TOWN CODE WILL NOT BE ELIGIBLE FOR FUTURE DEDICATION.
10. PRIOR TO THE COMMENCEMENT OF CONSTRUCTION ACTIVITY, EACH CONTRACTOR(S) AND SUBCONTRACTOR(S) THAT IS RESPONSIBLE FOR IMPLEMENTING THE STORMWATER POLLUTION PREVENTION PLAN SHALL IDENTIFY AT LEAST ONE INDIVIDUAL THAT HAS RECEIVED FOUR (4) HOURS OF NYSDC ENDORSED TRAINING IN PROPER EROSION AND SEDIMENT CONTROL PRINCIPLES. THE CONTRACTOR(S) AND SUBCONTRACTOR(S) WILL BE RESPONSIBLE FOR PROVIDING A CERTIFICATE OF COMPLETION ON OTHER SUCH PROOF THAT INDIVIDUAL TO THE TOWN OF SMITHTOWN ENGINEERING DEPARTMENT THAT TRAINING HAS BEEN RECEIVED AND IS CURRENT. THE OWNER/OPERATOR OF THE PROPERTY SHALL ENSURE THAT AT LEAST ONE TRAINED INDIVIDUAL IS ON SITE ON A DAILY BASIS WHEN SOIL DISTURBANCE ACTIVITIES ARE BEING PERFORMED.

GENERAL NOTES

1. A PRE-CONSTRUCTION MEETING WITH THE ENGINEERING DEPARTMENT SHALL BE HELD TWO WEEKS (2) PRIOR TO THE START OF CONSTRUCTION. ANY WORK PERFORMED PRIOR TO THIS MEETING SHALL BE DEEMED UNACCEPTABLE AND REMOVED AS DIRECTED BY THE TOWN ENGINEER.
2. THE ENGINEERING DEPARTMENT SHALL BE NOTIFIED 72 HOURS PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL ARRANGE FOR THE INSPECTION OF ALL ITEMS RELATING TO GRADING, DRAINAGE, PAVING AND CURBING.
3. THE CONTRACTOR SHALL NOT START ANY WORK UNTIL A BOARD OF SITE PLAN REVIEW APPROVAL IS OBTAINED. A SIGNED SITE PLAN IS OBTAINED AND A PRE-CONSTRUCTION MEETING IS HELD WITH THE ENGINEERING DEPARTMENT.
4. ALL EXISTING CATCH BASINS AND STORM WATER DRAINAGE POOLS TO REMAIN MUST BE CLEANED.
5. A REPRODUCIBLE AS BUILT SITE PLAN MUST BE SUBMITTED TO THE ENGINEERING DEPARTMENT SHOWING ALL FINISHED GRADES, TOPS AND BOTTOMS OF CURBS, PIPE THREES, INVERTS, RM ELEVATIONS, TOP AND BOTTOM OF RETAINING WALLS AND DIAMETER AND DEPTHS OF STORM WATER STORAGE POOLS.
6. PROPERTY MONUMENTS SHALL BE INSTALLED AT LOCATIONS WHERE PROPERTY ABUTS MUNICIPAL RIGHT-OF-WAY LINES AND A MONUMENT CERTIFICATION LETTER FROM A LICENSED LAND SURVEYOR MUST BE SUBMITTED TO THE ENGINEERING DEPARTMENT.
7. ANY BROKEN OR HAZARDOUS EXISTING SIDEWALK, DRIVEWAY APRONS AND CURBING ADJACENT TO AND ON THE SUBJECT SITE SHALL BE REPLACED IN ACCORDANCE WITH CURRENT STANDARDS.
8. ALL WORK, MATERIALS, SPECIFICATIONS, REGULATIONS, RULES, PERMITS, ETC. OF ALL STATE AND TOWN GOVERNMENTS AND ALL UTILITY AGENCIES SHALL BE FOLLOWED BY THE CONTRACTOR.
9. GENERAL CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE EXACT LOCATION AND DEPTH OF UTILITIES, PIPING, DRYWELLS, ETC. PRIOR TO THE START OF WORK. THE GENERAL CONTRACTOR MUST CONTACT THE "ONE CALL" SYSTEM TO ENSURE THAT ALL UTILITIES ARE PROPERLY AND COMPLETELY MARKED OUT IN THE FIELD PRIOR TO ANY WORK AT THE SITE. ANY DISCREPANCIES BETWEEN THE FIELD LOCATIONS AND THE PLANS SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY. THE GENERAL CONTRACTOR IS ADVISED THAT UTILITY INFORMATION SHOWN ON THIS PLAN IS A COMPILATION OF FIELD LOCATIONS AND RECORD DRAWINGS AND MUST BE CONFIRMED ACCORDINGLY. THE CONTRACTOR IS ALSO ADVISED THAT ALL SUCH UTILITIES DISTURBED DURING CONSTRUCTION MUST BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.
10. ALL EXCAVATION SHALL BE PROPERLY BACKFILLED WITH CLEAN EARTH AND COMPACTED TO 95% PROCTOR. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPACTION TESTING AND SHALL SUBMIT SUCH REPORTS AND VERIFICATION TO ENGINEER OF RECORD.
11. ALL UNSUITABLE MATERIAL, CONSTRUCTION DEBRIS, ETC. SHALL BE PROPERLY REMOVED AND DISPOSED OF OFF-SITE IN ACCORDANCE WITH ALL APPLICABLE CODES, ORDINANCES AND LAWS.
12. GENERAL CONTRACTOR IS TO EXERCISE EXTREME CARE WHEN PERFORMING ANY WORK ACTIVITIES ADJACENT TO EXISTING STRUCTURES TO REMAIN. CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING THE APPROPRIATE MEASURES AS NECESSARY TO ENSURE THE STRUCTURAL STABILITY OF FOUNDATIONS, WALLS/ FOOTINGS, SIDEWALKS, ETC. TO REMAIN. THE CONTRACTOR SHALL REPAIR AND/OR REPLACE ANY DISTURBED EXISTING FEATURES TO A CONDITION EQUAL TO OR BETTER THAN CURRENTLY EXISTED.
13. THIS FACILITY SHALL BE CONSTRUCTED IN ACCORDANCE WITH PLANS APPROVED BY THE TOWN OF SMITHTOWN. A COPY OF THE APPROVED PLANS MUST BE KEPT AT THE CONSTRUCTION SITE AT ALL TIMES.
14. GENERAL CONTRACTOR TO ENSURE ADA COMPLIANT GRADES AT ADA ACCESSIBLE ROUTES. SEE GRADING PLAN FOR DETAILS.
15. THE CONTRACTOR SHALL PROVIDE ALL BARRICADES, SIGNS, LIGHTS, ETC., FOR MAINTENANCE AND PROTECTION OF TRAFFIC FOR THE DURATION OF THIS CONTRACT, ALL IN ACCORDANCE WITH THE LATEST NYSDOT MANUAL OF TRAFFIC CONTROL DEVICES.
16. THE CONTRACTOR SHALL COMPLY WITH ALL RULES, REGULATIONS AND DIRECTIVES OF THE POLICE DEPARTMENT.
17. THE CONTRACTOR SHALL COMPLY WITH THE LATEST OSHA CONTAINED SPACE ENTRY REQUIREMENTS (29 CFR PART 1910) & SHEETING REQUIREMENTS (29 CFR PART 1926.650, 651, 652).
18. ALL HANDICAP RAMPS, PARKING SPACES AND ACCESS ISLES SHALL BE IN COMPLIANCE WITH THE LATEST BUILDING CODE OF NEW YORK STATE AND IBC/ANSI A117.1-2003 STANDARDS.
19. ALL ELEVATIONS SHOWN ARE IN NAVD83 DATUM.

SITE DATA:

APPLICANT / OWNER INFORMATION:
 OYDOWNE LLC
 ONE FLOWERFIELD-SUITE 24
 SAINT JAMES, NY 11780

ZONING CLASSIFICATION:
 U-1 DISTRICT

MIN. LOT AREA 80,000 SF
 MIN. FRONT YARD 50 FEET
 MIN. REAR YARD 50 FEET
 MIN. SIDE YARD 20 FEET
 MIN. BUFFER TO RESIDENTIAL DISTRICT 100 FEET

S-4-3 DISTRICT
 MIN. LOT AREA 43,560 SF
 MIN. LOT FRONTAGE AT SETBACK 150 FEET
 MIN. FRONT YARD 40 FEET
 MIN. REAR YARD 60 FEET
 MIN. SIDE YARD 100 FEET
 MIN. SIDE YARD WIDTH ANY ONE 24 FEET, BOTH 60 FEET

AREA OF SITE:
 74.98 AC

65.41 AC IN ZONING DISTRICT U-1
 9.57 AC IN ZONING DISTRICT S-4-3

TOTAL NUMBER OF PROPOSED LOTS: 8
 TAX LOT 13.4, TAX LOT 14, & TAX LOT 15

SCM:
 DISTRICT 0800, SECTION 40, BLOCK 2, TAX LOT 4, TAX LOT 13.3,
 TAX LOT 13.4, TAX LOT 14, & TAX LOT 15

DISTRICTS:
 SCHOOL DISTRICT - SMITHTOWN CENTRAL SCHOOL DISTRICT
 WATER DISTRICT - ST. JAMES WATER DISTRICT
 FIRE DISTRICT - ST. JAMES FIRE DISTRICT
 POLICE DISTRICT - SUFFOLK COUNTY POLICE DEPARTMENT
 POST OFFICE - ST. JAMES

OUT/FILL:
 CUT VOLUME: 37,897 CY
 FILL VOLUME: 773 CY
 NET TOTAL: 37,124 CY (CUT)

TOTAL AREA OF DISTURBANCE: #693,239 SF (#15.91 AC)
 EXISTING IMPERVIOUS WITHIN AREA OF DISTURBANCE: #203,792 SF (4.68 AC)
 PROPOSED IMPERVIOUS WITHIN AREA OF DISTURBANCE: #184,978 SF (4.25 AC)

LEGEND (PROPOSED)

- PROPERTY LINE
- SUBDIVISION LOT LINE
- NATURE TRAIL
- CURB
- LOT NUMBER AND AREA
- PROPOSED SANITARY MANHOLE
- PROPOSED SANITARY LEACHING POOL
- PROPOSED EASEMENT AREAS

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PROJECT NAME:
MAP OF FLOWERFIELD FINAL ENGINEERING

PROJECT FOR:
**1 FLOWERFIELD DRIVE
 ST. JAMES, NY 11780**

TITLE:
OVERALL STREETS PLAN

DISCIPLINE:
CIVIL

PROJECT ENGINEER:
 OC
 CE0338A

DESIGNED BY:
 OC
 CE0338A

DRAWN BY:
 OC

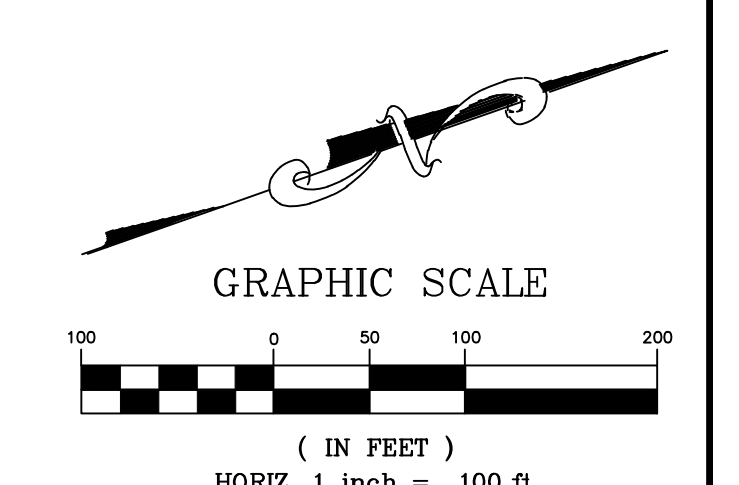
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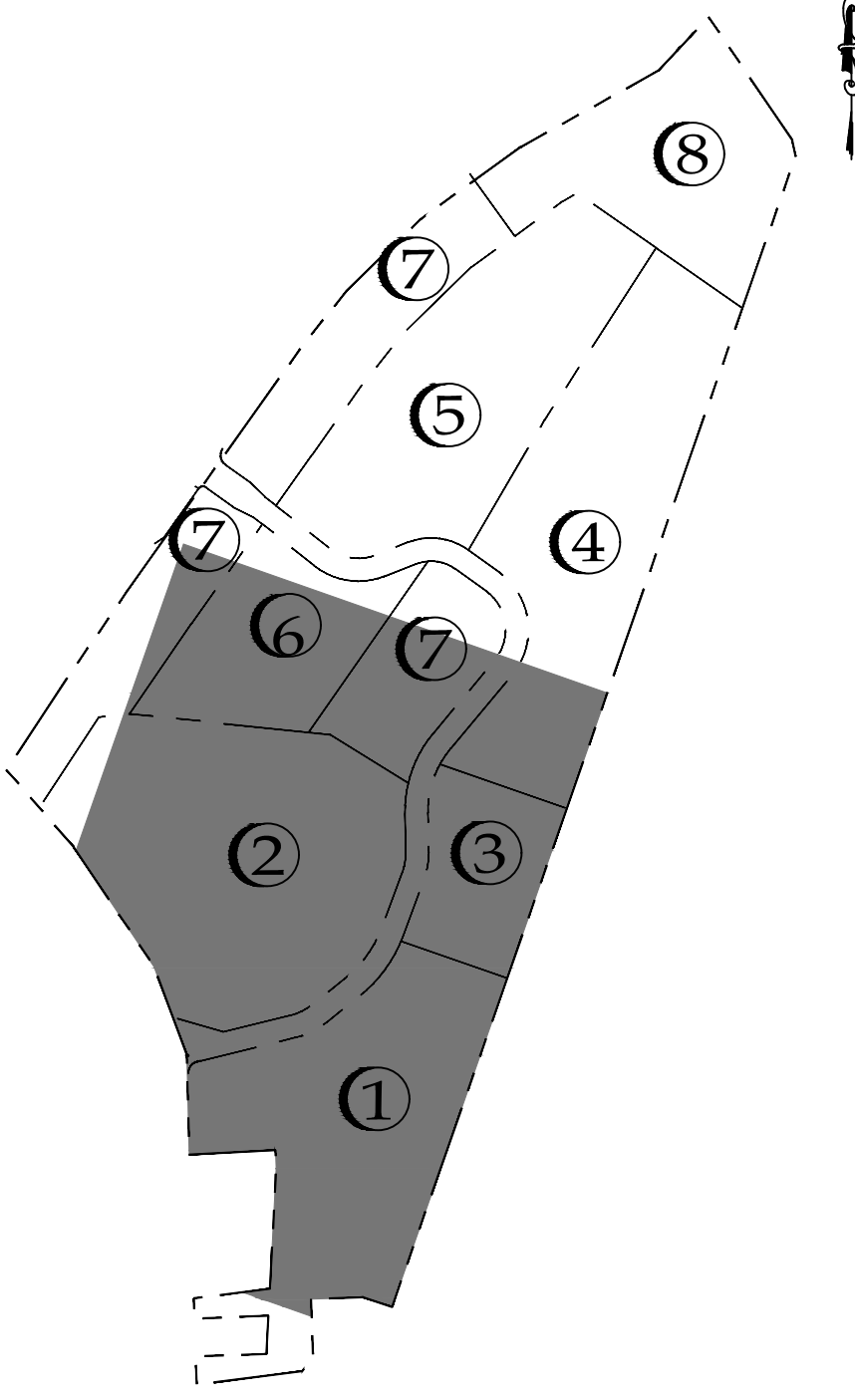
DATE:
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SCALE:
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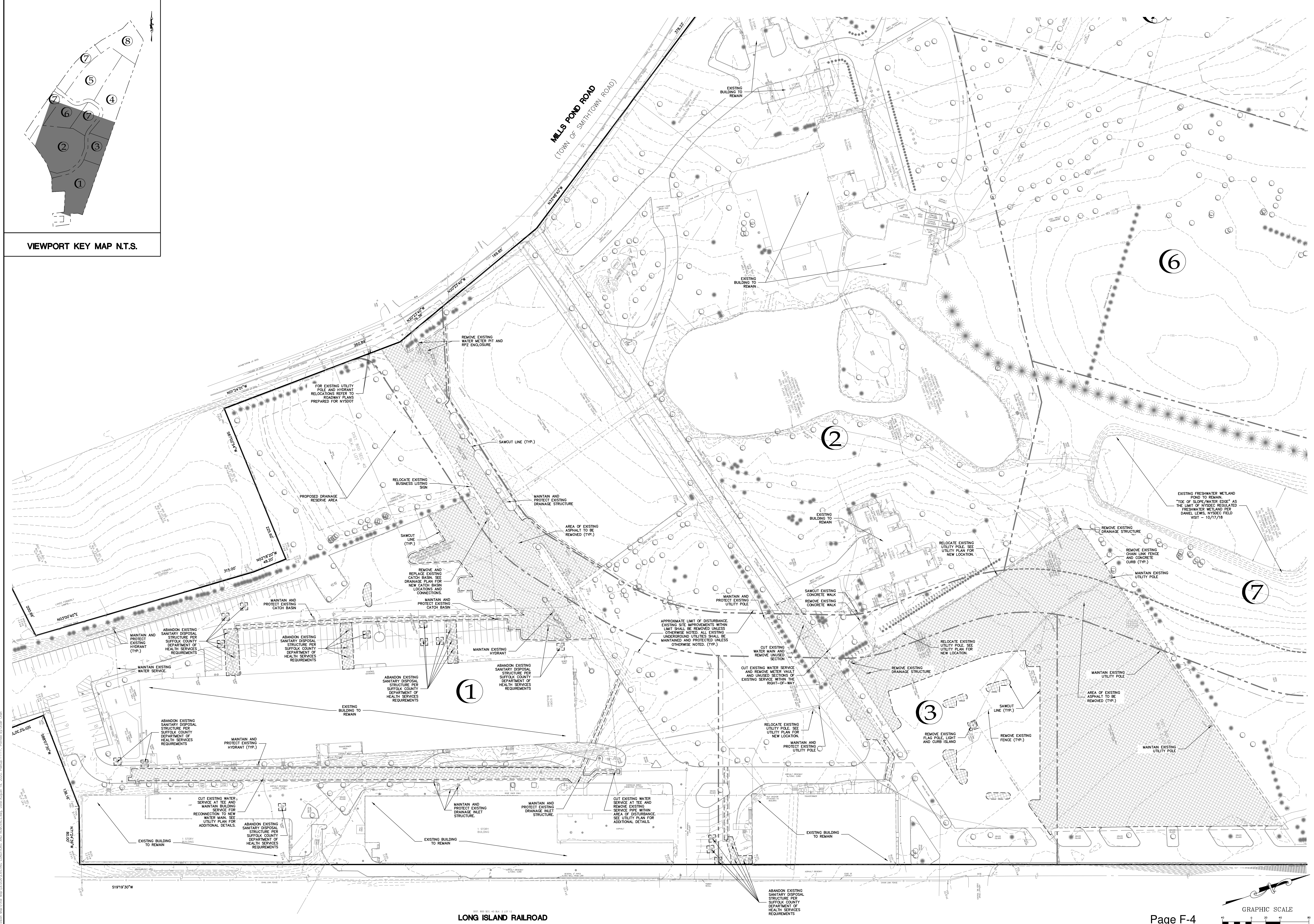
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SHEET
C-O
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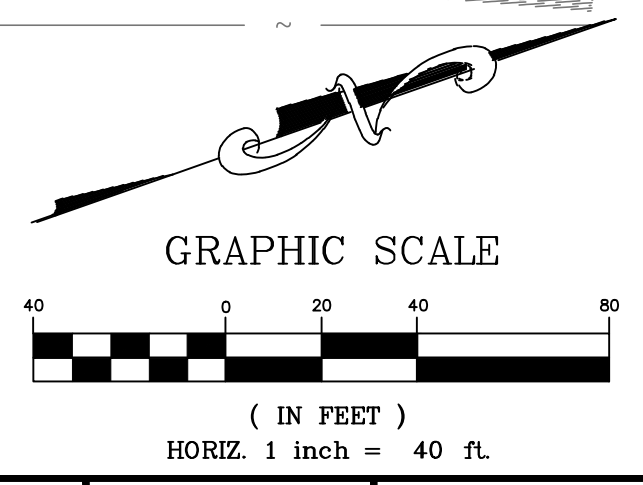




VIEWPORT KEY MAP N.T.S.



LONG ISLAND RAILROAD



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PROJECT NAME:
MAP OF FLOWERFIELD FINAL ENGINEERING

PROJECT FOR:
1 FLOWERFIELD DRIVE ST. JAMES, NY 11780

TITLE:
EXISTING CONDITIONS AND REMOVALS (1)

DISCIPLINE:
CIVIL

PROJECT ENGINEER:
 OC
 CE0338A

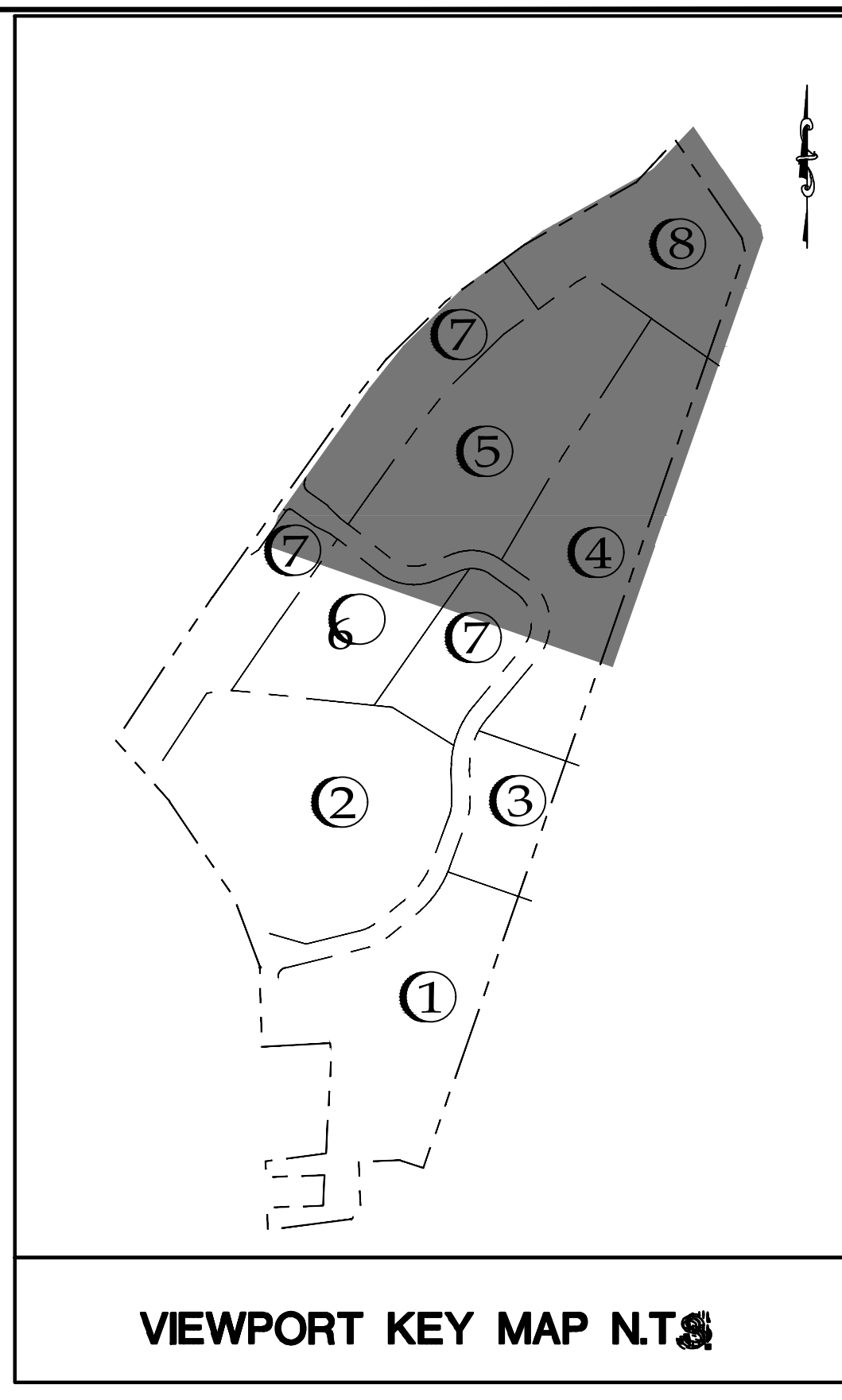
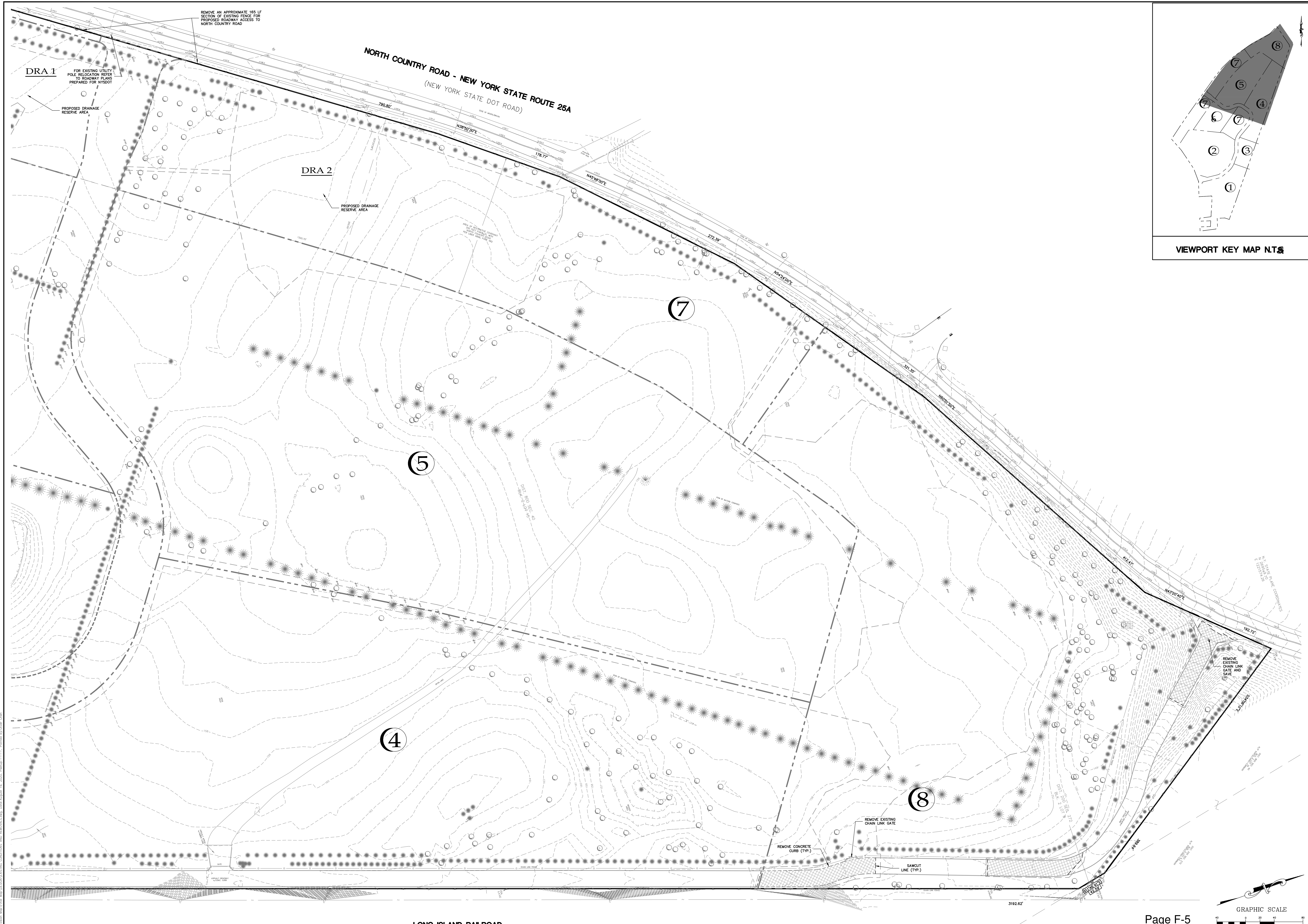
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DATE:
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SCALE:
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SHEET
C-1
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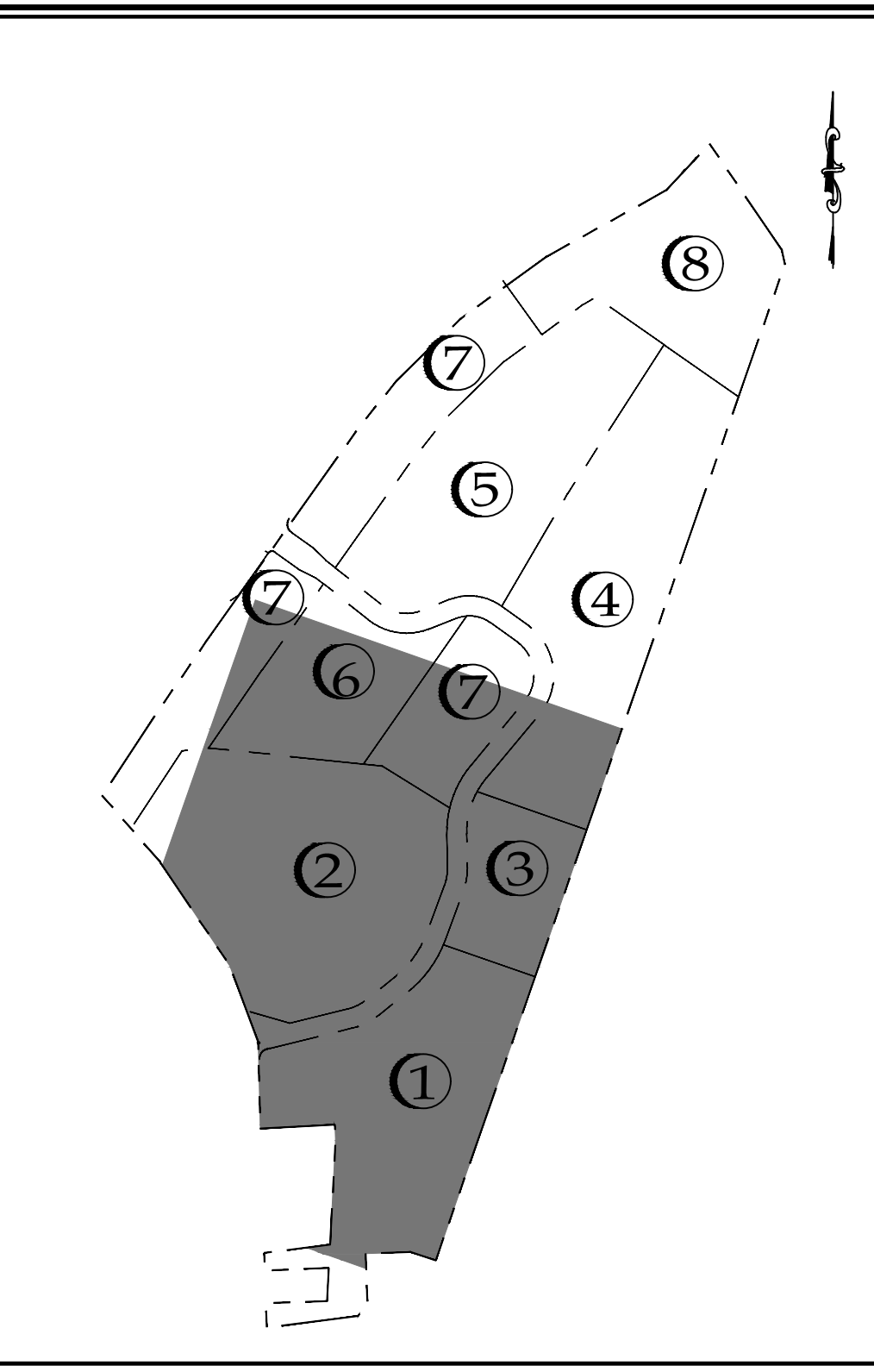
PROJECT NAME:
MAP OF FLOWERFIELD

PROJECT FOR:
**1 FLOWERFIELD DRIVE
 ST. JAMES, NY 11780**

TITLE:
**EXISTING CONDITIONS AND
 REMOVALS PLAN (2)**

DISCIPLINE:
CIVIL

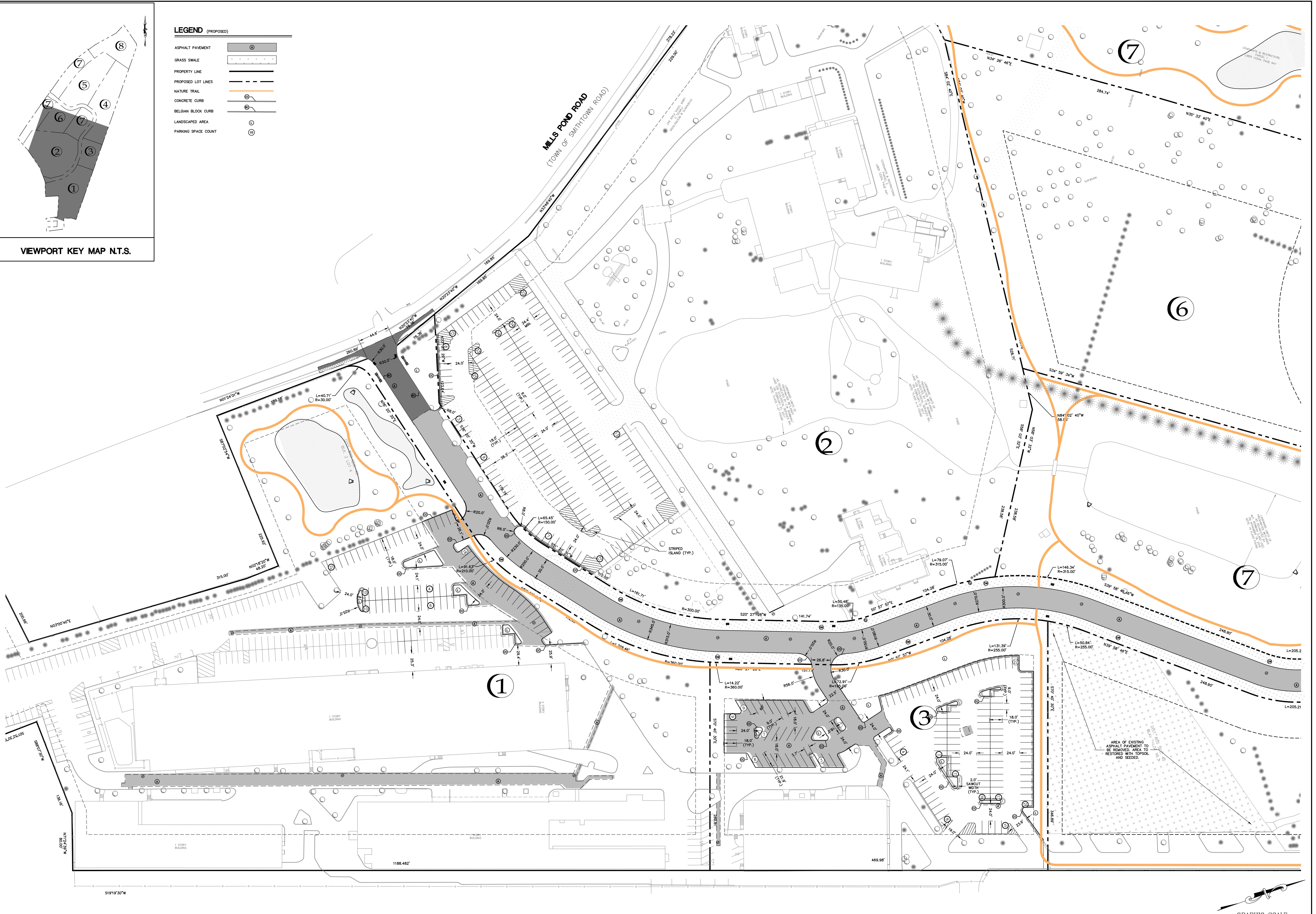
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DESIGNED BY: OC	JOB NO. CE0338A	SHEET 3 of 22
DRAWN BY: OC	DATE: APRIL 2020	SCALE: AS SHOWN
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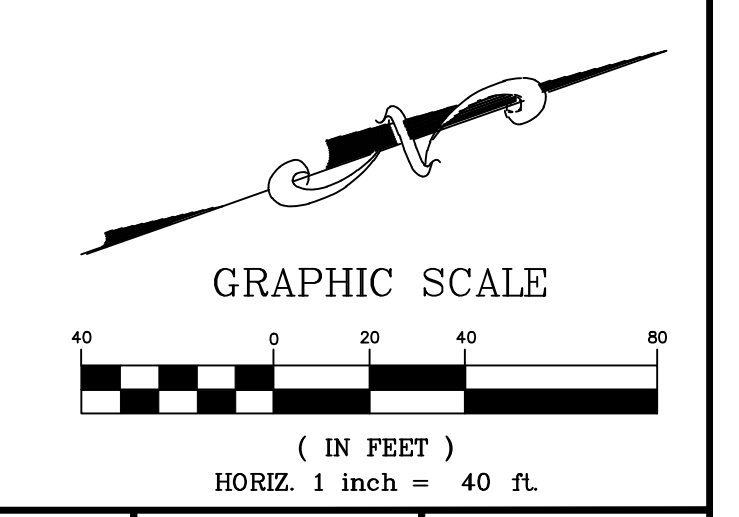
LEGEND (PROPOSED)

ASPHALT PAVEMENT	
GRASS SWALE	
PROPERTY LINE	
PROPOSED LOT LINES	
NATURE TRAIL	
CONCRETE CURB	
BELGIAN BLOCK CURB	
LANDSCAPED AREA	
PARKING SPACE COUNT	

VIEWPORT KEY MAP N.T.S.



LONG ISLAND RAILROAD



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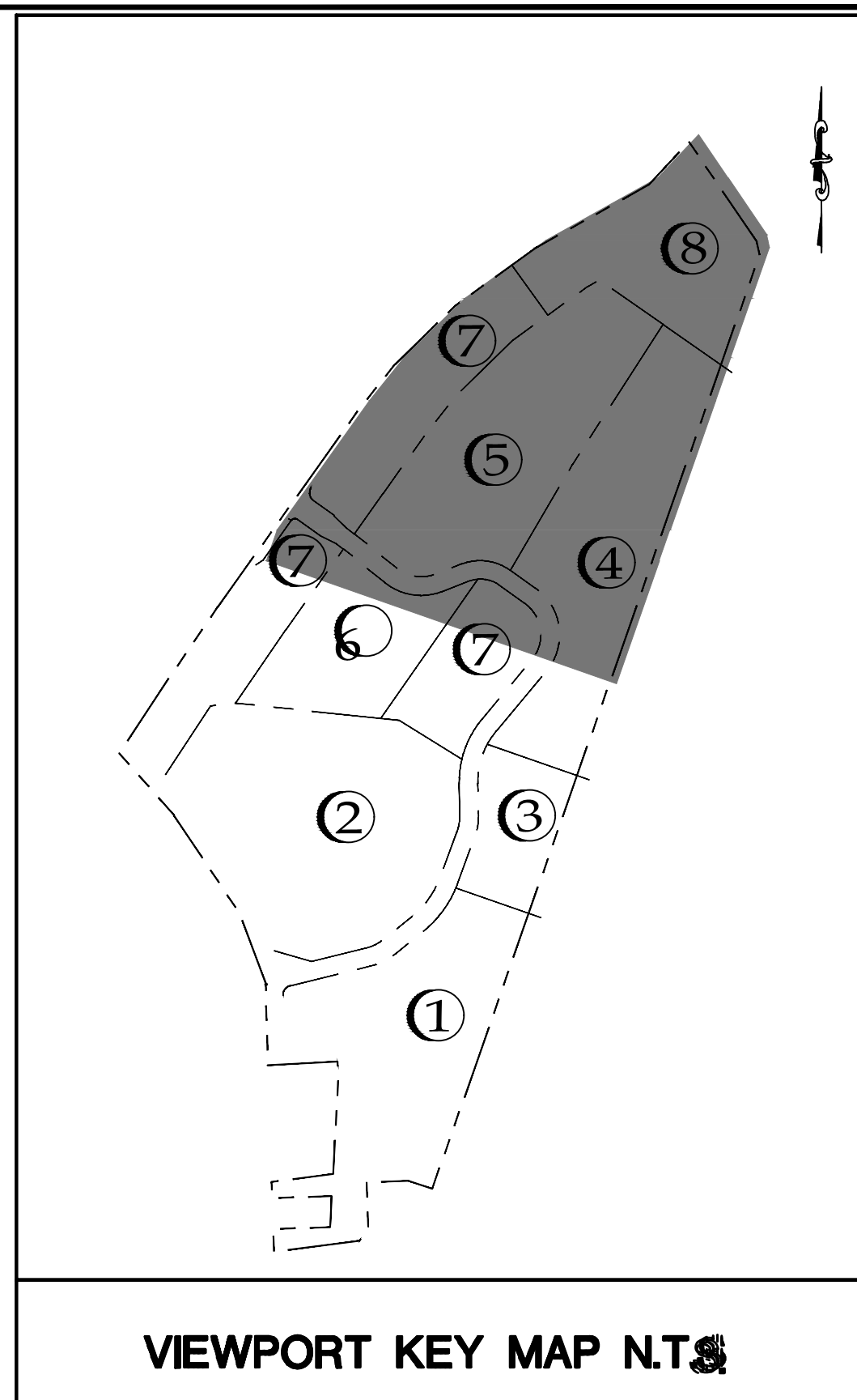
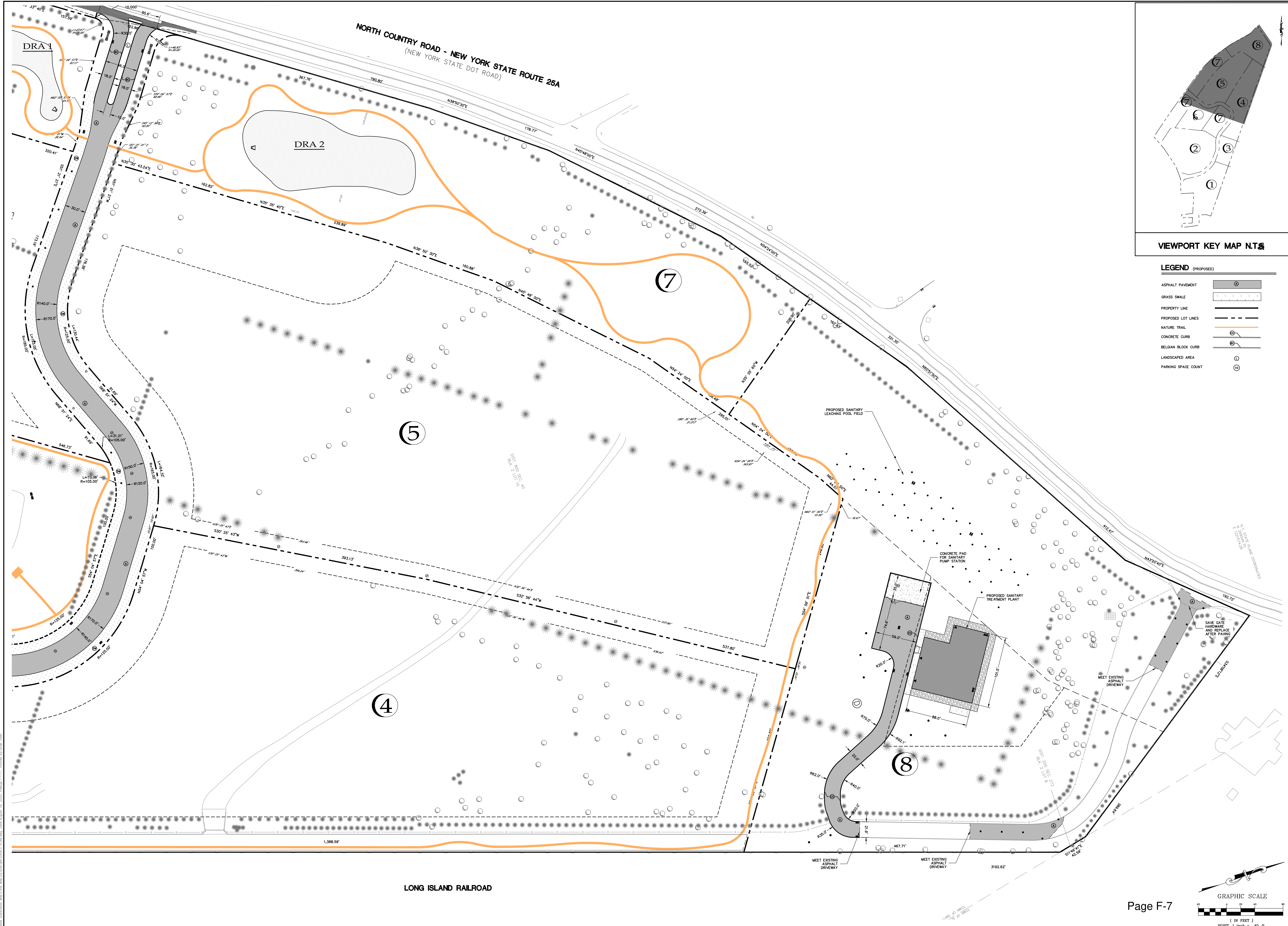
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PROJECT NAME:
MAP OF FLOWERFIELD FINAL ENGINEERING
PROJECT FOR:
1 FLOWERFIELD DRIVE ST. JAMES, NY 11780

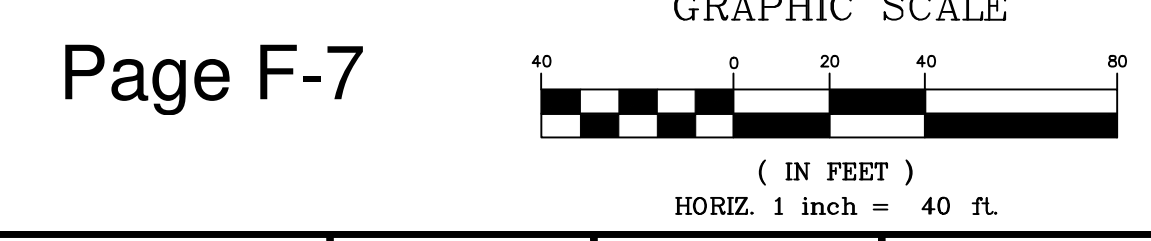
TITLE:
MATERIALS AND ALIGNMENT PLAN (1)
DISCIPLINE:
CIVIL

PROJECT ENGINEER:	OC	PROJECT NO.:	CE0338A	C-3 SHEET 4 of 22
DESIGNED BY:	OC	JOB NO.:	CE0338A	
DRAWN BY:	OC	DATE:	APRIL 2020	
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LEGEND (PROPOSED)

- ASPHALT PAVEMENT
- GRASS SWALE
- PROPERTY LINE
- PROPOSED LOT LINES
- NATURE TRAIL
- CONCRETE CURB
- BELGIAN BLOCK CURB
- LANDSCAPED AREA
- PARKING SPACE COUNT



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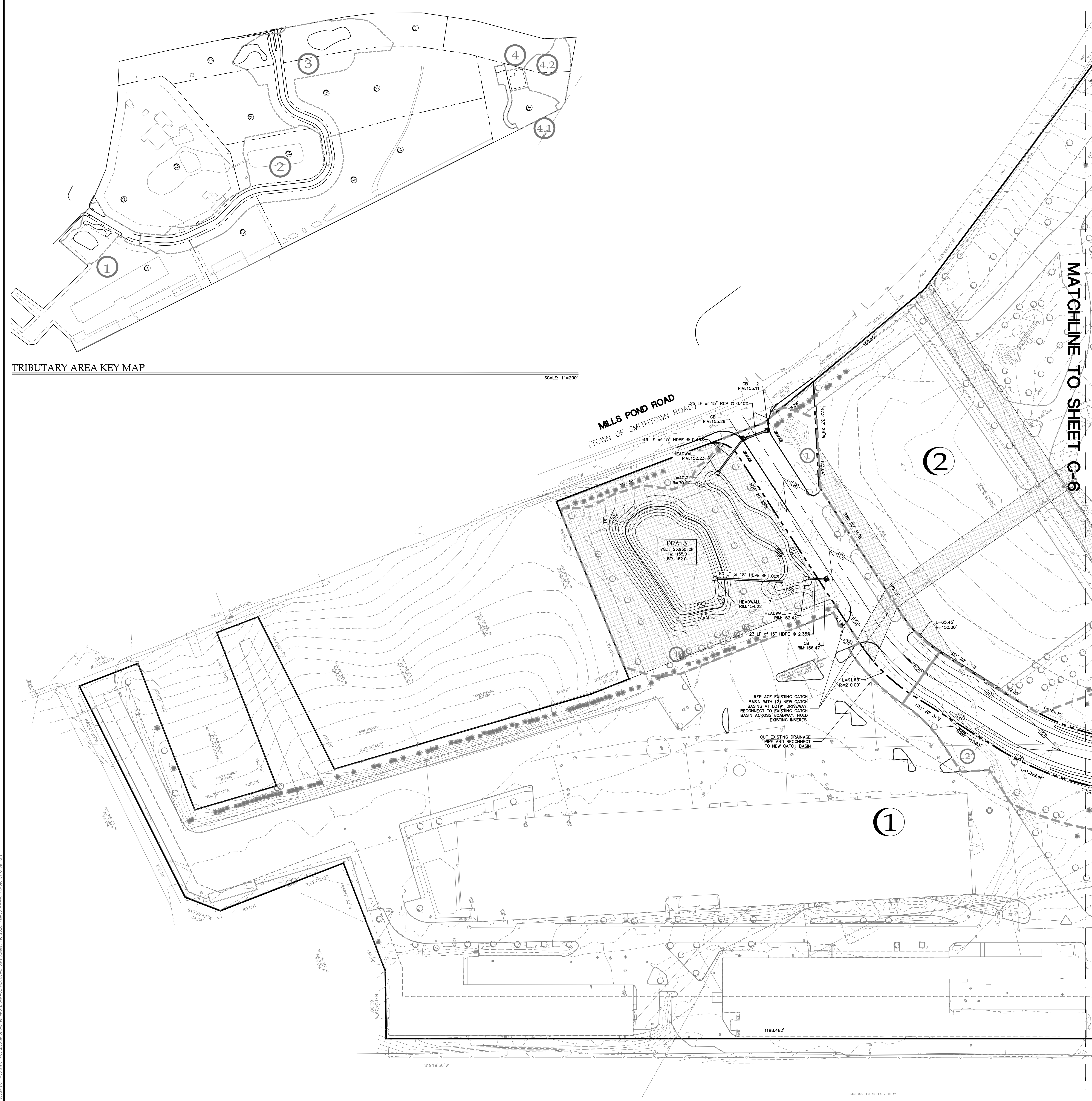
PROJECT FOR:
**1 FLOWERFIELD DRIVE
 ST. JAMES, NY 11780**

TITLE:
MATERIALS AND ALIGNMENT PLAN (2)

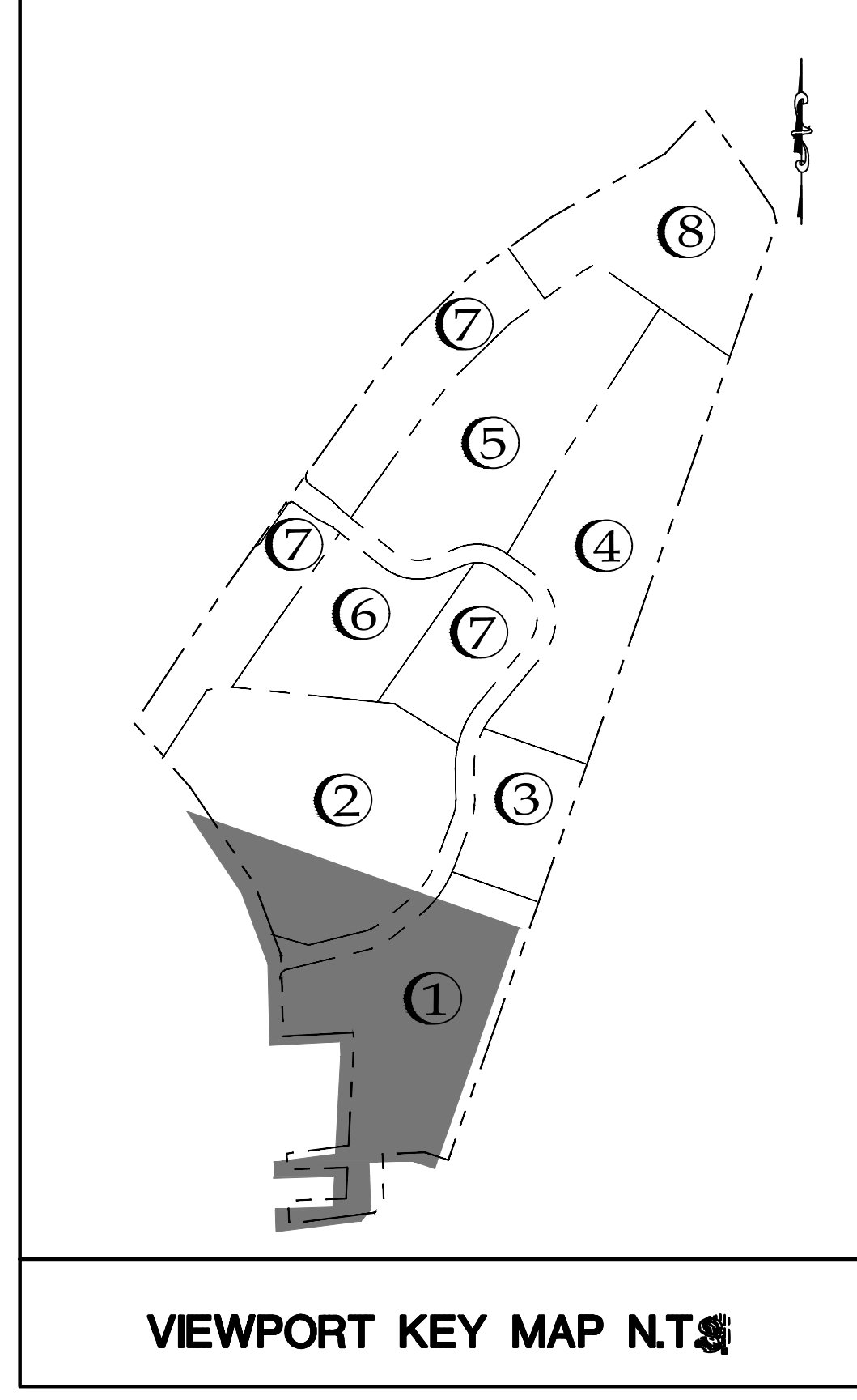
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CIVIL

PROJECT ENGINEER: OC	PROJECT NO. CE0338A	DRAWING NO.
DESIGNED BY: OC	JOB NO. CE0338A	
DRAWN BY: OC	DATE: APRIL 2020	C-4
CHECKED BY: RZ	SCALE: AS SHOWN	SHEET 5 of 22

NYS SEAL: 103183-02 - Engineer - Civil - Statewide Multi-Year Multi-Subject Matter License - Date: August 18, 2020. Expires: 08/18/2025. Provided by: Other - Other



TRIBUTARY AREA KEY MAP



PIPE FLOW CALCULATIONS

DESIGN DISCHARGE RATE FROM SUB-SHED #1:

A (AREA OF SUB-SHED) = 1.75 AC
I (COEFFICIENT OF RUNOFF) = 0.41
R (RAINFALL INTENSITY) = 120 / (1+20)
T (TIME OF CONCENTRATION) = 6 MIN.
Q = 4.615 IN/HR
Q (DESIGN DISCHARGE RATE) = A x I x R
Q = 1.75 AC x 0.41 x 4.615 IN/HR
Q = 3.5 CFS

DESIGN DISCHARGE RATE FROM SUB-SHED #2 (WETLAND POND):

A (AREA OF SUB-SHED) = 6.75 AC
I (COEFFICIENT OF RUNOFF) = 0.48
R (RAINFALL INTENSITY) = 120 / (1+20)
T (TIME OF CONCENTRATION) = 9.48 MIN.
Q = 4.07 IN/HR
Q (DESIGN DISCHARGE RATE) = A x I x R
Q = 6.75 AC x 0.48 x 4.07 IN/HR
Q = 13.19 CFS

ASSUME DESIGN STORM EVENT OCCURS WHEN WATER LEVEL IN THE WETLAND POND IS AT THE HIGH WATER ELEVATION OF 148.5 AND OVERFLOW FROM WETLAND POND INTO OVERFLOW STRUCTURE WILL OCCUR AT THE DESIGN DISCHARGE RATE DETERMINED ABOVE.

DESIGN DISCHARGE RATE FROM SUB-SHED #4 (PAVED AREA):

A (AREA OF SUB-SHED) = 0.44 AC
I (COEFFICIENT OF RUNOFF) = 1.0
R (RAINFALL INTENSITY) = 120 / (1+20)
T (TIME OF CONCENTRATION) = 6 MIN.
Q = 4.615 IN/HR
Q (DESIGN DISCHARGE RATE) = A x I x R
Q = 0.44 AC x 1.0 x 4.615 IN/HR
Q = 2.03 CFS

PER TOWN OF SMITHTOWN DESIGN REQUIREMENTS, DESIGN VELOCITY SHALL BE DETERMINED USING MANNING'S FORMULA FOR PIPE VELOCITY. PIPE DESIGN VELOCITY SHALL NOT EXCEED 15 FPS.

MANNING'S FORMULA FOR PIPE FLOW (Q):
Q = 1.486/n x A x R^{2/3} x S^{1/2}

MANNING'S FORMULA FOR PIPE VELOCITY (V):
V = 1.486/n x R^{2/3} x S^{1/2}

PIPE DIAMETER	n	R	Q (GPM)	V (FPS)	Q (GPM)	V (FPS)
18"	0.013	8.0	138	2.88	-	-
24"	0.013	8.0	-	-	18	6.08

DRAINAGE CALCULATIONS

TOTAL DRAINAGE SHED AREA OF PROPOSED IMPROVEMENTS: 697,279 SF

A. SUB-SHED #1 (ORA 3):
SUB-SHED TRIBUTARY AREA: 76,022 SF
REQUIRED STORAGE:
LANDSCAPED AREA: 64,131 SF x 0.3 x 8"/12" = 12,826 CF
PAVED AREA: 11,891 SF x 1.0 x 8"/12" = 7,927 CF
TOTAL REQUIRED SUB-SHED STORAGE = 20,753 CF
PROPOSED STORAGE:
DRA 3 STORAGE VOLUME = 25,950 CF

B. SUB-SHED #2 (WETLAND POND):
SUB-SHED TRIBUTARY AREA: 294,073 SF
REQUIRED STORAGE:
LANDSCAPED AREA: 220,388 SF x 0.3 x 8"/12" = 44,078 CF
PAVED AREA: 40,168 SF x 1.0 x 8"/12" = 26,779 CF
POND AREA: 33,517 SF x 1.0 x 8"/12" = 22,345 CF
TOTAL REQUIRED SUB-SHED STORAGE = 93,202 CF
PROPOSED STORAGE:
WETLAND POND STORAGE VOLUME = 75,538 CF
OVERFLOW VOLUME:
OVERFLOW VOLUME IS CONVEYED TO DRAs 1 AND 2. = 17,664 CF

C. SUB-SHED #3 (ORA 1 & 2):
SUB-SHED TRIBUTARY AREA: 300,003 SF
REQUIRED STORAGE:
LANDSCAPED AREA: 273,167 SF x 0.3 x 8"/12" = 54,633 CF
PAVED AREA: 26,836 SF x 1.0 x 8"/12" = 17,891 CF
OVERFLOW VOLUME FROM SUB-SHED #2: = 17,664 CF
TOTAL REQUIRED SUB-SHED STORAGE = 90,188 CF
PROPOSED STORAGE:
(9) - 10" DRYWELLS WITH 18" EFFECTIVE DEPTH
9 x 68.4 CF/AF x 17 VF = 10,465 CF
DRA 1 STORAGE VOLUME = 20,893 CF
DRA 2 STORAGE VOLUME = 59,309 CF
TOTAL PROVIDED STORAGE VOLUME = 90,711 CF

D. SUB-SHED #4:
SUB-SHED TRIBUTARY AREA: 27,181 SF
REQUIRED STORAGE:
PAVED AREA: 19,235 SF x 1.0 x 8"/12" = 12,823 CF
BUILDING AREA: 7,946 SF x 1.0 x 8"/12" = 5,297 CF
PROPOSED STORAGE:
PAVED AREA:
(7) - 12" DRYWELLS WITH 15" EFFECTIVE DEPTH
7 x 100.88 CF/AF x 19 VF = 13,417 CF
BUILDING AREA:
(3) - 12" DRYWELLS WITH 18" EFFECTIVE DEPTH
3 x 100.88 CF/AF x 18 VF = 5,448 CF

E. SUB-SHED #1.1:
SUB-SHED TRIBUTARY AREA: 47,191 SF
REQUIRED STORAGE:
PAVED AREA: 6,271 SF x 1.0 x 5.05"/12" = 2,639 CF
FORESTED AREA: 40,920 SF x 0.15 x 5.05"/12" = 2,583 CF
TOTAL REQUIRED SUB-SHED STORAGE = 5,222 CF
PROPOSED STORAGE:
(5) - 10" DRYWELLS WITH 18" EFFECTIVE DEPTH
5 x 68.4 CF/AF x 16 VF = 5,472 CF
TOTAL PROVIDED STORAGE VOLUME = 5,472 CF

F. SUB-SHED #1.2:
SUB-SHED TRIBUTARY AREA: 70,645 SF
REQUIRED STORAGE:
PAVED AREA: 9,678 SF x 1.0 x 5.05"/12" = 4,072 CF
FORESTED AREA: 60,967 SF x 0.15 x 5.05"/12" = 3,849 CF
TOTAL REQUIRED SUB-SHED STORAGE = 7,921 CF
PROPOSED STORAGE:
(8) - 10" DRYWELLS WITH 15" EFFECTIVE DEPTH
8 x 68.4 CF/AF x 15 VF = 8,208 CF
TOTAL PROVIDED STORAGE VOLUME = 8,208 CF

*SUB-SHEDS #1 AND #2 ARE SIZED FOR 5.05 INCHES OF RAINFALL IN ACCORDANCE WITH NYSDOT REQUEST FOR THE SEWER TREATMENT PLANT ACCESS DRIVE TO CONTAIN THE 10-YEAR STORM EVENT.

- GRADING AND DRAINAGE NOTES**
- STORM WATER MANAGEMENT SYSTEM IS DESIGNED TO RETAIN A TOTAL OF 8 INCHES OF RAINFALL WITHIN THE BOUNDARIES OF THE PROPOSED PUBLIC IMPROVEMENTS AND LANDS ADJACENT TO THE PUBLIC RIGHT-OF-WAY.
 - ALL MATERIALS AND METHODS OF CONSTRUCTION SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE TOWN, COUNTY AND STATE LAWS AND APPLICABLE CODES.
 - RUNOFF COEFFICIENTS:
IMPERVIOUS AREAS (ROOF, PAVED, WALKWAY) = 1.0
PERVIOUS AREAS (LANDSCAPED) = 0.30
 - ALL INTERCONNECTING DRAINAGE PIPING BETWEEN CATCH BASIN AND MANHOLES SHALL BE MIN. 15" HDPE @ MIN. 0.3% OF SLOPE.
 - ALL DRAINAGE STRUCTURES SHALL MEET TOWN OF SMITHTOWN AND SUFFOLK COUNTY STANDARDS.
 - MASS GRADING FILL AREAS SHALL BE FIRST STRIPPED OF TOPSOIL AND MASS GRADING FILL SHALL BE COMPACTED IN 6 INCH LIFTS, COMPACTED TO 95% STANDARD PROCTOR DENSITY.
 - ALL 1:2 AND 3:3 SLOPE AREAS WILL BE PROTECTED AGAINST EROSION DURING CONSTRUCTION AND PERMANENT GRASS COVER SHALL BE ESTABLISHED AS SOON AS POSSIBLE.
 - CREATE SMOOTH TRANSITIONS FROM PROPOSED GRADED AREAS TO EXISTING TERRAIN.
 - ALL EXISTING DRIVEWAYS SHALL BE PROPERLY REPAIRED OR RECONSTRUCTED AS NECESSARY.
 - NEW ASPHALT SURFACES SHALL PROVIDE A SMOOTH TRANSITION TO EXISTING PAVEMENT TO REMAIN.
 - NEW ASPHALT DRIVEWAYS SHALL USE THE CORRECT MIXTURE TO PREVENT SEGREGATION AND DEBRIS FROM WASHING TO ADJACENT AREAS, FURTHER SPECIFIED IN EROSION CONTROL NOTES AND DETAILS.
 - ALL DISTURBED AREAS TO BE TOPSOILED, PINE GRADED AND SEEDDED.
- LEGEND (PROPOSED)**
- PROPERTY BOUNDARY
 - PROPOSED LOT LINE
 - PROPOSED ROADWAY LIMITS
 - PROPOSED ROADWAY CENTER LINE
 - PROPOSED CONTOUR
 - PROPOSED SPOT ELEVATION
 - CATCH BASIN
 - 10" DIA. DRYWELL W/ BURIED INLET
 - 10" DIA. DRYWELL W/ BURIED COVER
 - 10" DIA. DRYWELL W/ TRAFFIC SLAB AND SOLID COVER TO GRADE
 - 10" DIA. DRYWELL W/ RECTANGULAR INLET
 - 12" DIA. DRYWELL W/ SOLID COVER TO GRADE
 - CONCRETE HEADWALL
 - SUB-SHED LINE
 - SUB-SHED NUMBER
- LEGEND (EXISTING)**
- A.E. UNIT
 - OVERHEAD WIRE
 - UTILITY POLE
 - 8" DIA. WIRE
 - 6" DIA. WIRE
 - DRYWELL INLET
 - LIGHT POLE
 - HYDRANT
 - WATER VALVE
 - WATER METER
 - IRRIGATION CONTROL VALVE
 - ELECTRIC MANHOLE
 - ELECTRIC BOX
 - TELEPHONE MANHOLE
 - TELEPHONE BOX
 - CABLE BOX
 - MANHOLE
 - DRAIN MANHOLE
 - SEWER MANHOLE

GRAPHIC SCALE
(IN FEET)
HORIZ. 1 inch = 40 ft.

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Corporate Headquarters: 177 Chambers Park Drive, Woodbury, NY 11797

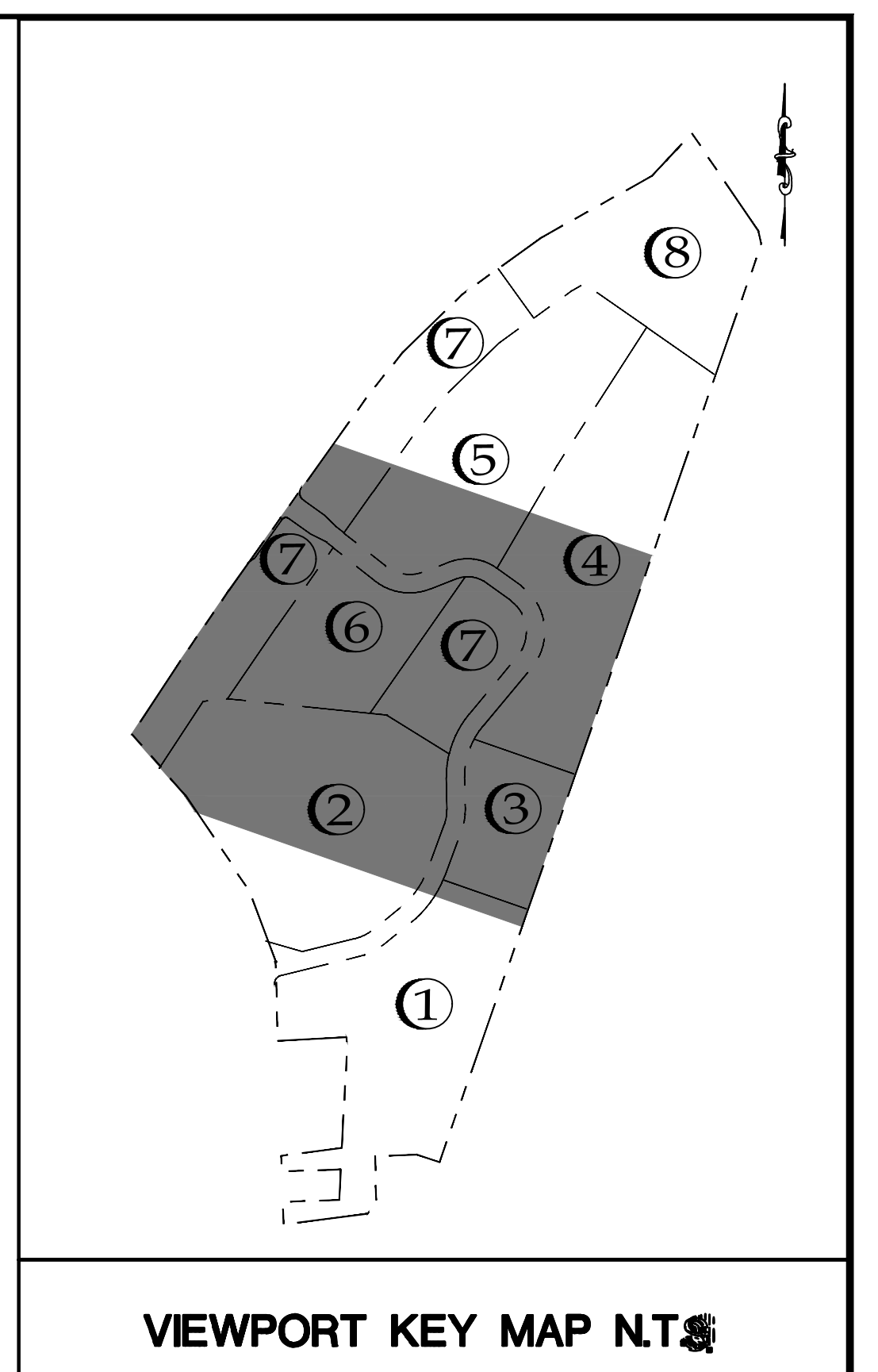
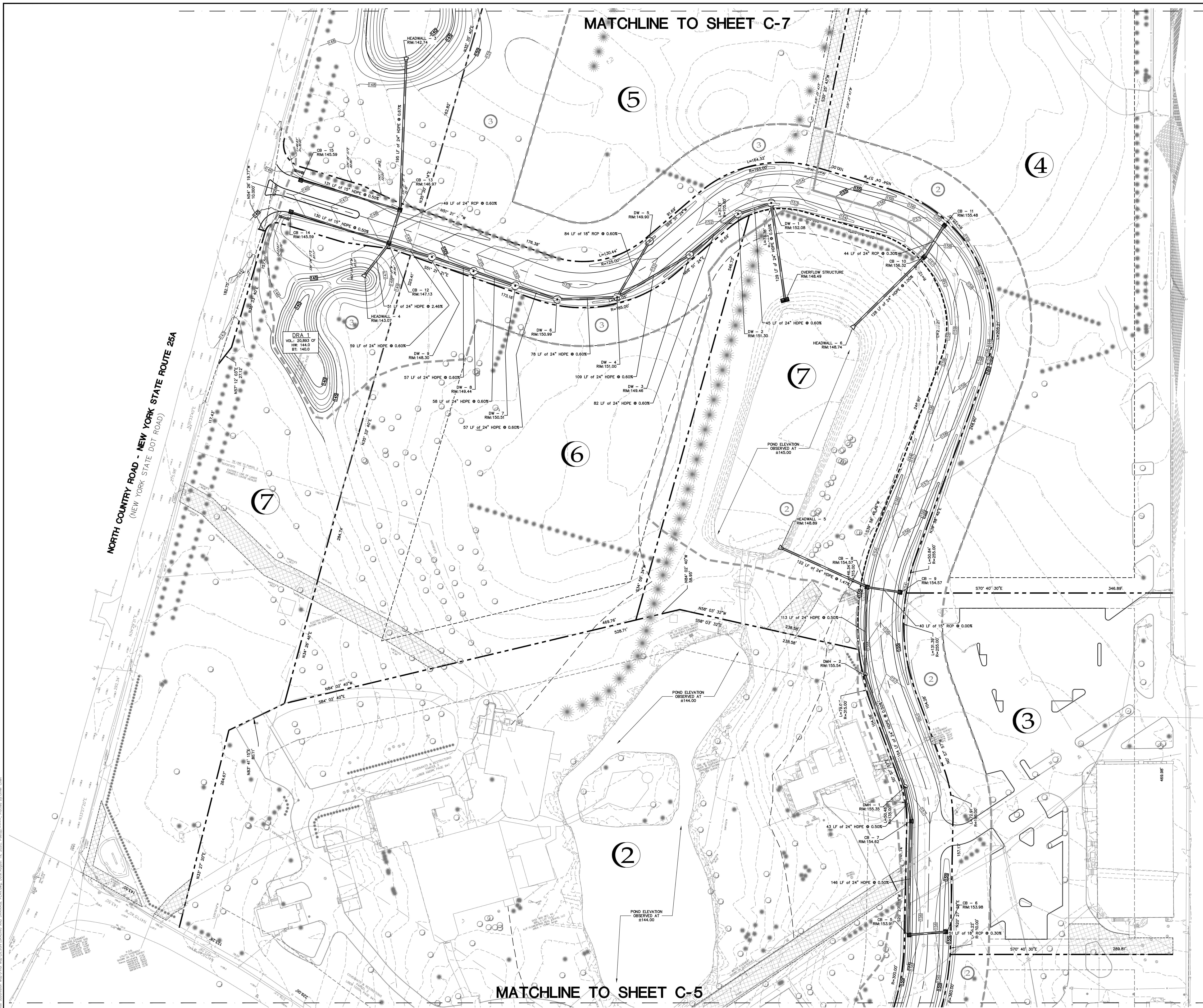
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T: 516.827.4800
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PROJECT NAME:
MAP OF FLOWERFIELD FINAL ENGINEERING

PROJECT FOR:
**1 FLOWERFIELD DRIVE
ST. JAMES, NY 11780**

TITLE:
GRADING AND DRAINAGE PLAN (1)

DISCIPLINE:
CIVIL

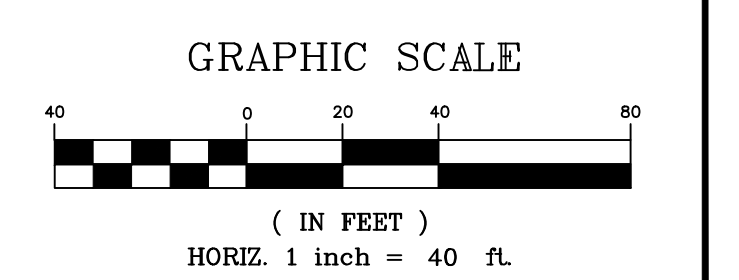
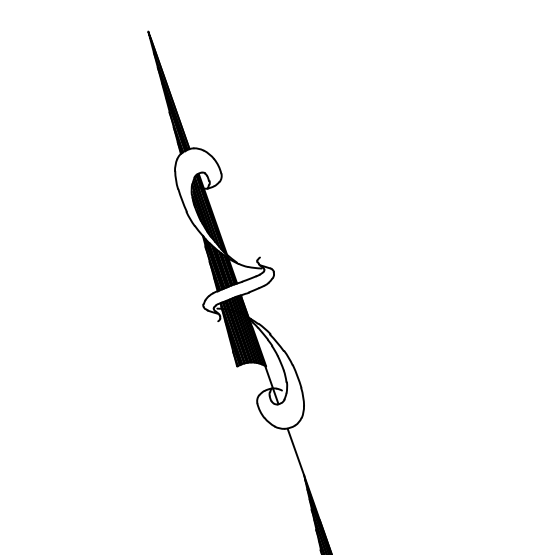


LEGEND (PROPOSED)

- PROPERTY BOUNDARY
- PROPOSED LOT LINE
- PROPOSED ROADWAY LIMITS
- PROPOSED ROADWAY CENTER LINE
- PROPOSED CONTOUR
- PROPOSED SPOT ELEVATION
- CATCH BASIN
- 10" DIA. DRYWELL W/ GRATE INLET
- 10" DIA. DRYWELL W/ BURIED COVER
- 10" DIA. DRYWELL W/ TRAFFIC SLAB AND SOLID COVER TO GRADE
- 10" DIA. DRYWELL W/ RECTANGULAR INLET
- 12" DIA. DRYWELL W/ SOLID COVER TO GRADE
- CONCRETE HEADWALL
- SUB-SHED LINE
- SUB-SHED NUMBER

LEGEND (EXISTING)

- 4" C. LINE
- OVERHEAD WIRE
- UTILITY POLES
- GUY WIRE
- SIGN
- GRASSME INLET
- LIGHT POLE
- HYDRANT
- WATER VALVE
- WATER METER
- IRRIGATION CONTROL VALVE
- ELECTRIC MANHOLE
- ELECTRIC BOX
- TELEPHONE MANHOLE
- TELEPHONE BOX
- CABLE BOX
- MANHOLE
- SEWER MANHOLE
- SEWER MANHOLE



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PROJECT NAME:
MAP OF FLOWERFIELD FINAL ENGINEERING

PROJECT FOR:
1 FLOWERFIELD DRIVE ST. JAMES, NY 11780

TITLE:
GRADING AND DRAINAGE PLAN (2)

DISCIPLINE:
CIVIL

PROJECT ENGINEER:
OC

DESIGNED BY:
OC

DRAWN BY:
OC

CHECKED BY:
RZ

PROJECT NO.:
CE0338A

JOB NO.:
CE0338A

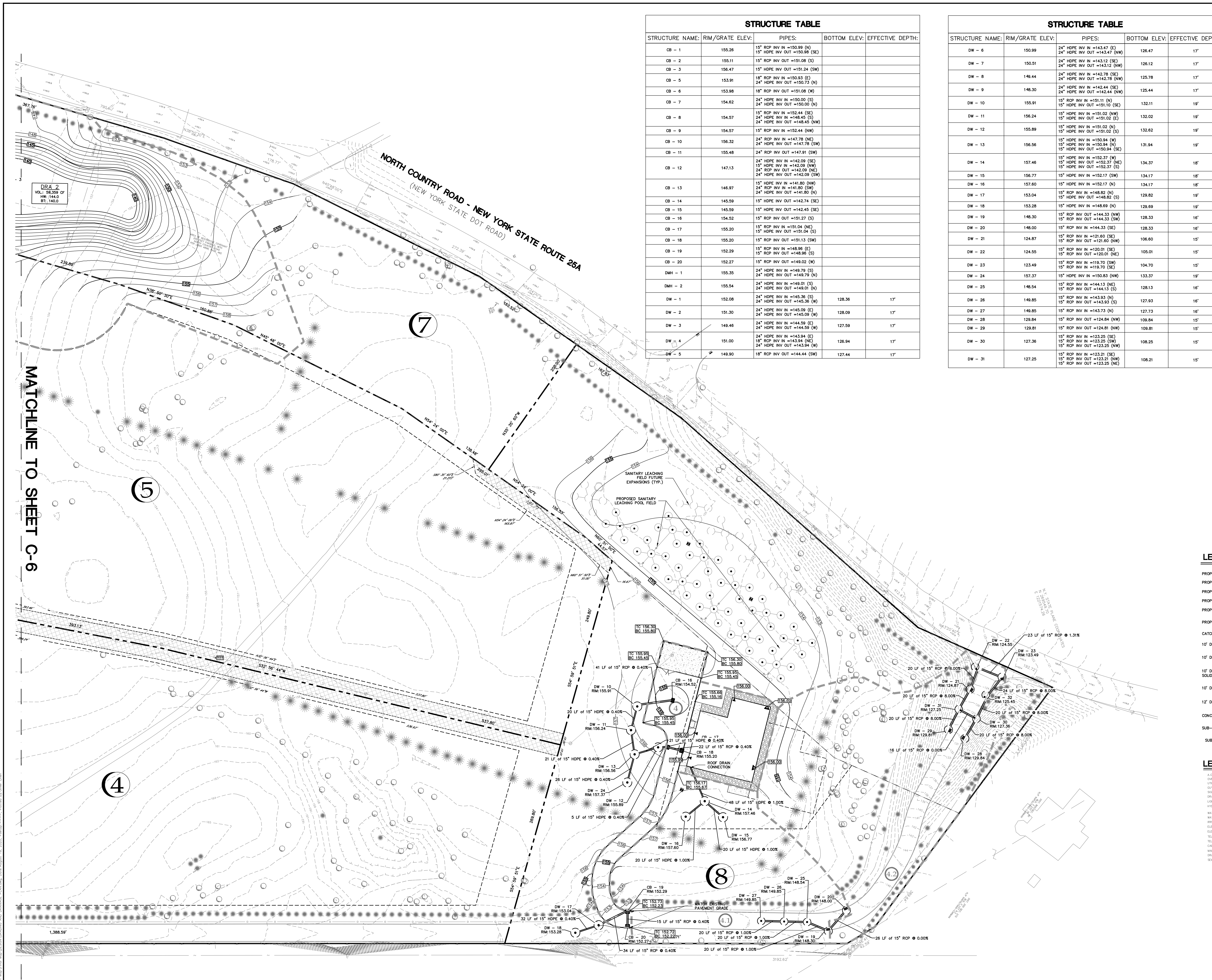
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APRIL 2020

SCALE:
AS SHOWN

DRAWING NO.:
C-6

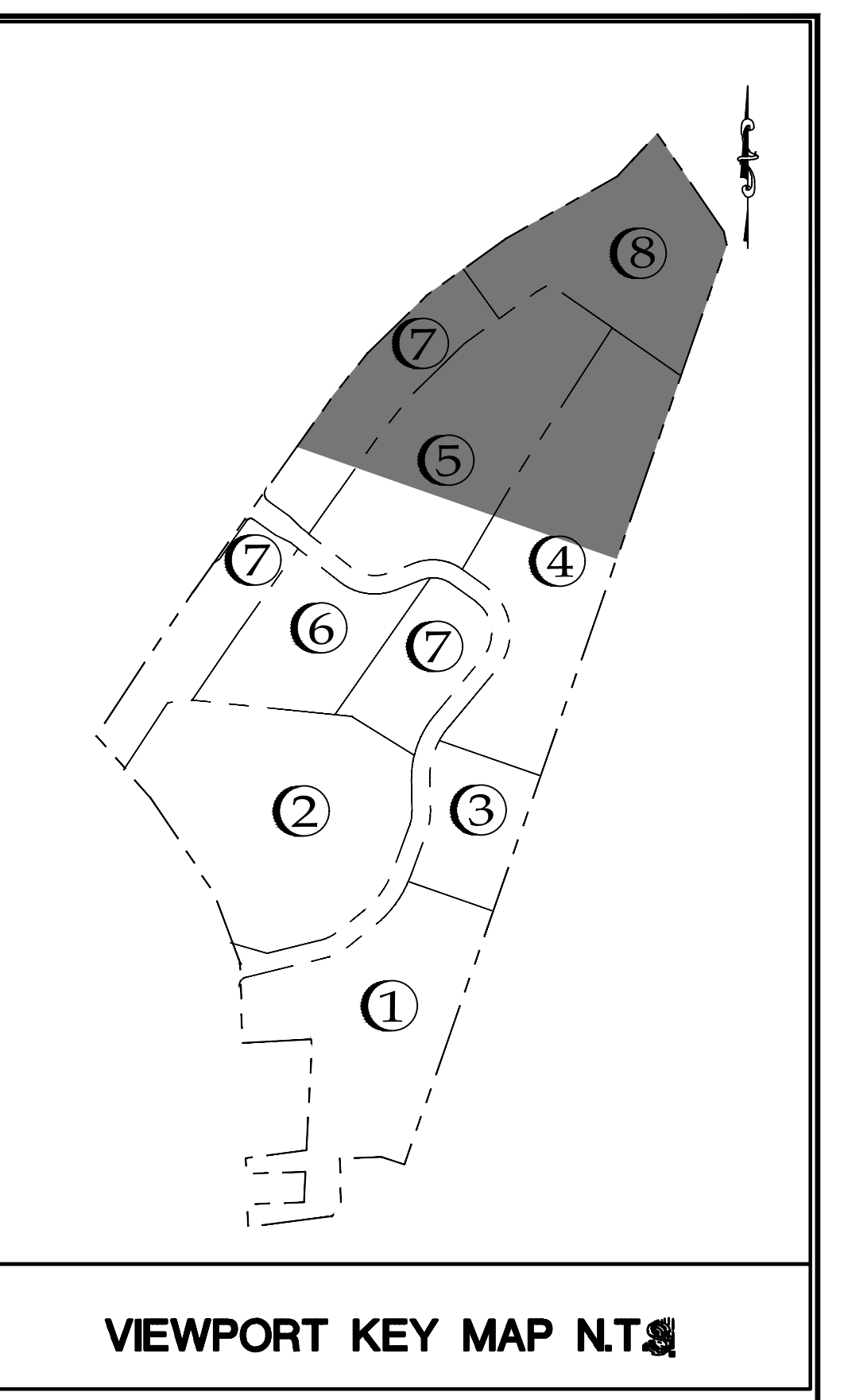
SHEET:
7 of 22

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 FILED: SUFFOLK COUNTY CLERK 06/14/2022 03:06 PM
 INDEX NO. 698051/2022 RECEIVED NYSCRF: 06/14/2022
 DRAWING: GRADING AND DRAINAGE PLAN (2)
 SHEET: 7 of 22
 PROJECT: MAP OF FLOWERFIELD FINAL ENGINEERING
 PROJECT FOR: 1 FLOWERFIELD DRIVE ST. JAMES, NY 11780
 PROJECT ENGINEER: OC
 DESIGNED BY: OC
 DRAWN BY: OC
 CHECKED BY: RZ
 DATE: APRIL 2020
 SCALE: AS SHOWN
 SHEET: 7 of 22

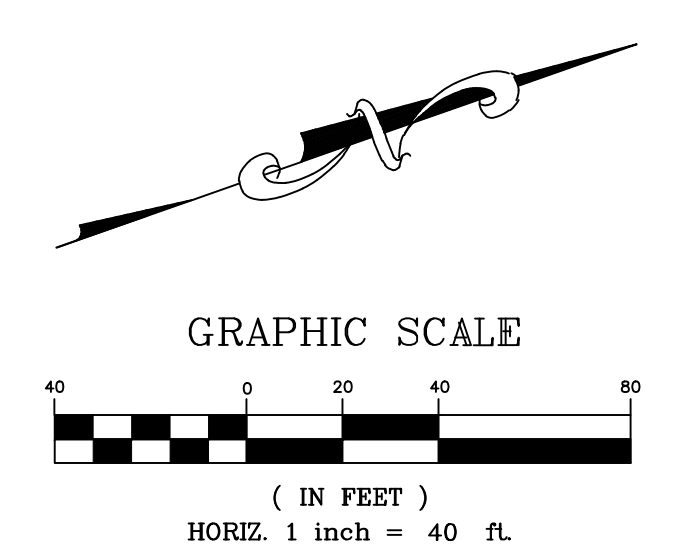
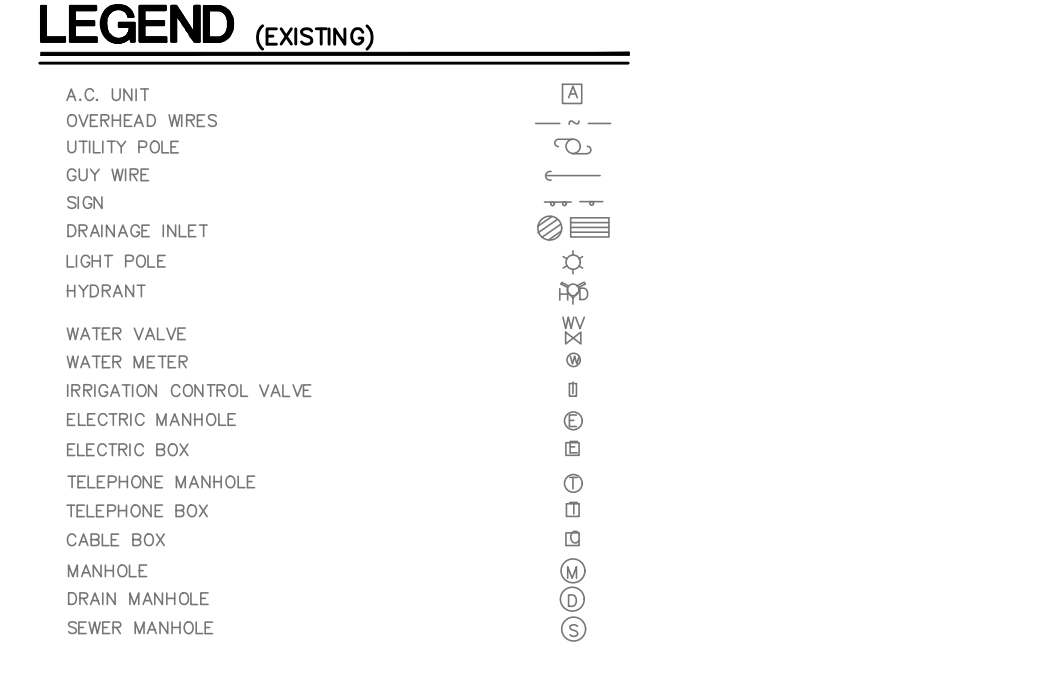
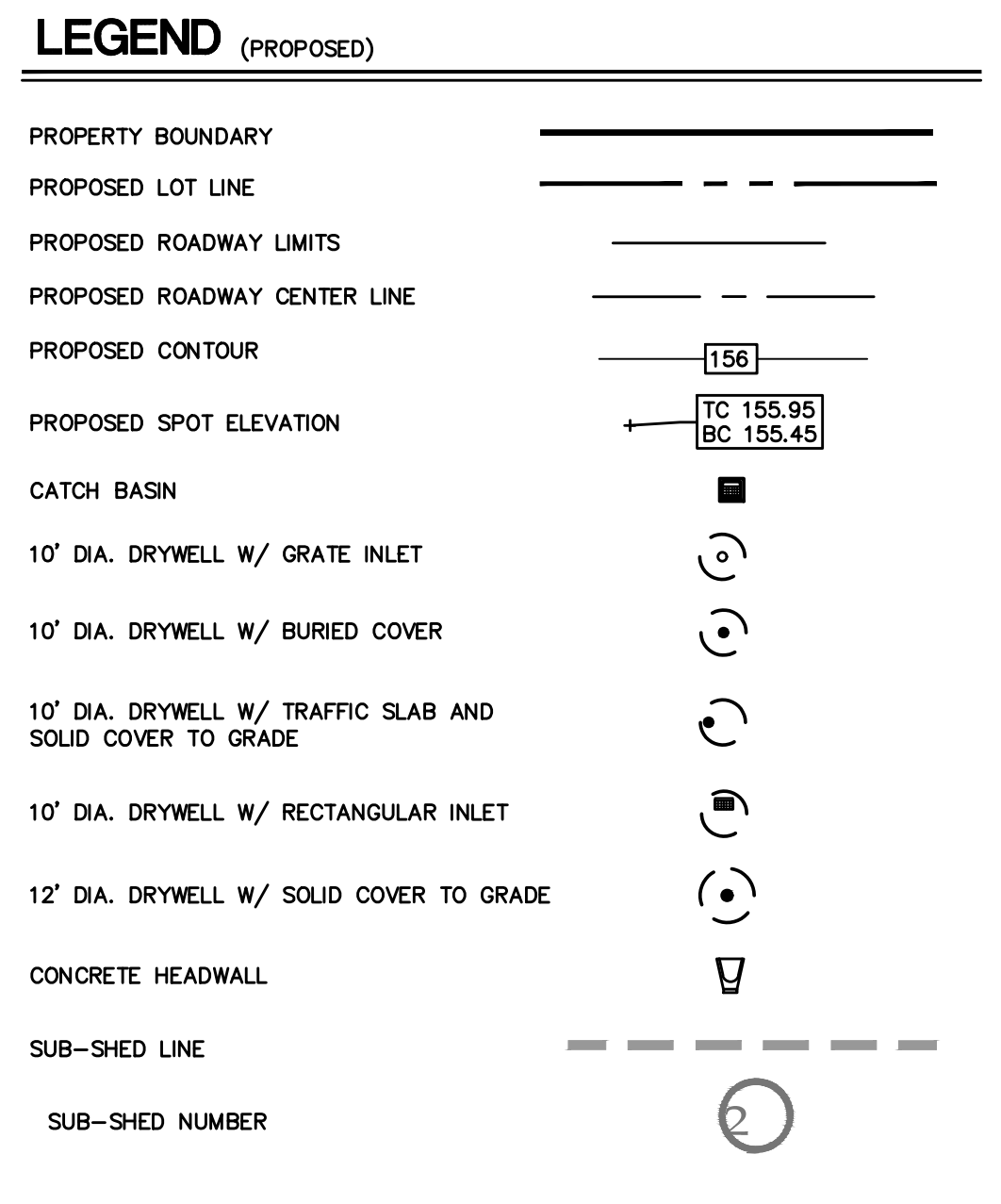


STRUCTURE TABLE				
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STRUCTURE TABLE				
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MATCHLINE TO SHEET C-6



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PROJECT NAME:
MAP OF FLOWERFIELD FINAL ENGINEERING

PROJECT FOR:
1 FLOWERFIELD DRIVE ST. JAMES, NY 11780

TITLE:
GRADING AND DRAINAGE PLAN (3)

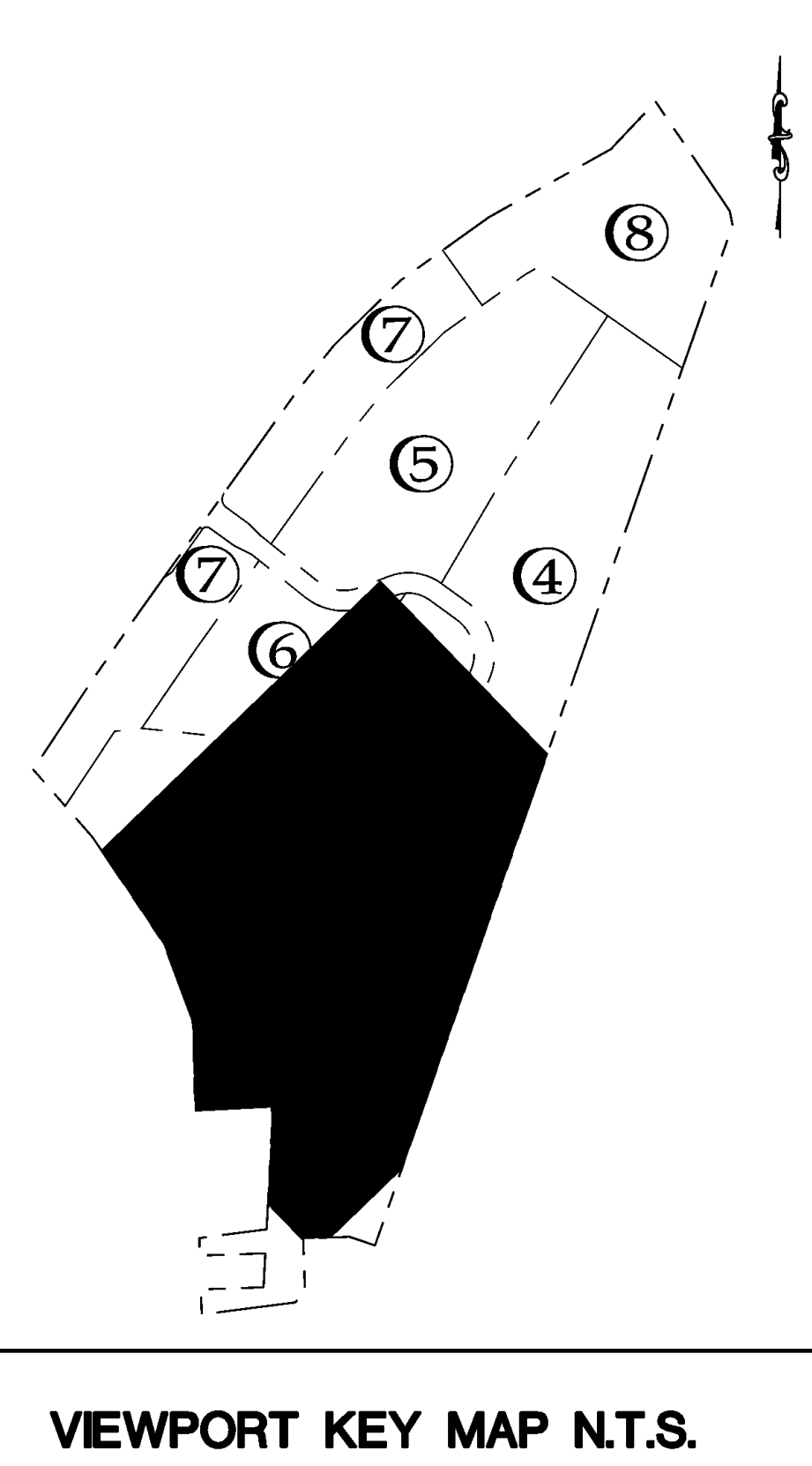
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PROJECT ENGINEER: OC
 JOB NO.: CE0338A
 DATE: APRIL 2020
 SCALE: AS SHOWN

PROJECT NO.: CE0338A
 DRAWING NO.: C-7
 SHEET 8 OF 22

- LEGEND (PROPOSED)**
- OVERALL PROPERTY LINE
 - SUBDIVISION LOT LINE
 - WATER MAIN/SERVICE
 - FIRE SERVICE
 - SANITARY SEWER MAIN
 - UTILITY POLE
 - SANITARY MANHOLE
 - UTILITY CROSSING NUMBER
 - FIRE HYDRANT
 - WATER VALVE
 - WATER METER PIT

- LEGEND (EXISTING)**
- A.C. UNIT
 - OVERHEAD WIRE
 - UTILITY POLE
 - GUY WIRE
 - SOIL
 - DRAINAGE INLET
 - LIGHT POLE
 - HYDRANT
 - WATER VALVE
 - WATER METER
 - IRRIGATION CONTROL VALVE
 - ELECTRIC MANHOLE
 - ELECTRIC BOX
 - TELEPHONE MANHOLE
 - TELEPHONE BOX
 - CABLE BOX
 - MANHOLE
 - DRAIN MANHOLE
 - SEWER MANHOLE



SANITARY SEWER NOTES

- THIS PROPERTY IS LOCATED WITHIN THE SUFFOLK COUNTY DEPARTMENT OF PUBLIC WORKS (SCDPW) SEWER DISTRICT.
- THE DEVELOPER/SUB-CONTRACTOR SHALL INSTALL THE SANITARY SEWERS IN ACCORDANCE WITH SCDPW STANDARD SPECIFICATIONS AND DETAILS FOR THE CONSTRUCTION OF SANITARY SEWERS, UNDER A PERMIT AND/OR SEWER AGREEMENT WITH THE COUNTY.
- THE GENERAL SUB-CONTRACTOR SHALL NOTIFY THE DEPARTMENT OF PUBLIC WORKS A MINIMUM OF TWO (2) WORKING DAYS PRIOR TO WORK INVOLVING ANY SANITARY SEWER NOTIFICATION IS TO BE MADE BY CALLING (516) 854-4185. ALL SUCH WORK MUST BE PERFORMED IN THE PRESENCE OF A COUNTY INSPECTOR.
- WHERE SANITARY OR HOUSE CONNECTION SEWERS CROSS OVER A DRAINAGE TRENCH AREA, THE SEWER SHALL BE REPLACED WITH DUCTILE IRON EXTENDING A MINIMUM OF FIVE (5) FEET EACH SIDE OF THE CROSSING TO UNDISTURBED SOIL. THE SAME REPLACEMENT SHALL APPLY FOR SEWERS UNDER A DRAINAGE TRENCH AREA WITHIN TWELVE (12) INCHES CLEARANCE, BOTTOM OF DRAIN TO TOP OF SEWER.
- ALL PIPES, MANHOLES AND APPURTENANCES SHALL HAVE THE COUNTY APPROVAL STAMP THEREON OR WRITTEN CERTIFICATION ACCEPTABLE TO THE COUNTY BEFORE THE MATERIAL CAN BE INSTALLED.
- THE MAXIMUM HEIGHT LIMITS FOR BROOKROOF FOR NEW MANHOLES AREA FOUR (4) INCHES. ADJUSTMENTS TO CHIMNEY HEIGHT TO MEET THE LIMITS SHALL BE BY ALTERNATING THE PRECAST MANHOLE BRICK. ADDITIONAL REVISIONS TO MEET THE LIMITS SHALL BE BY ALTERNATING THE PRECAST MANHOLE BRICK.
 - ONLY CONCRETE BRICK WILL BE USED FOR BROOKROOF.
 - IT IS TO BE SET IN PORTLAND CEMENT CONCRETE. BROCK MORTAR WILL NOT BE PERMITTED.
 - THE MANHOLE TOP IS TO BE SET IN PORTLAND CEMENT CONCRETE. BROCK MORTAR WILL NOT BE PERMITTED.
 - THE PAVEMENT TO MEET IMPROPERLY SET MANHOLES WILL NOT BE PERMITTED.
 - THE MANHOLES AND COVERS MUST BE CLEAN AND FREE FROM ALL ROAD PAVING MATERIALS AND DEBRIS PRIOR TO PAINTING THE CASTINGS.
- ALL CONNECTIONS TO MANHOLES MUST BE BY THE KOR-N-SEAL METHOD.
- THE GENERAL SUB-CONTRACTOR SHALL COMPLY WITH ALL OSHA REQUIREMENTS FOR ENTRY INTO A CONFINED SPACE HOWEVER IT IS NECESSARY FOR A GENERAL SUB-CONTRACTOR'S EMPLOYEE TO ENTER A COUNTY SANITARY SEWER MANHOLE. THE MINIMUM REQUIREMENTS FOR ENTRY TO SUB-CONTRACTOR MUST COMPLY WITH:
 - GENERAL SUB-CONTRACTOR SHALL OBTAIN "ENTRY PERMIT".
 - CONFINED SPACE ENTRY MONITOR TO TEST FOR TOXIC, EXPLOSIVE AND OXYGEN DEFICIENT.
 - CONFINED SPACE RESCUE AND RETRIEVAL EQUIPMENT.
- THE GENERAL SUB-CONTRACTOR WILL NOT BE PERMITTED TO WORK IN A COUNTY SANITARY SEWER MANHOLE UNLESS HE COMPLETES ALL APPLICABLE OSHA REQUIREMENTS.
- ON-SITE GRANTY SEWER MAINS AND MANHOLES SHALL BE OWNED AND MAINTAINED BY THE PROPERTY OWNER.
- ALL SANITARY SEWER HOUSE CONNECTION PIPING TO BE DR-18 PIPE.
- NO CONSTRUCTION SHALL COMMENCE UNTIL THE FOLLOWING CONDITIONS ARE MET:
 - THE OWNER, ENGINEER AND CONTRACTOR ATTEND A PRECONSTRUCTION MEETING WITH DEPARTMENT OF PUBLIC WORKS REPRESENTATIVE.
 - AN APPROVAL TO CONSTRUCT TO BE ISSUED BY THE SUFFOLK COUNTY DEPARTMENT OF PUBLIC WORKS (SCDPW) HAVE BEEN RECEIVED.
 - THE GENERAL SUB-CONTRACTOR HAS NOTIFIED THE SCDPW IN WRITING OF HIS INTENTION TO COMMENCE CONSTRUCTION AND SUBMIT AN ACCURATE SCHEDULE OF ANTICIPATED WORK PROGRESS AT LEAST 14 CALENDAR DAYS PRIOR TO ACTUAL COMMENCEMENT DATE. IF FOR ANY REASON CONSTRUCTION IS TEMPORARILY SUSPENDED, A WRITTEN NOTICE OF SAME SHALL BE TRANSMITTED TO THE SCDPW. WRITTEN NOTICE OF RESUMPTION OF CONSTRUCTION TOGETHER WITH A REVISED PROGRESS SCHEDULE SHALL BE TRANSMITTED TO THE SCDPW AT LEAST THREE (3) WORK DAYS PRIOR TO ACTUAL RESUMPTION DATE.
 - ALL NECESSARY APPLICATIONS WITH INSIDE, THE SUFFOLK COUNTY DEPARTMENT OF HEALTH SERVICES, SUFFOLK COUNTY DEPARTMENT OF PUBLIC WORKS, THE LOCAL TOWN AND HAS OBTAINED ALL NECESSARY APPROVALS.

SANITARY TREATMENT PLANT LEACHING POOL (EFFLUENT RECHARGE) CALCULATION

THE PROPOSED SANITARY TREATMENT PLANT AND TREATED EFFLUENT LEACHING FIELD IS SIZED FOR A 100,000 GPD WASTEWATER FLOW.

STP DESIGN FLOW: 100,000 GPD
 SOIL LEACHING RATE (W/ FILTER): 10 GPD/SF SIDEWALL AREA
 REQUIRED LEACHING AREA: 100,000 GPD / 10 GPD/SF = 10,000 SF OF SIDEWALL AREA
 REQUIRED LEACHING POOLS: 10,000 SF / 31.4 SF/WF = 318.47 WF / 12 FT = 26.54

USE (27) - 10'x10' LEACHING POOLS W/ 12' EFFECTIVE DEPTH
 PROVIDED LEACHING POOLS: (26) - 10'x10' LEACHING POOLS W/ 12' EFFECTIVE DEPTH**
 100% FUTURE EXPANSION: (26) - 10'x10' LEACHING POOLS W/ 12' EFFECTIVE DEPTH**

**200% OF REQUIRED LEACHING POOLS (54) PROPOSED TO MEET SCDPW EFFLUENT RECHARGE (DEEP SUB-SURFACE DISPOSAL). ADDITIONAL 50% LEACHING POOL PROPOSED FOR EASE OF CONSTRUCTION.

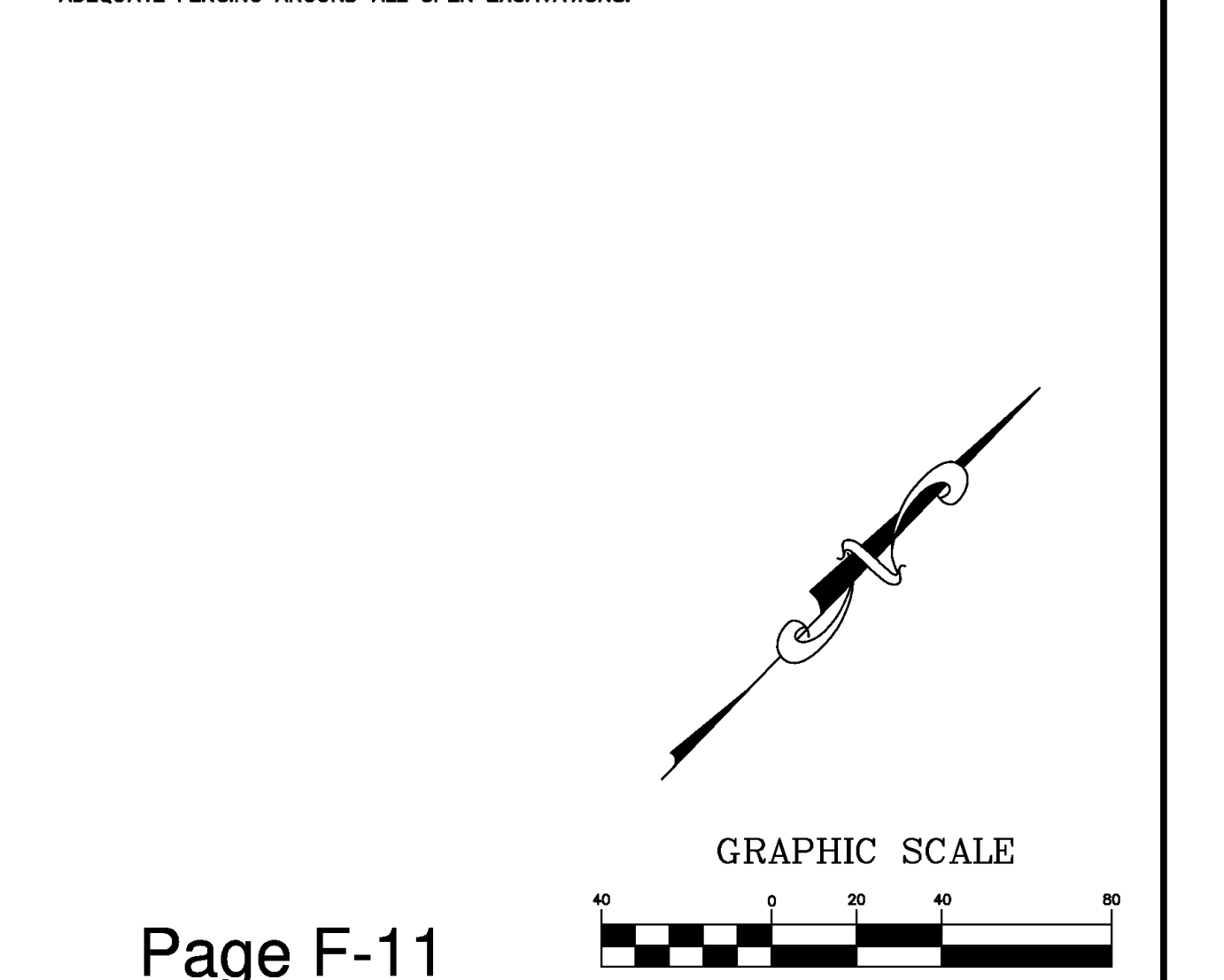
**26 LEACHING POOLS ARE SHOWN FOR FUTURE EXPANSION PLUS ADDITIONAL 50% LEACHING POOL INSTALLED, TO MEET 100% FUTURE EXPANSION POOLS.

SANITARY DENSITY CALCULATIONS

AREA OF SITE: 74.98 AC
 SCDS# (G/W ADMT. ZONE): = 600 GPD/ACRE
 ALLOWABLE DENSITY: 74.98 AC x 600 GPD/ACRE = 44,988 GPD
 PROPOSED DEVELOPED DENSITY:

Lot #	Building #	Structure Use	Quantity/Size	Density Load Rate	Density Flow (gpd)	Kitchen/Gray Load Rate	Kitchen/Gray Flow (gpd)	Hydraulic Load/Wastewater Flow (gpd)
Existing Uses	Bldg A	Existing Industrial Park	7,665 sf	5.00 gpd/occupant	765	2.50 gpd/occupant	383	1,148
		Screenings and other occasional (50 #occupant/153 occupants)	2,130 sf	0.03 gpd/sf	64	2.50 gpd/occupant	178	241
		Industrial (no process water)	10,292 sf	0.04 gpd/sf	412			412
		Rest (Live Store w/ Food)	750 sf	0.10 gpd/sf	23	0.12 gpd/sf	90	113
		Medical Office	28,297 sf	0.10 gpd/sf	2,830			2,830
		Fitness Center w/ showers (Over 5,000 S.F.) (no food service)	15,491 sf	0.10 gpd/sf	1,549	0.20 gpd/sf	3,098	4,647
		Fitness Center (Under 5,000 S.F.) (no food service or showers)	3,469 sf	0.10 gpd/sf	347			347
		Occupancy Existing Vacant Space	4,522 sf	0.10 gpd/sf	452			452
		25% Medical Office	13,588 sf	0.04 gpd/sf	543			543
		75% Industrial (no process water)	27,563 sf	0.04 gpd/sf	1,102			1,102
Bldg. E	Existing Cafeteria (874 seats)	874 seats	5.00 gpd/seat	4,370	2.50 gpd/seat	2,185	6,555	
		1 home	300.00 gpd/home	300			300	
		1 home	300.00 gpd/home	300			300	
Lot 3	Bldg. A	Non-Medical Office (Building 1)	21,850 sf	0.06 gpd/sf	1,313		1,313	
		Total Flow of Existing Uses (gpd)			12,217		6,373	20,592
Proposed Uses	Lot 4	Tech/Medical Office	87,500 sf	0.10 gpd/sf	8,750		8,750	
		Non-Medical Office	46,010 sf	0.06 gpd/sf	2,761		2,761	
		Hotel	125 rooms (400 sf grk w/ kitchenette)	18,750	150 gpd/room	18,750		18,750
Lot 5	4,000 Conference Space (133 seats, no food service)	133 seats	3.00 gpd/seat	399		399		
		Assisted Living	250 bed	110 gpd/bed	27,500		27,500	
Lot 6	Bldg. A	Total Flow of Proposed Uses (gpd)		46,449		0	59,336	
		Total Projected Flow (gpd):			58,666		79,928	
		Add 10% buffer:					7,993	
		Total Design Flow (gpd):					87,921	

- IF THE SEWER LINE OR HOUSE CONNECTION DOES NOT END INTO A MANHOLE, THE TAPE SHALL BE EXTENDED AT LEAST THREE (3) FEET BEYOND THE PLUGGED END OF THE LINE.
- THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION AND ELEVATION OF EXISTING UTILITIES (INCLUDING EXISTING SEWERS AND SADDLES) BY FIELD INVESTIGATION. THE CONTRACTOR IS RESPONSIBLE TO DETERMINE THE ACCURACY OF ALL EXISTING SEWERS TO ACCEPT CONNECTION OF THE PROPOSED WORK. THE ENGINEER SHALL BE NOTIFIED OF ANY CONFLICTS BETWEEN EXISTING UTILITIES AND ANY INADEQUACY OF THE EXISTING SEWER LINES TO PROPERLY ACCEPT PROPOSED CONNECTIONS AND FLOWS.
- IT IS THE RESPONSIBILITY OF THE GENERAL SUB-CONTRACTOR TO RESTORE ALL AREAS DISTURBED BY HIS WORK TO THE CONDITION AS BEFORE THE WORK COMMENCED.
- THE GENERAL SUB-CONTRACTOR(S) SHALL BE RESPONSIBLE FOR MAINTAINING THE CONSTRUCTION SITE IN A NON-HAZARDOUS CONDITION ALL IN ACCORDANCE WITH THE REQUIREMENTS OF THE NEW YORK STATE DEPARTMENT OF LABOR CODES AND ALL OTHER AGENCIES HAVING JURISDICTION. UNLESS OTHERWISE APPROVED IN WRITING BY SCDPW, THE CONTRACTOR SHALL ERECT AND MAINTAIN ADEQUATE FENCING AROUND ALL OPEN EXCAVATIONS.



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PROJECT NAME: **MAP OF FLOWERFIELD FINAL ENGINEERING**

PROJECT FOR: **1 FLOWERFIELD DRIVE ST. JAMES, NY 11780**

TITLE: **UTILITY PLAN (1)**

DISCIPLINE: **CIVIL**

PROJECT ENGINEER: **CE0338A**

DRAWING NO.: **C-8**

DATE: **APRIL 2020**

SHEET: **9 OF 22**

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1175 Thompson Park Avenue, Bldg. 1000
 1411 Huntington Park, New York, NY 11780
 1175 Thompson Park Avenue, Bldg. 1000
 1411 Huntington Park, New York, NY 11780

PROJECT ENGINEER: **CE0338A**

DRAWN BY: **CE0338A**

CHECKED BY: **AS SHOWN**

FINAL
Environmental
Impact Statement

For the:

Gyrodyne, LLC

**Map of Flowerfield
Subdivision Application**

Hamlet of St. James, Town of Smithtown
Suffolk County, New York

VOLUME 3 OF 4

December 2020



Cameron Engineering & Associates, LLP

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VOLUME 3 OF 4

Appendix F: Final Engineering Plans

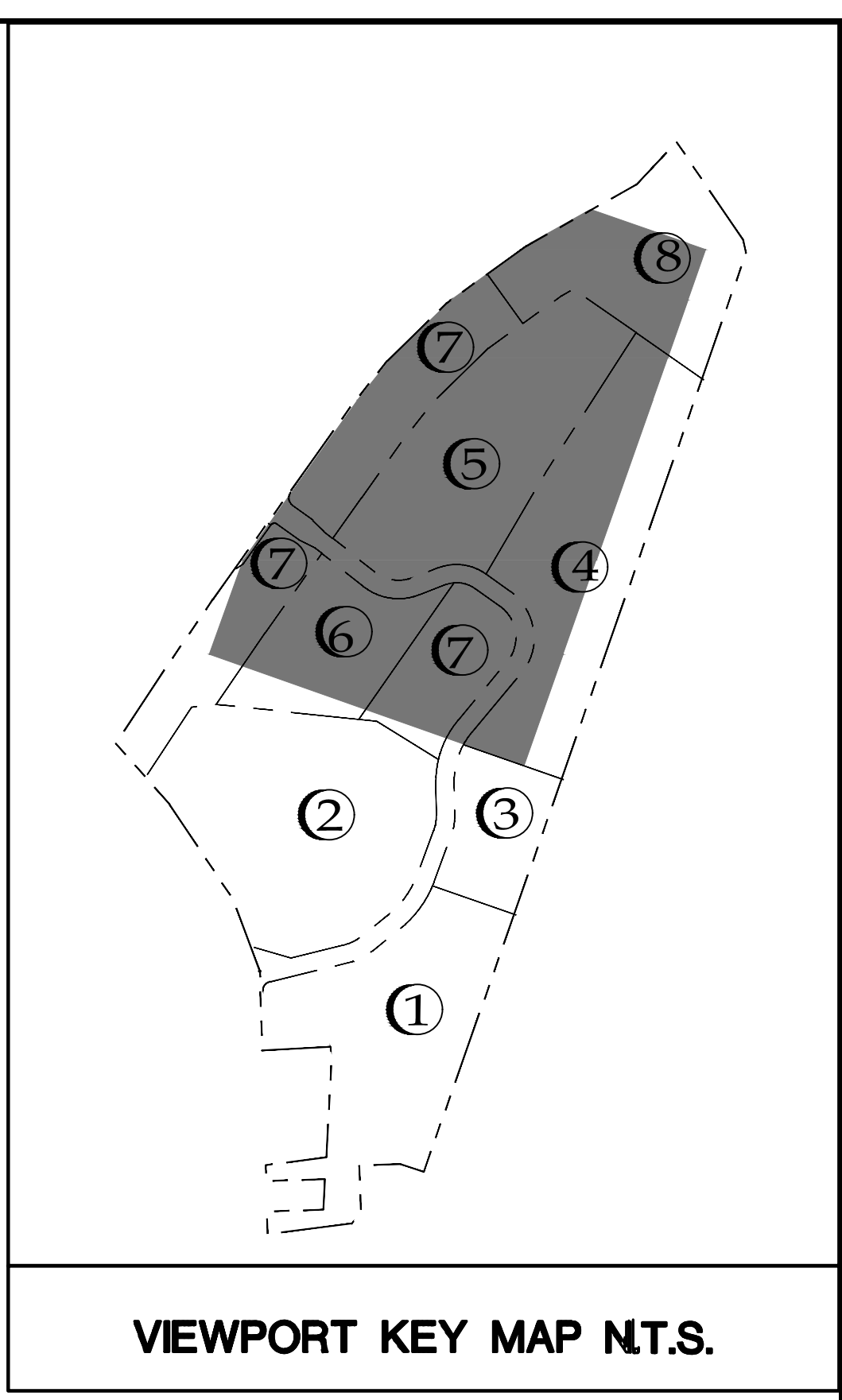
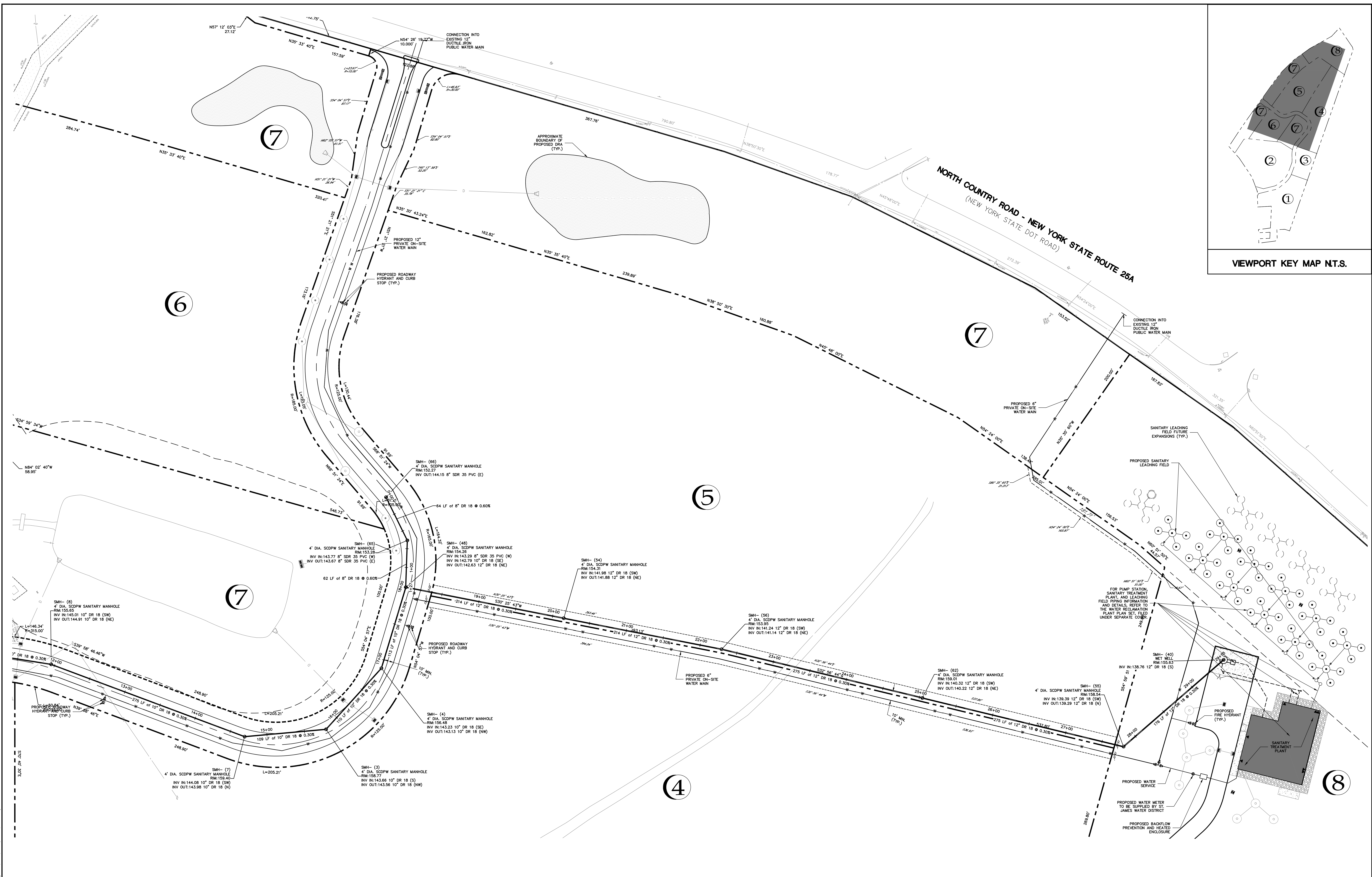
Sheets C-9 through C-21

*Final Environmental Impact Statement
Map of Flowerfield Subdivision Application*

December 2020

Appendix F: Final Engineering Plans
(continued)

- C-9 Utility Plan (2)
- C-10 Erosion and Sediment Control Plan (1)
- C-11 Erosion and Sediment Control Plan (2)
- C-12 Road Plan & Profile Road A: STA 10+00 to 24+9.00
- C-13 Road Plan & Profile Road A: STA 24+9.00 to 35+78.03
- C-14 Sanitary Sewer Profiles (1)
- C-15 Sanitary Sewer Profiles (2)
- C-16 Mills Pond Driveway Pavement Marking Plan
- C-17 Tree Preservation and Land Clearing Plan (1)
- C-18 Tree Preservation and Land Clearing Plan (2)
- C-19 Landscape Plan
- C-20 Detail
- C-21 Details (2)



VIEWPORT KEY MAP N.T.S.

LEGEND (PROPOSED)

OVERALL PROPERTY LINE	---
SUBDIVISION LOT LINE	---
WATER MAIN/SERVICE	---
FIRE SERVICE	---
SANITARY SEWER MAIN	---
UTILITY POLE	⊕
SANITARY MANHOLE	⊕
UTILITY CROSSING NUMBER	⊕
FIRE HYDRANT	⊕
WATER VALVE	⊕
WATER METER PIT	⊕

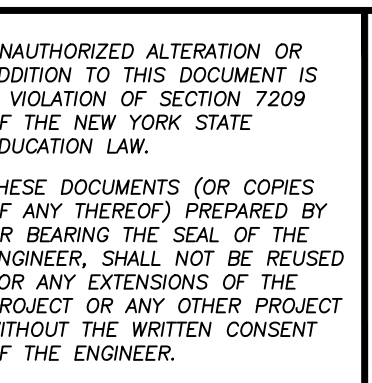
LEGEND (EXISTING)

A.C. UNIT	⊕
OVERHEAD WIRES	---
UTILITY POLE	⊕
SOFT WIRE	---
SOFT	---
DRAINAGE INLET	⊕
LIGHT POLE	⊕
HYDRANT	⊕
WATER VALVE	⊕
WATER METER	⊕
IRRIGATION CONTROL VALVE	⊕
ELECTRIC MANHOLE	⊕
ELECTRIC BOX	⊕
TELEPHONE MANHOLE	⊕
TELEPHONE BOX	⊕
CABLE BOX	⊕
MANHOLE	⊕
DRAIN MANHOLE	⊕
SEWER MANHOLE	⊕

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1-800-627-6888
 1-212-224-8888
 1-914-751-4888
 516-339-0212

PROJECT NAME:
MAP OF FLOWERFIELD FINAL ENGINEERING

PROJECT FOR:
1 FLOWERFIELD DRIVE ST. JAMES, NY 11780

TITLE:
UTILITY PLAN (2)

DISCIPLINE:
CIVIL

PROJECT ENGINEER:
 OC
 DESIGNED BY:
 OC
 DRAWN BY:
 OC
 CHECKED BY:
 RZ

PROJECT NO.
CE0338A

JOB NO.
CE0338A

DATE:
APRIL 2020

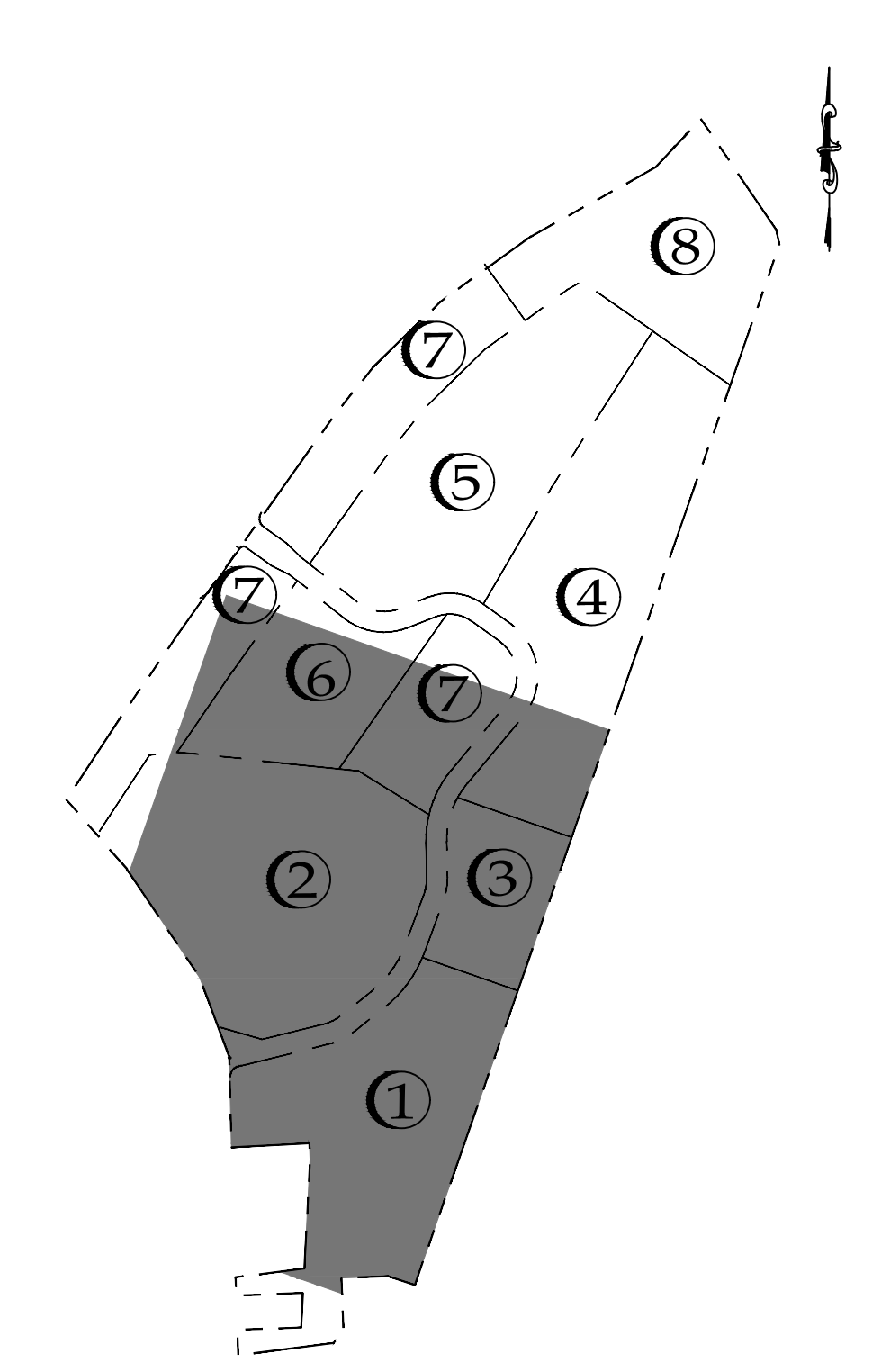
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GRAPHIC SCALE
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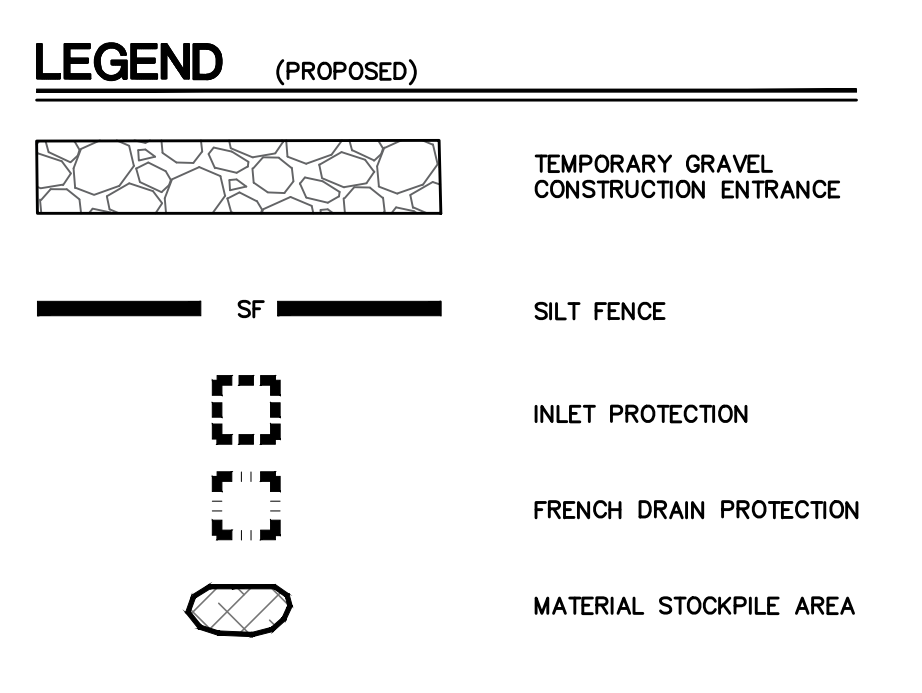
C-9
 SHEET 10 of 22

GRADING NOTES
ALL 1:2 & 1:3 SLOPE AREAS WILL BE PROTECTED AGAINST EROSION DURING CONSTRUCTION AND PERMANENT GROUND COVER SHALL BE SUCH THAT EROSION WILL BE PREVENTED. NECESSARY MEASURES SHALL INCLUDE, BUT NOT BE LIMITED TO, HAY BALES, SILT FENCES, SILT TRAPS/BASINS, JUTE MESH, HYDROSEEDING, ETC. AND SHALL BE MAINTAINED FOR THE DURATION OF CONSTRUCTION AS WELL AS FOLLOWING THE COMPLETION OF CONSTRUCTION UNTIL SUCH TIME THAT THE PROPOSED PLANTINGS HAVE BECOME ACCUMULATED/ESTABLISHED.

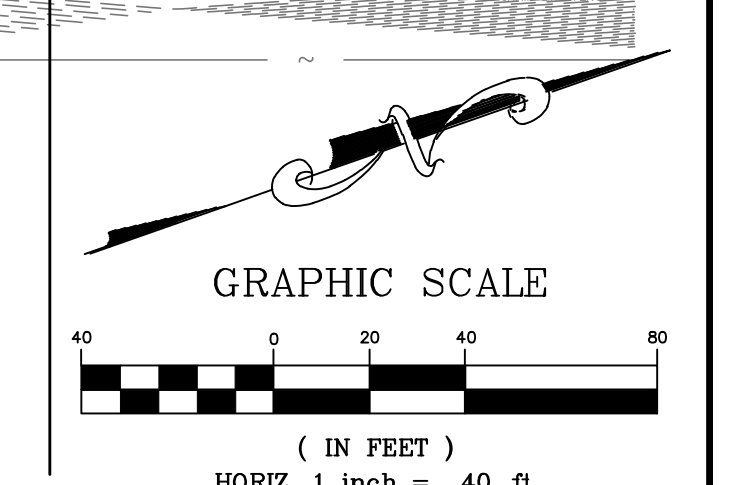
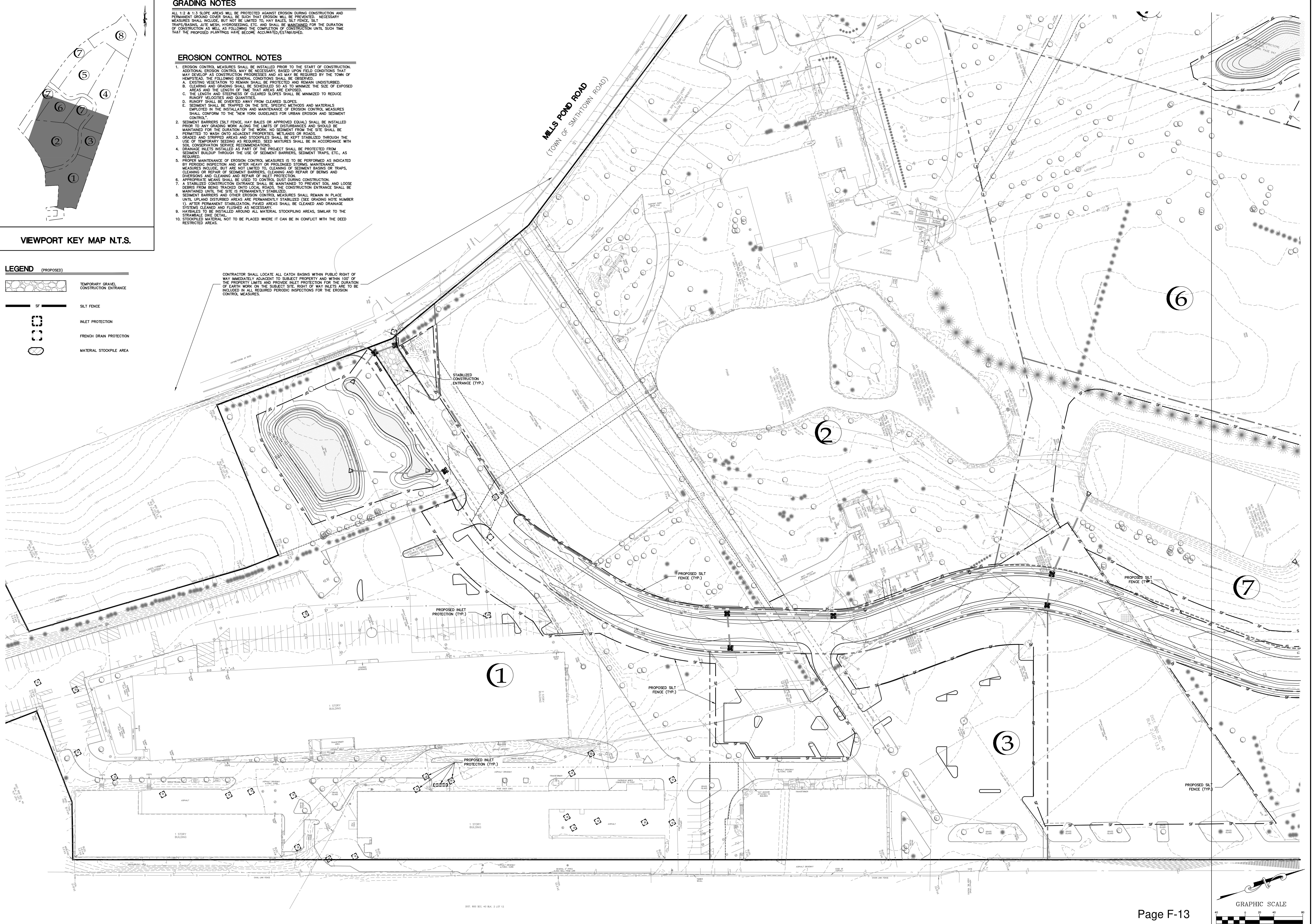
- EROSION CONTROL NOTES**
1. EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE START OF CONSTRUCTION. ADDITIONAL EROSION CONTROL MAY BE NECESSARY BASED UPON FIELD CONDITIONS THAT MAY DEVELOP AS CONSTRUCTION PROGRESSES AND AS MAY BE REQUIRED BY THE TOWN OF HEMPSTEAD. THE FOLLOWING GENERAL CONDITIONS SHALL BE OBSERVED:
 - A. EXISTING VEGETATION TO REMAIN SHALL BE PROTECTED AND REMAIN UNDISTURBED.
 - B. CLEARING AND GRADING SHALL BE SCHEDULED SO AS TO MINIMIZE THE SIZE OF EXPOSED AREAS AND THE LENGTH OF TIME THAT AREAS ARE EXPOSED.
 - C. THE LENGTH AND STEEPNESS OF CLEARED SLOPES SHALL BE MINIMIZED TO REDUCE RUNOFF VELOCITIES AND QUANTITIES.
 - D. SEDIMENT SHALL BE DIVERTED AWAY FROM CLEARED SLOPES.
 - E. SEDIMENT SHALL BE TRAPPED ON THE SITE. SPECIFIC METHODS AND MATERIALS EMPLOYED IN THE INSTALLATION AND MAINTENANCE OF EROSION CONTROL MEASURES SHALL CONFORM TO THE 'NEW YORK GUIDELINES FOR URBAN EROSION AND SEDIMENT CONTROL'.
 2. SEDIMENT BARRIERS (SILT FENCE, HAY BALES OR APPROVED EQUAL) SHALL BE INSTALLED PRIOR TO ANY GRADING WORK ALONG THE LIMITS OF DISTURBANCES AND SHOULD BE MAINTAINED FOR THE DURATION OF THE WORK. NO SEDIMENT FROM THE SITE SHALL BE PERMITTED TO WASH ONTO ADJACENT PROPERTIES, WETLANDS OR ROADS.
 3. GRAZED AND STOCKED AREAS AND STOCKPILES SHALL BE KEPT STABILIZED THROUGH THE USE OF TEMPORARY SEEDING AS REQUIRED. SEED MIXTURES SHALL BE IN ACCORDANCE WITH SOIL CONSERVATION SERVICE RECOMMENDATIONS.
 4. DRAINAGE INLETS INSTALLED AS PART OF THE PROJECT SHALL BE PROTECTED FROM SEDIMENT BUILDUP THROUGH THE USE OF SEDIMENT BARRIERS, SEDIMENT TRAPS, ETC., AS REQUIRED.
 5. PROPER MAINTENANCE OF EROSION CONTROL MEASURES IS TO BE PERFORMED AS INDICATED BY PERIODIC INSPECTION AND AFTER HEAVY OR PROLONGED STORMS. MAINTENANCE MEASURES INCLUDE, BUT ARE NOT LIMITED TO, CLEANING OF SEDIMENT BASINS OR TRAPS, CLEANING OR REPAIR OF SEDIMENT BARRIERS, CLEANING AND REPAIR OF BERMS AND OVERSEEDING AND CLEANING AND REPAIR OF INLET PROTECTION.
 6. APPROPRIATE MEANS SHALL BE USED TO CONTROL DUST DURING CONSTRUCTION.
 7. A STABILIZED CONSTRUCTION ENTRANCE SHALL BE MAINTAINED TO PREVENT SOIL AND LOOSE BERMS FROM BEING TRACKED ONTO LOCAL ROADS. THE CONSTRUCTION ENTRANCE SHALL BE MAINTAINED UNTIL THE SITE IS PERMANENTLY STABILIZED.
 8. SEDIMENT BARRIERS AND OTHER EROSION CONTROL MEASURES SHALL REMAIN IN PLACE UNTIL UP AND DISTURBED AREAS ARE PERMANENTLY STABILIZED (SEE GRADING NOTE NUMBER 1). AFTER PERMANENT STABILIZATION, PAVED AREAS SHALL BE CLEANED AND DRAINAGE SYSTEMS CLEANED AND FLOWED AS NECESSARY.
 9. HAYBALES TO BE INSTALLED AROUND ALL MATERIAL STOCKPILING AREAS, SIMILAR TO THE STRANBLE DICE DETAIL.
 10. STOCKPILED MATERIAL NOT TO BE PLACED WHERE IT CAN BE IN CONFLICT WITH THE DEED RESTRICTED AREAS.



VIEWPORT KEY MAP N.T.S.



CONTRACTOR SHALL LOCATE ALL CATCH BASINS WITHIN PUBLIC RIGHT OF WAY IMMEDIATELY ADJACENT TO SUBJECT PROPERTY AND WITHIN 100' OF THE PROPERTY LIMITS AND PROVIDE INLET PROTECTION FOR THE DURATION OF EARTH WORK ON THE SUBJECT SITE. RIGHT OF WAY INLETS ARE TO BE INCLUDED IN ALL REQUIRED PERIODIC INSPECTIONS FOR THE EROSION CONTROL MEASURES.



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PROJECT NAME:
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PROJECT FOR:
1 FLOWERFIELD DRIVE ST. JAMES, NY 11780

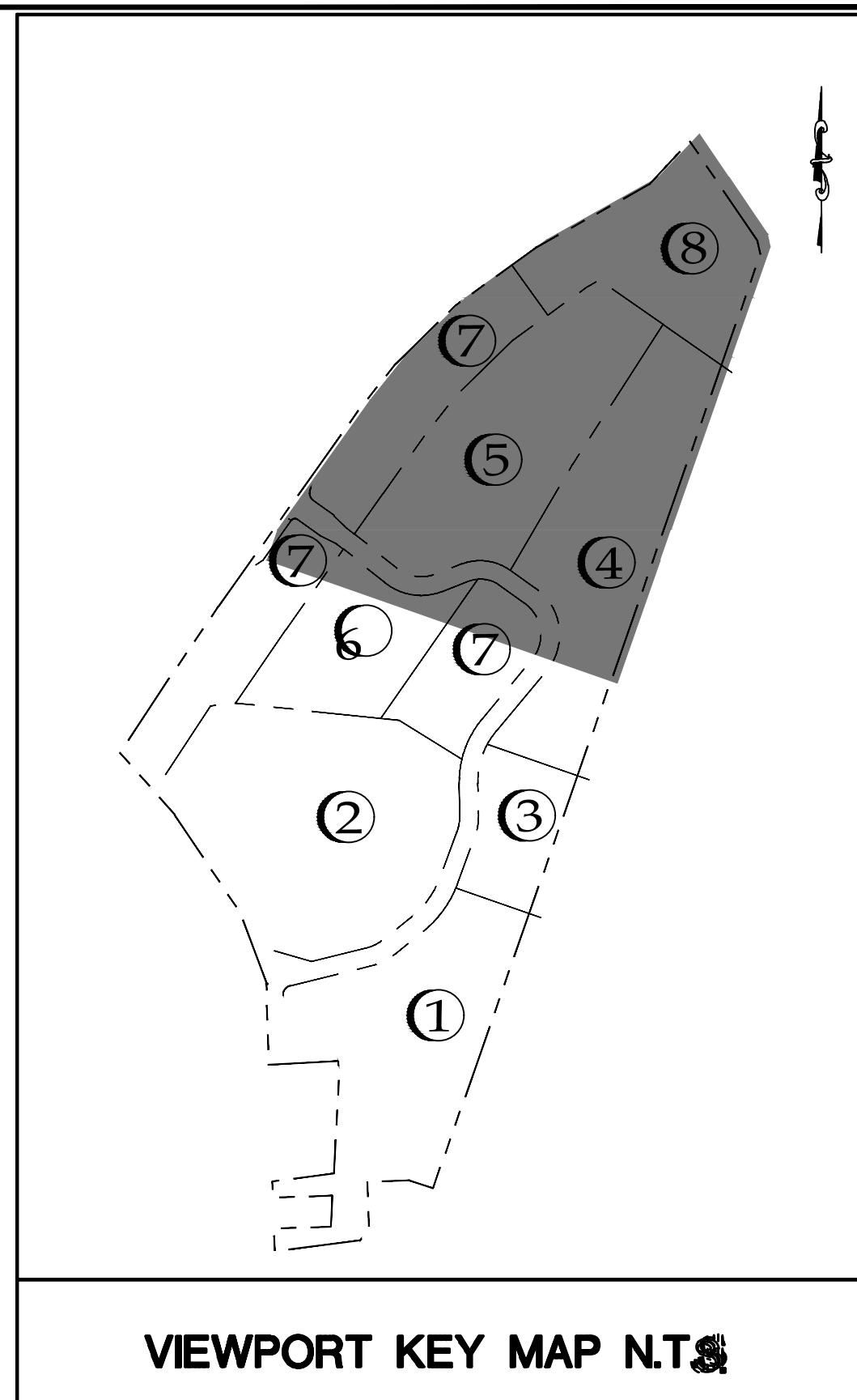
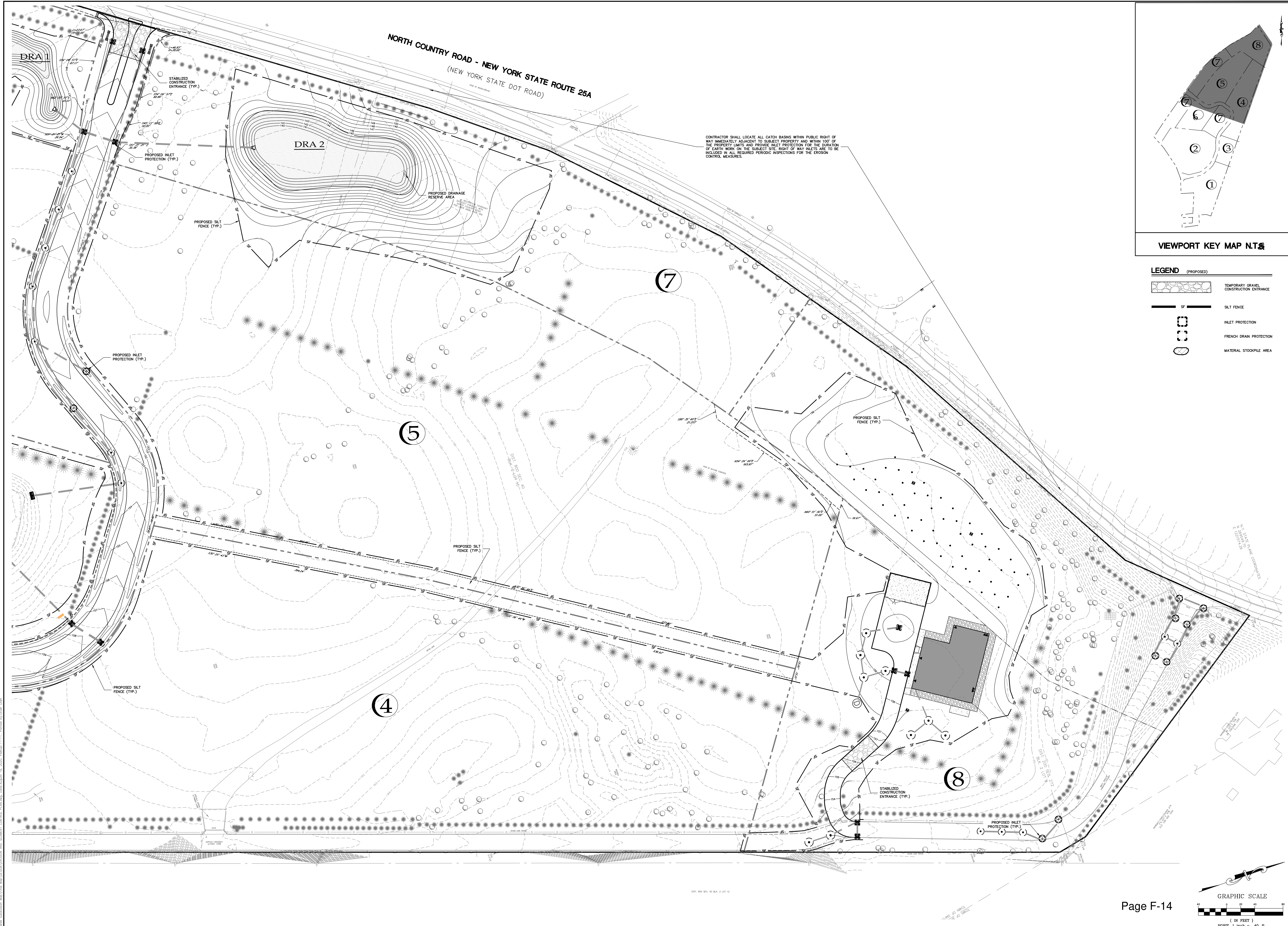
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DRAWN BY: OC	JOB NO.: CE0338A
CHECKED BY: RZ	DATE: APRIL 2020
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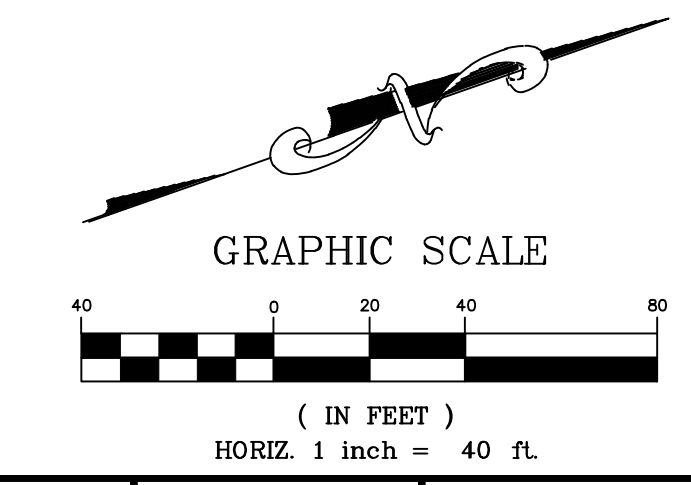
C-10

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LEGEND (PROPOSED)

	TEMPORARY GRAVEL CONSTRUCTION ENTRANCE
	SILT FENCE
	INLET PROTECTION
	FRENCH DRAIN PROTECTION
	MATERIAL STOCKPILE AREA



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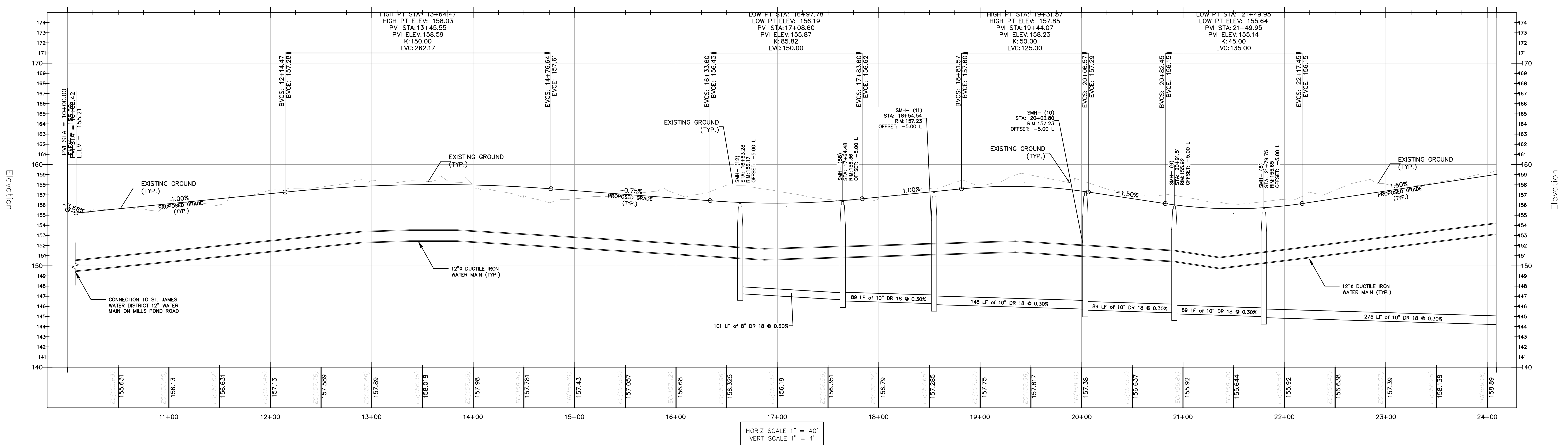
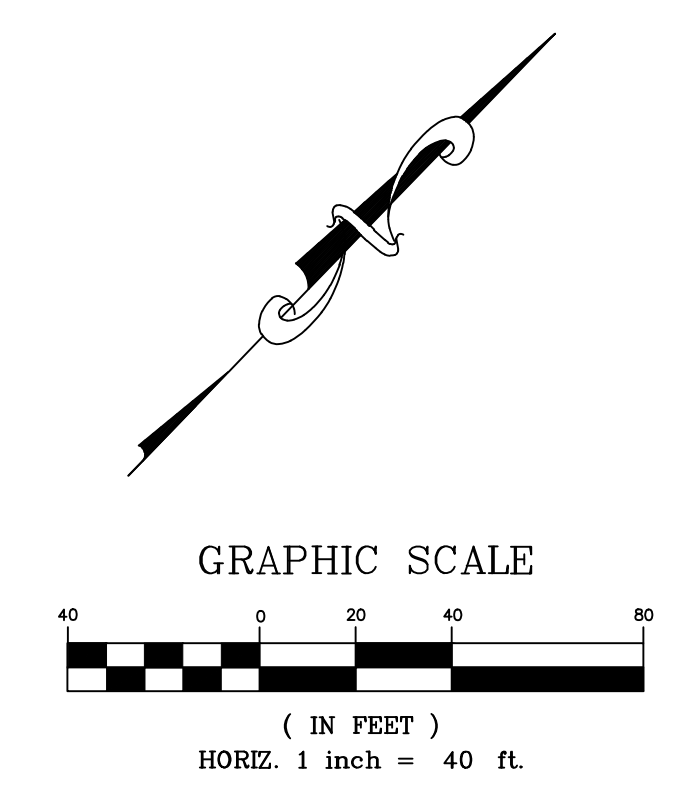
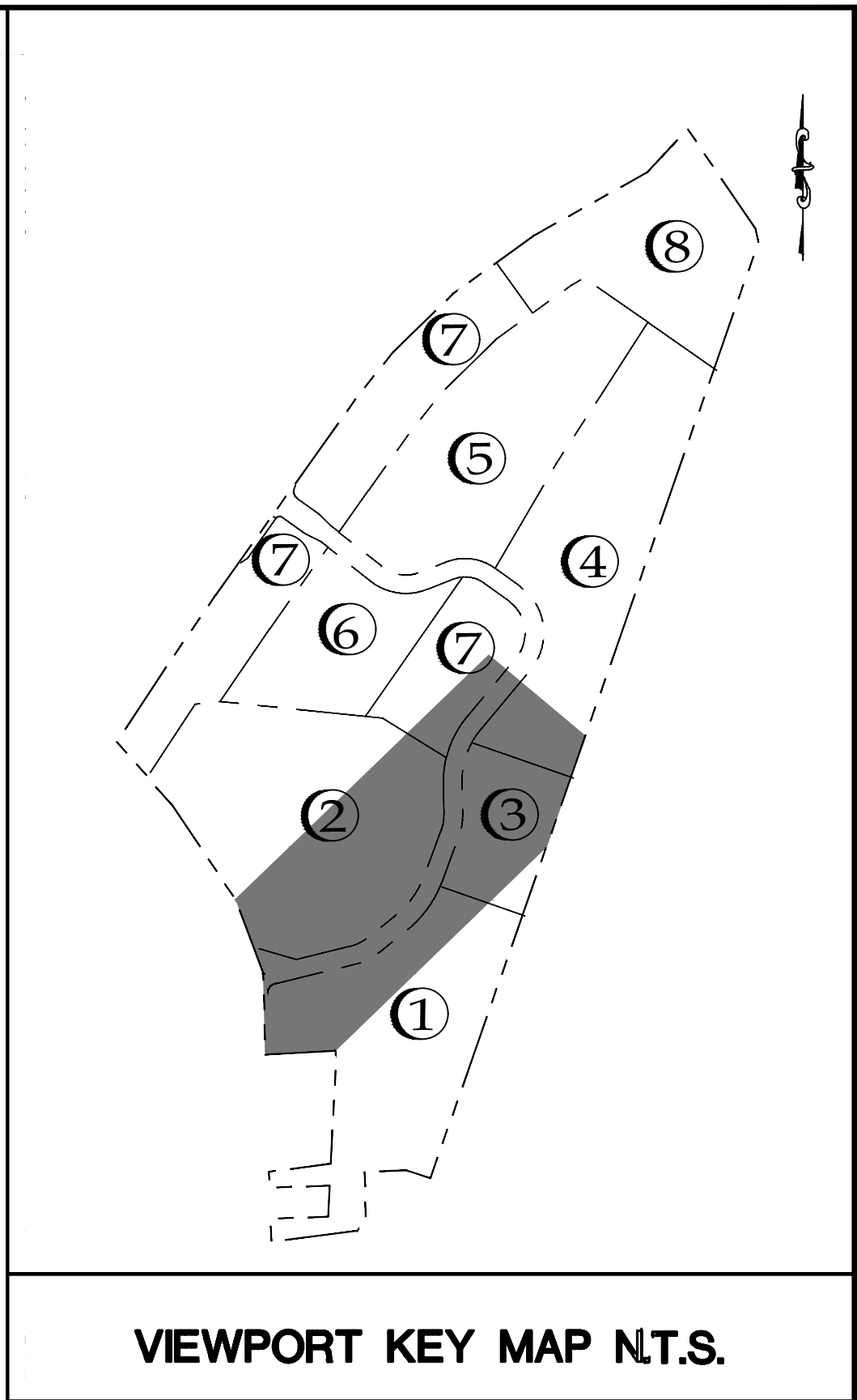
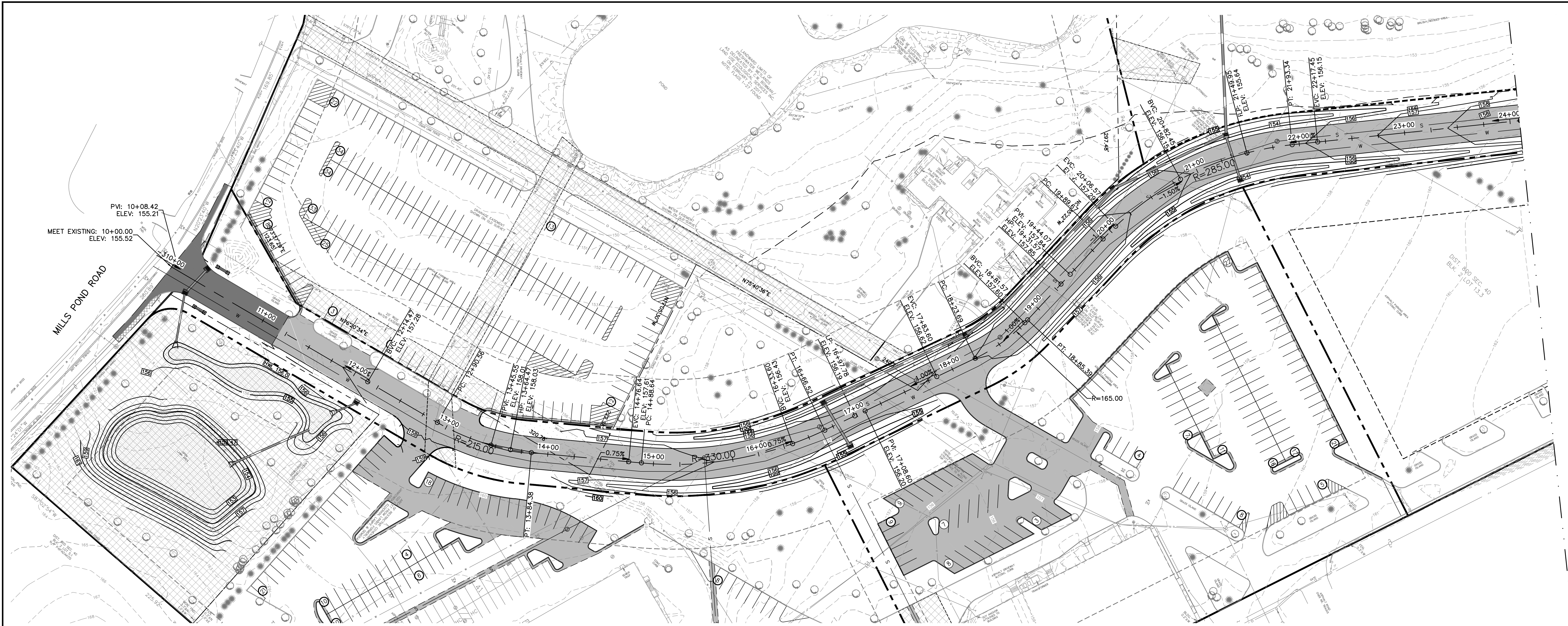
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TITLE:
ROAD PLAN & PROFILE ROAD A - STA 10+00.00 TO STA 24+09.00

DISCIPLINE:
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PROJECT ENGINEER:
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DESIGNED BY:
 OC

DRAWN BY:
 OC

CHECKED BY:
 RZ

PROJECT NO.:
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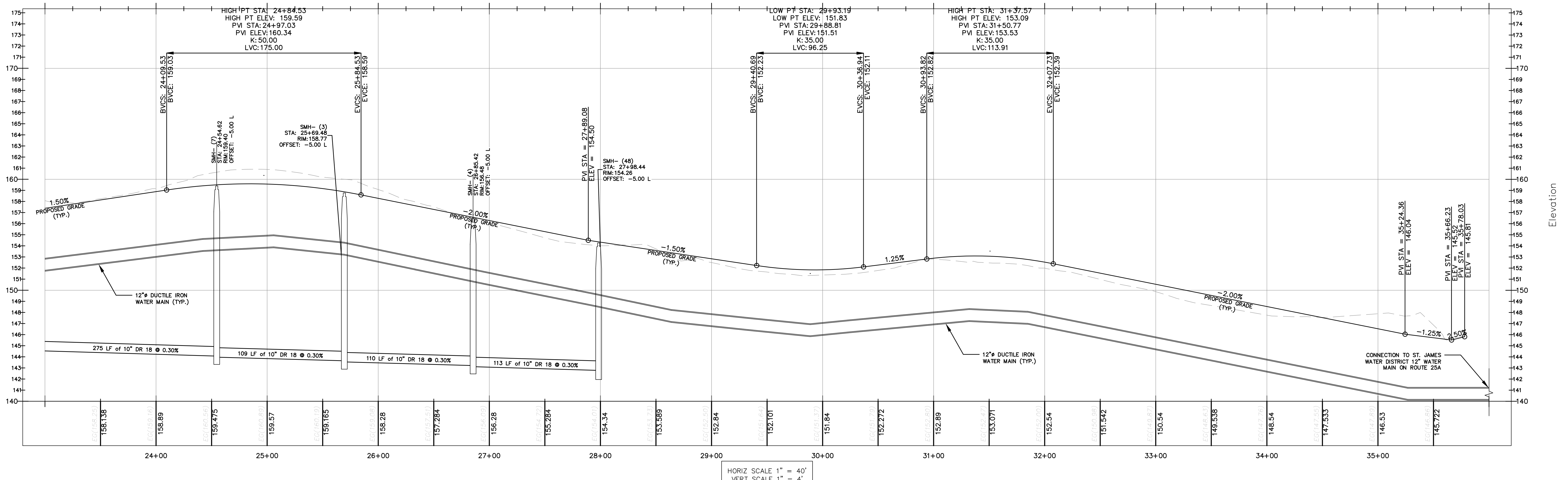
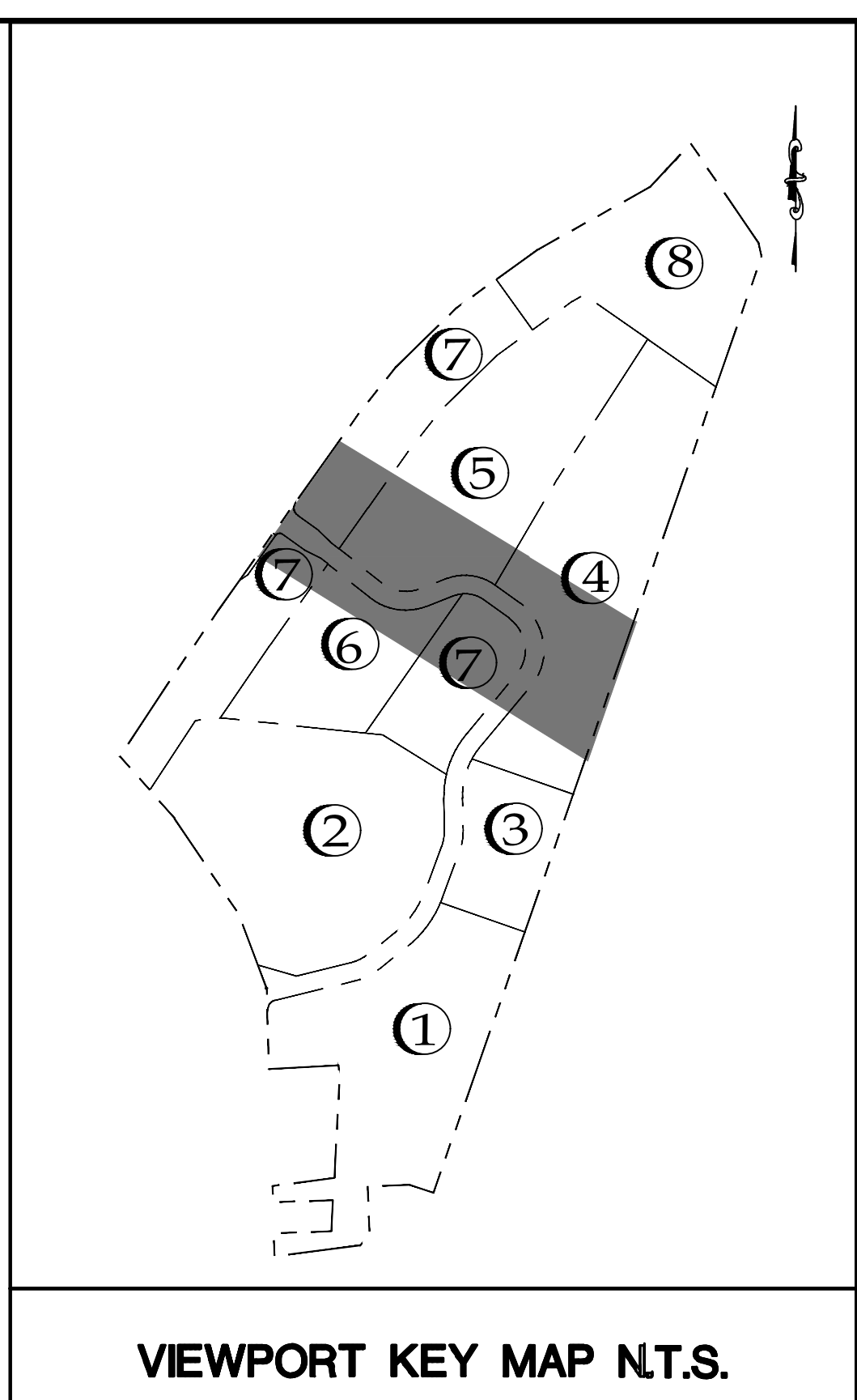
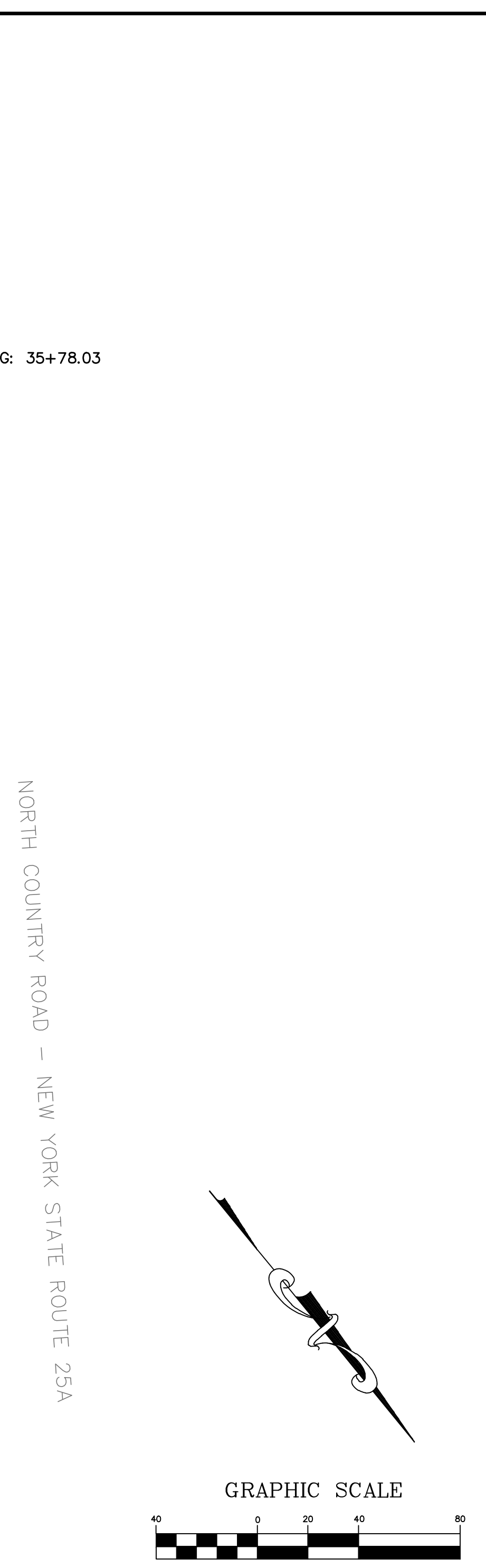
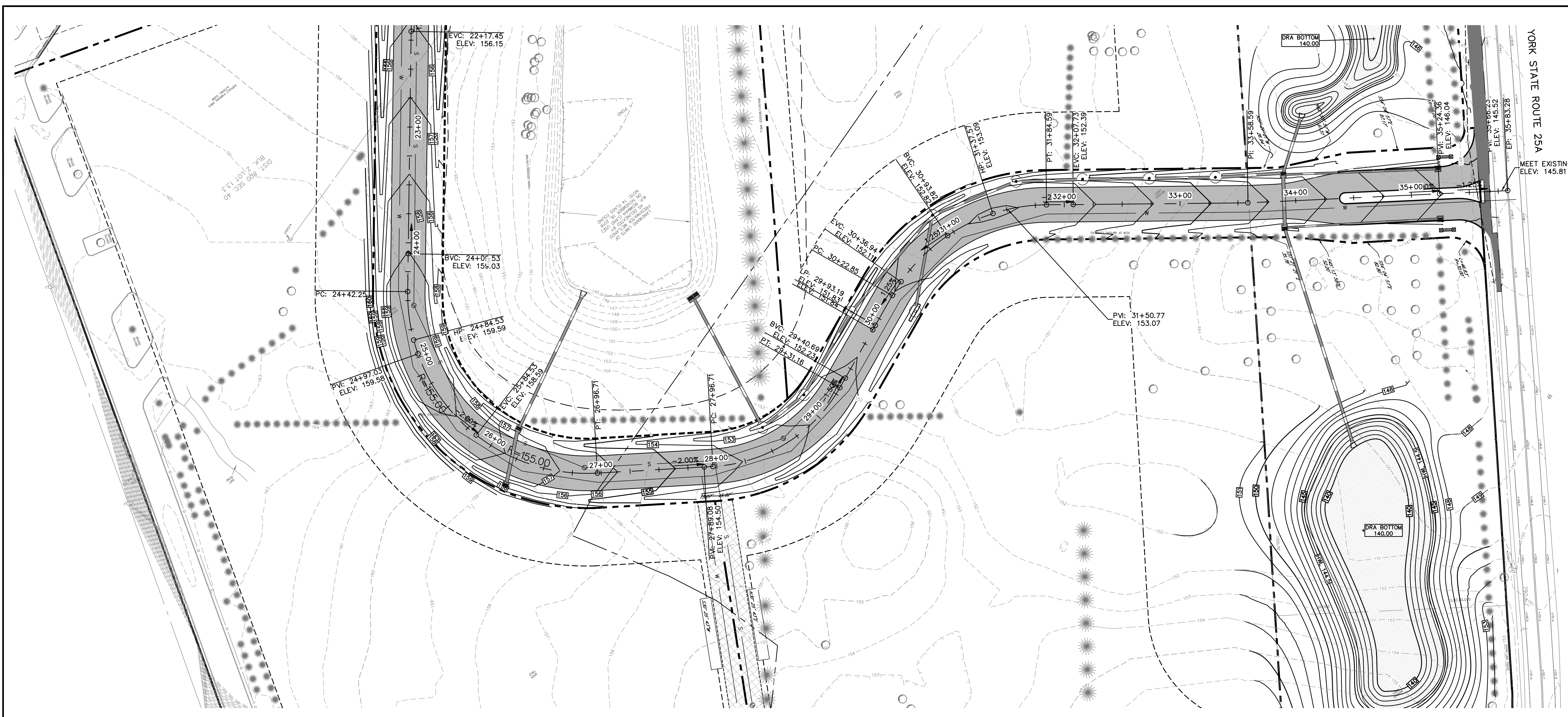
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HORIZ SCALE 1" = 40'
 VERT SCALE 1" = 4'

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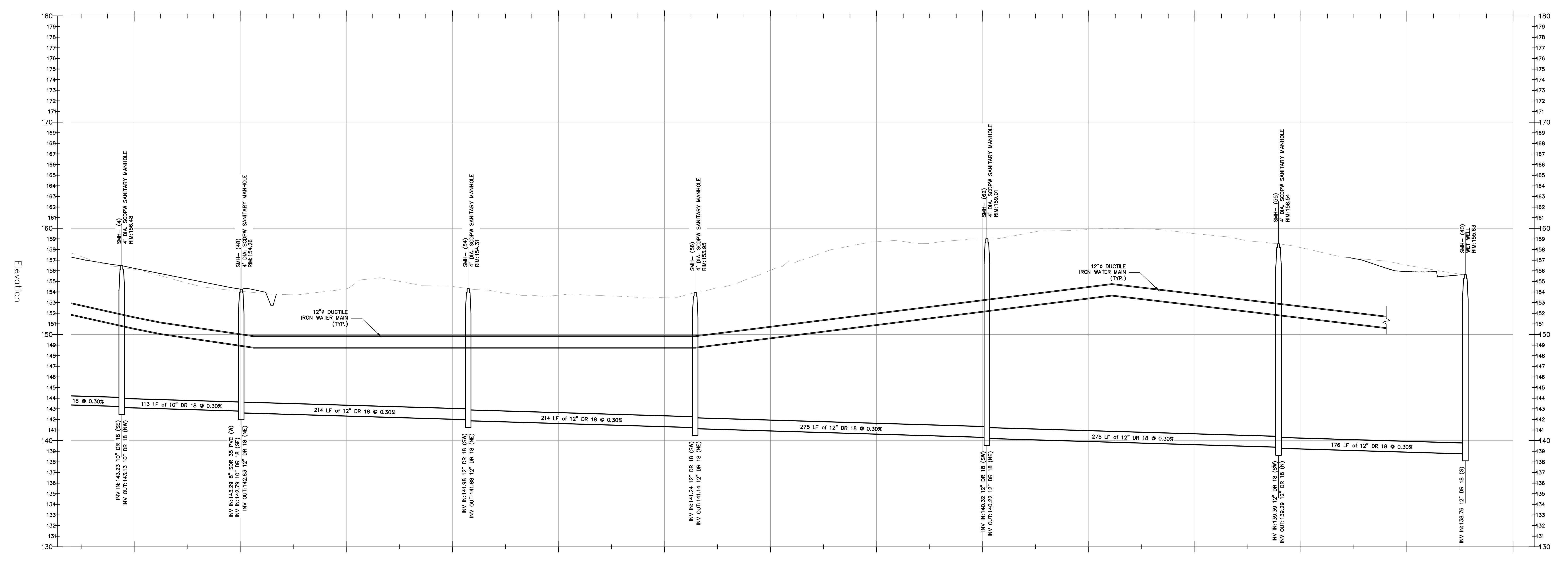
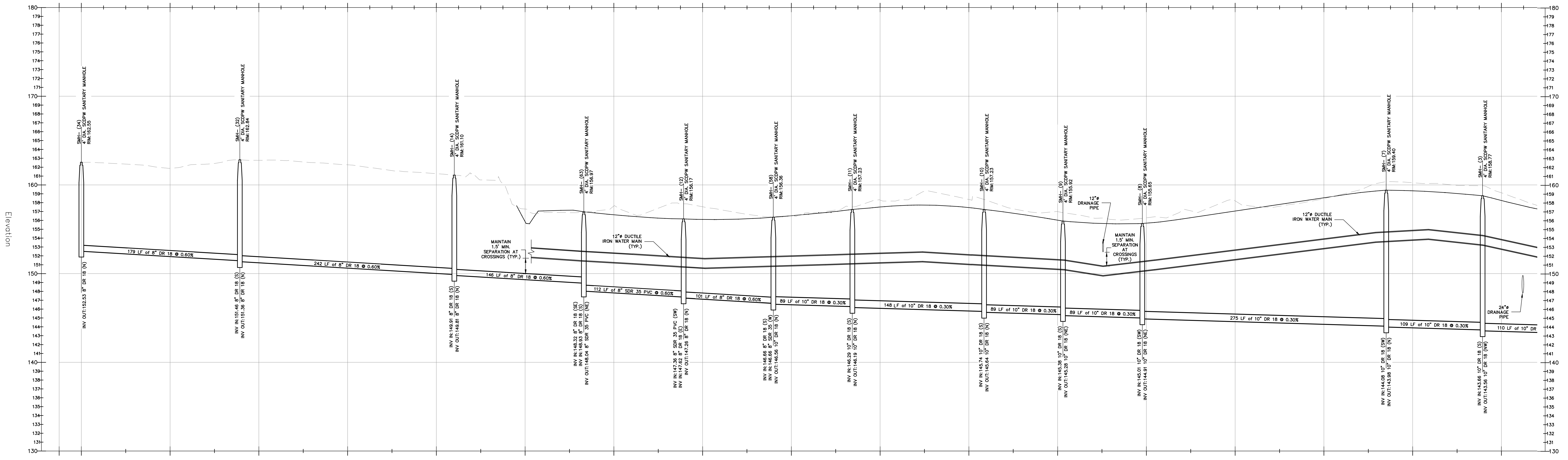
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PROJECT ENGINEER: OC
 DESIGNED BY: OC
 DRAWN BY: OC
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 JOB NO.: CE0338A
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 SHEET 14 OF 22



MAIN SEWER PROFILE

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VER. SCALE 1" = 4'

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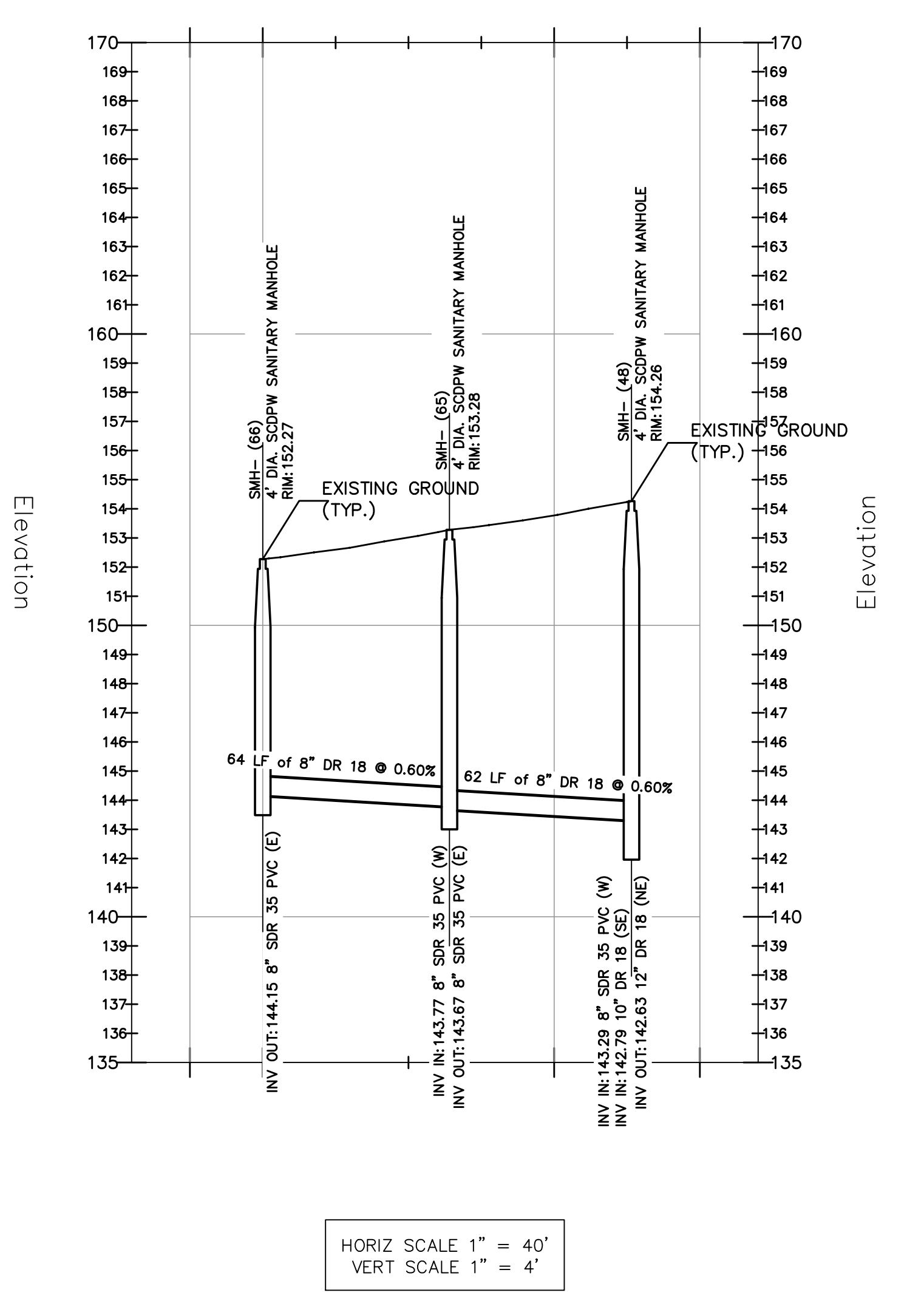
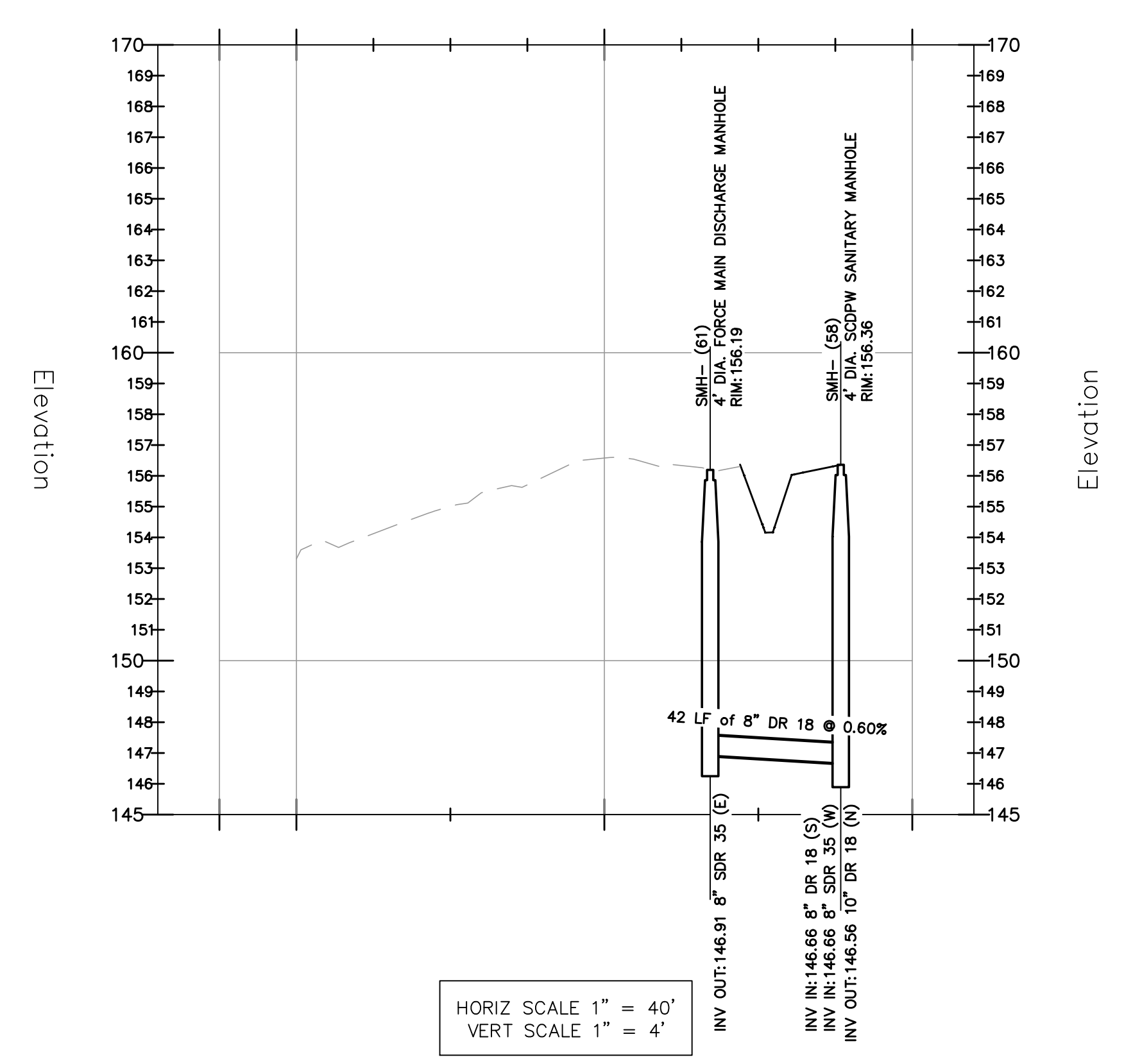
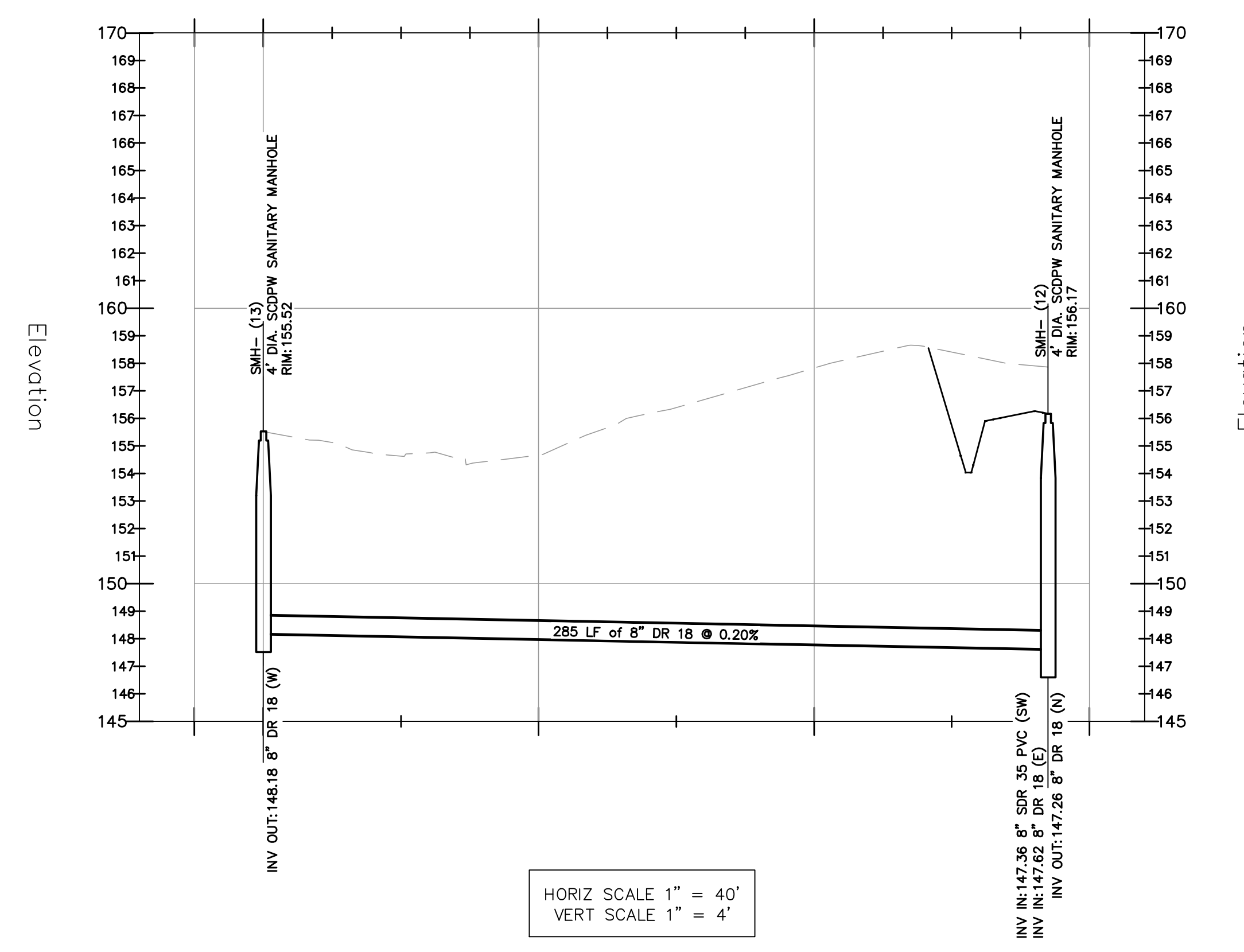
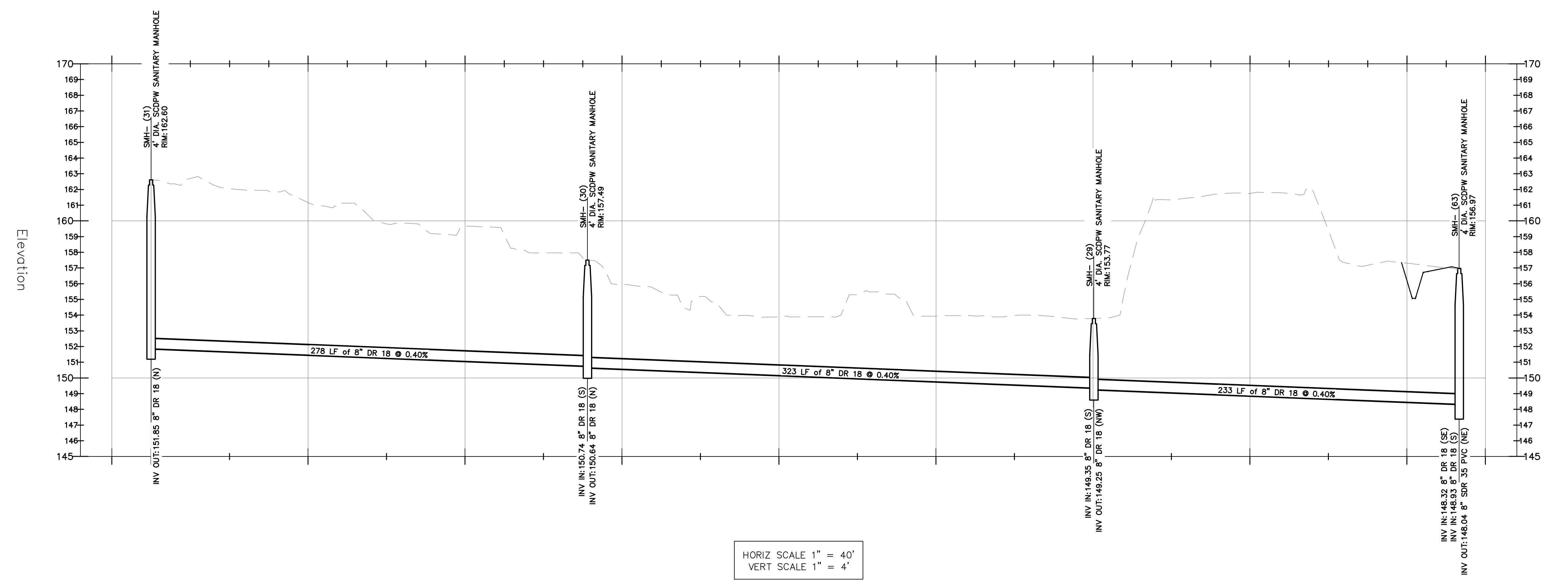
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MAP OF FLOWERFIELD FINAL ENGINEERING
 PROJECT FOR:
1 FLOWERFIELD DRIVE ST. JAMES, NY 11780

TITLE:
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 DISCIPLINE:
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PROJECT ENGINEER: OC
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PROJECT NO.: CE0338A
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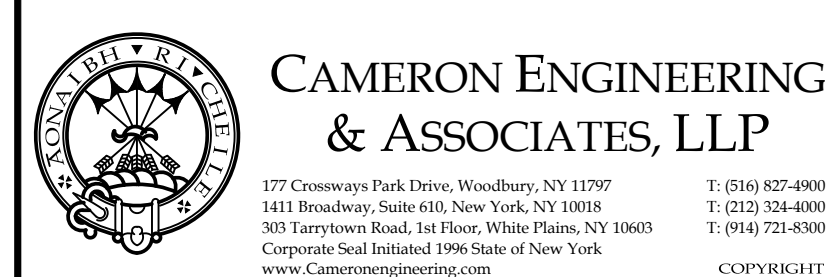
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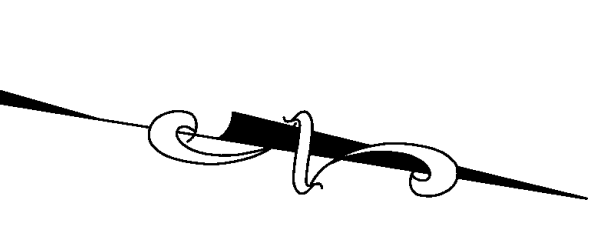
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MAP OF FLOWERFIELD FINAL ENGINEERING
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1 FLOWERFIELD DRIVE ST. JAMES, NY 11780

TITLE:
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PROJECT ENGINEER:
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 OC
 CHECKED BY:
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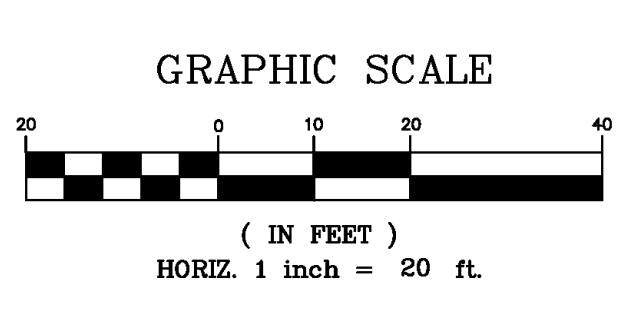
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 JOB NO.
 CE0338A
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C-15
 SHEET
 16 of 22



LEGEND

	PROPOSED SIGN
	CURB TO BE REMOVED
	PROPOSED CURB
	ASPHALT
	TOPSOIL AND SOD
	SAWDUST



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PROJECT FOR:
1 FLOWERFIELD DRIVE ST. JAMES, NY 11780

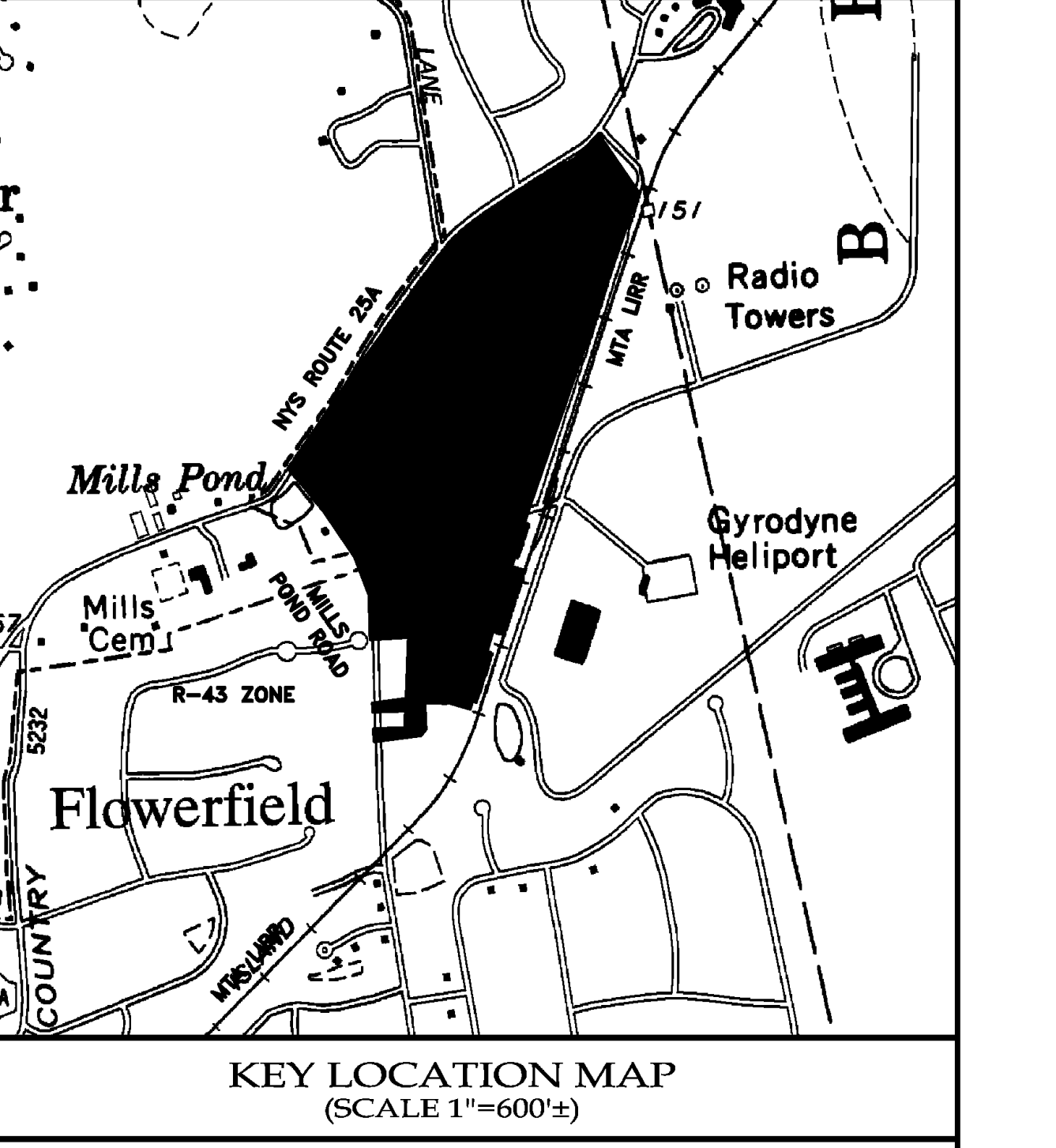
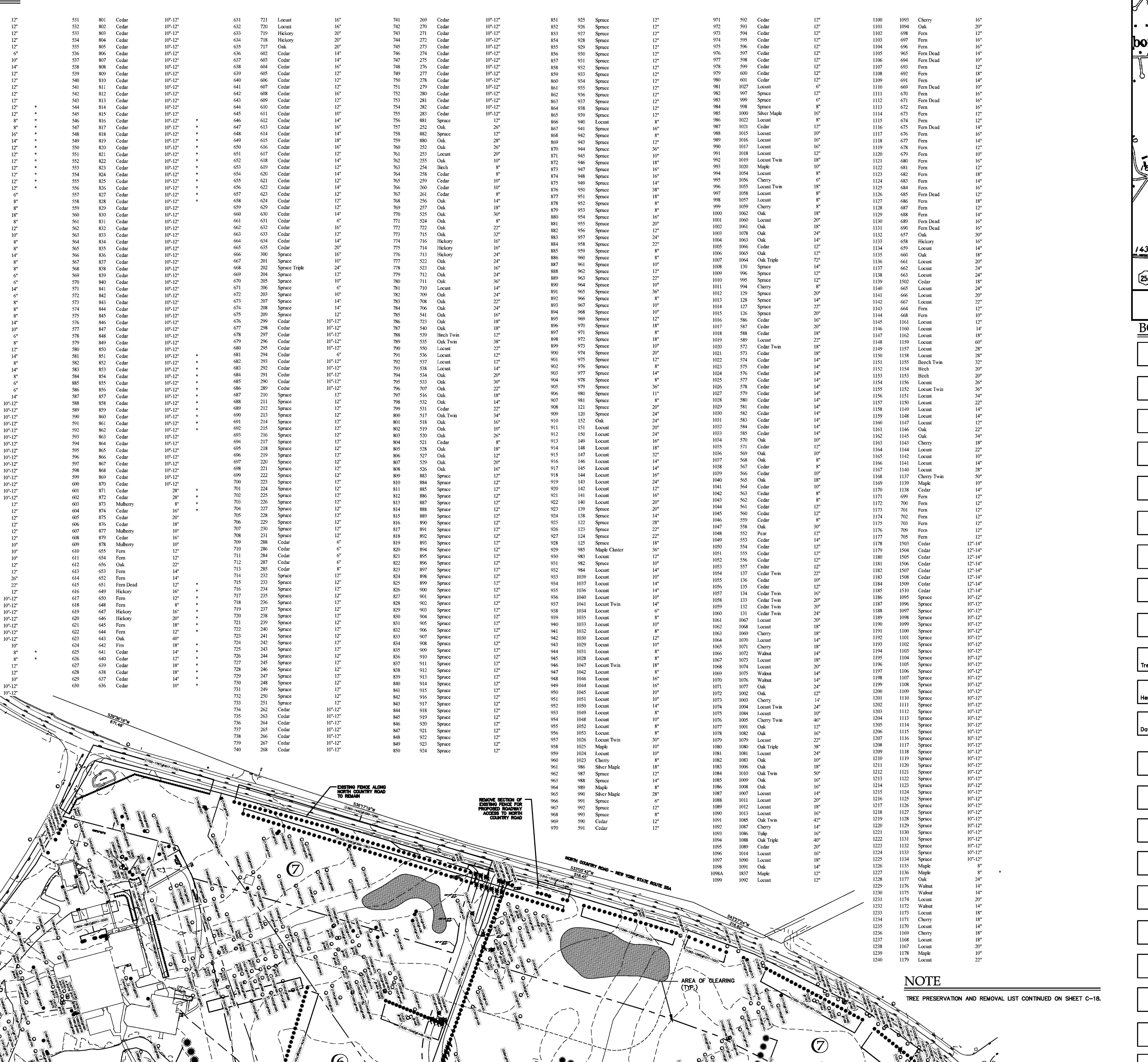
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MILLS POND DRIVEWAY PAVEMENT MARKING PLAN

DISCIPLINE:
CIVIL

PROJECT ENGINEER: OC	PROJECT NO. CE0338A	DRAWING NO.
DESIGNED BY: OC	JOB NO. CE0338A	C-16
DRAWN BY: OC	DATE: APRIL 2020	
CHECKED BY: RZ	SCALE: AS SHOWN	
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TREE PRESERVATION AND REMOVAL LIST (REFER TO BOTANICAL LIST OF EXISTING TREES FOR GENUS/SPECIES)

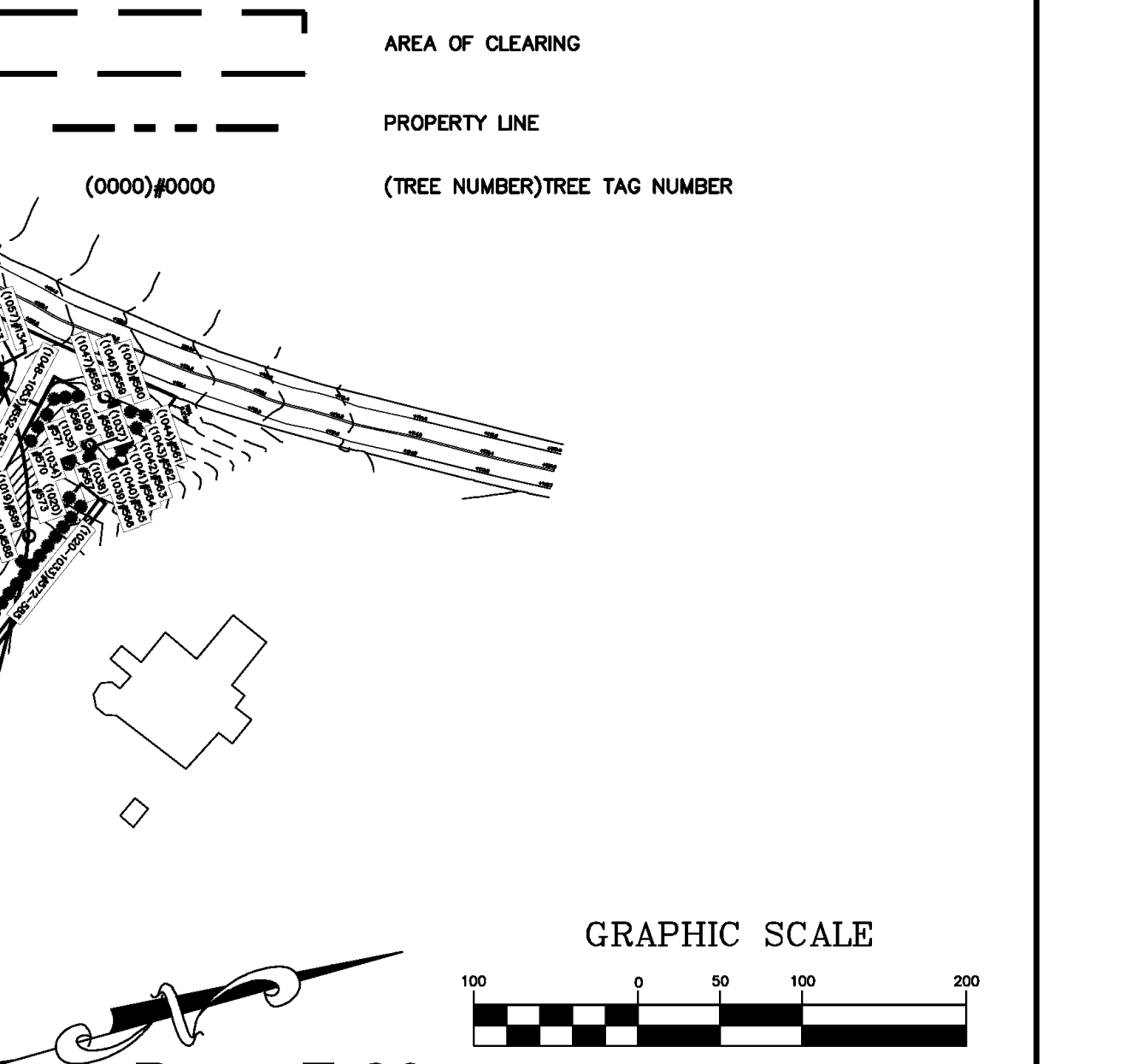
Table with columns: TREE NUMBER, TREE TAG, TREE TYPE, TREE TO BE KEPT, TREE TO BE REMOVED. Lists tree IDs and species for removal.



BOTANICAL LIST OF EXISTING TREES

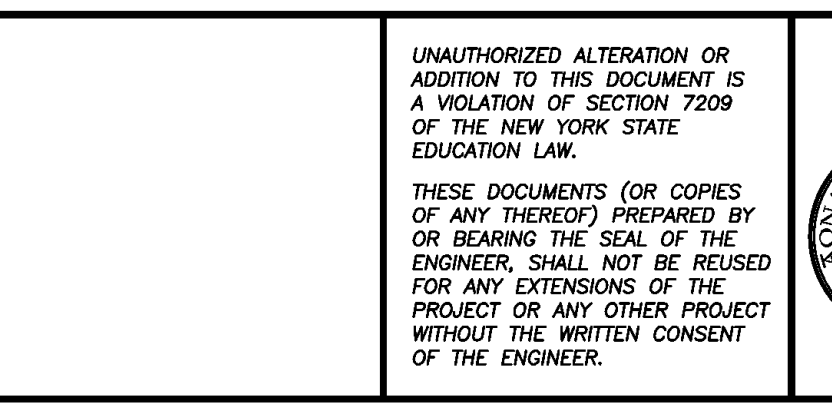
Table with columns: Botanical Name, Common Name. Lists various tree species and their common names.

LEGEND



Revision table with columns: NO., DATE, REVISION DESCRIPTION, BY, CHECKED BY.

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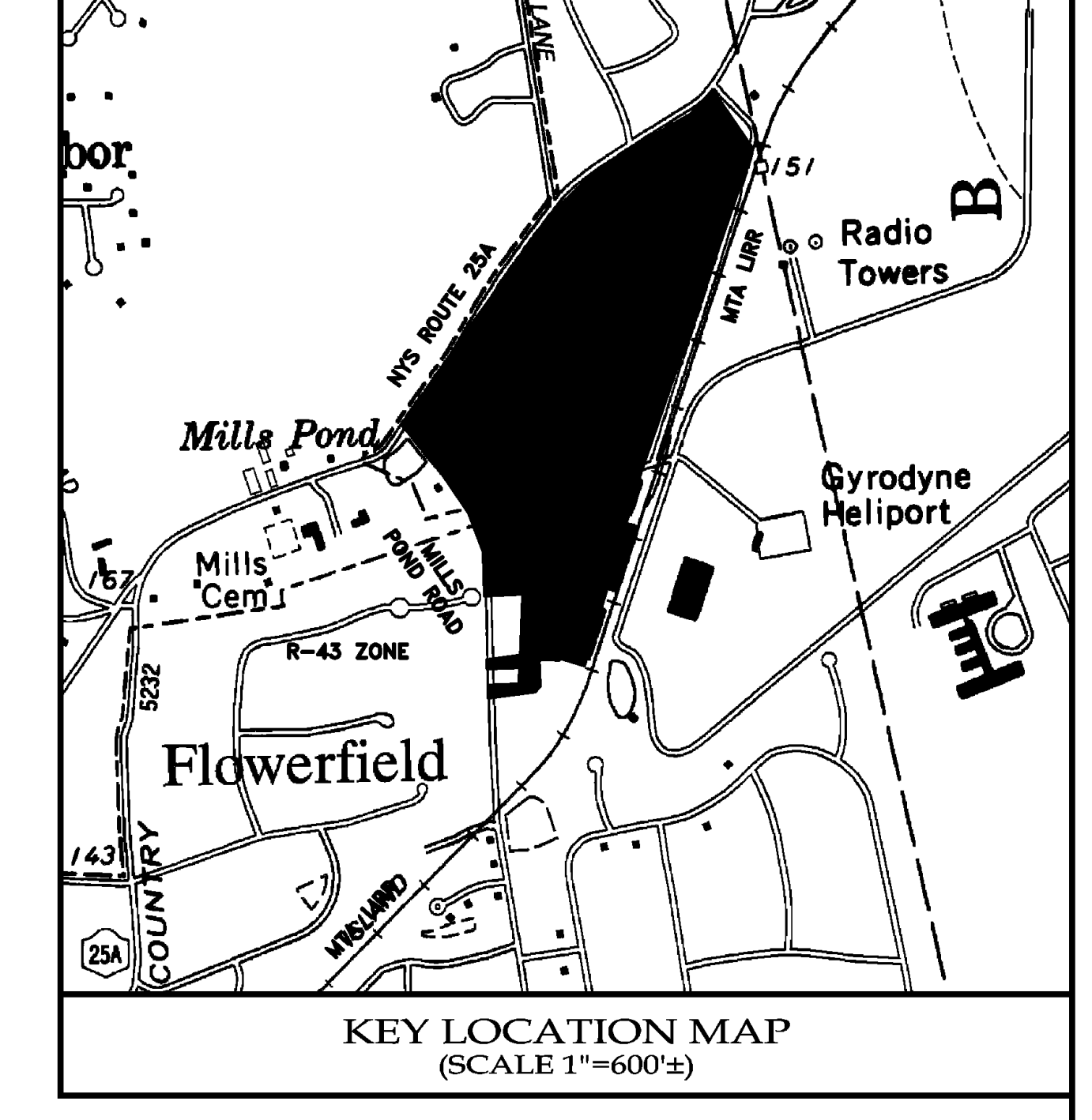
PROJECT NAME: MAP OF FLOWERFIELD FINAL ENGINEERING. PROJECT FOR: 1 FLOWERFIELD DRIVE ST. JAMES, NY 11780.

TITLE: TREE PRESERVATION AND LAND CLEARING PLAN (1). DISCIPLINE: CIVIL.

Project information table including Project No. (CE0338A), Date (APRIL 2020), and Sheet No. (18 of 22).

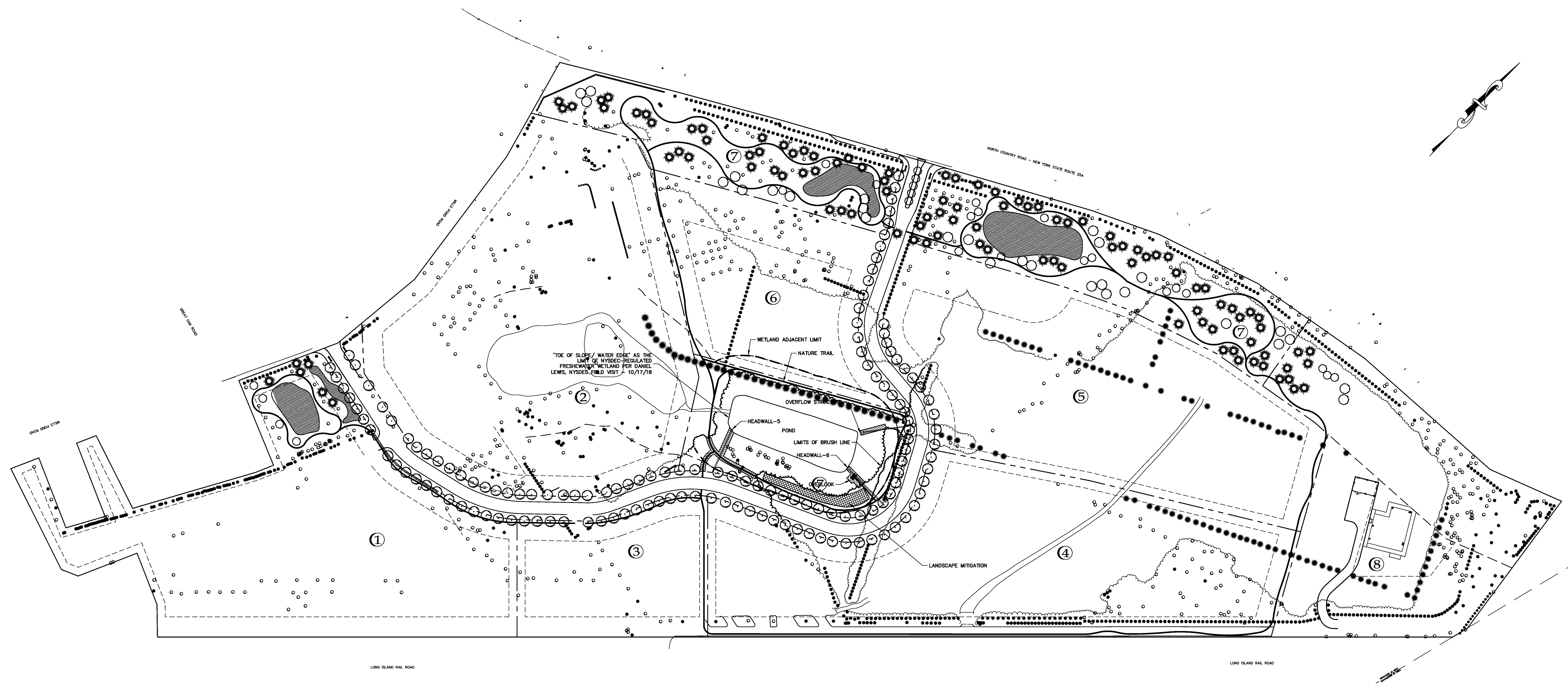
TREE PRESERVATION AND REMOVAL LIST (REFER TO BOTANICAL LIST OF EXISTING TREES FOR GENUS/SPECIES)

Table with columns for ID, Date, Revision, Description, and Int. It lists numerous tree species and their locations, such as 1341 Cherry, 1342 Locust, 1343 Locust, etc., with corresponding coordinates and notes.



NOTE PLEASE SEE SHEET C-17 FOR TREE SPECIES INFORMATION.

Project information block including: PROJECT NAME: MAP OF FLOWERFIELD FINAL ENGINEERING; PROJECT ENGINEER: OC; PROJECT NO.: CE0338A; DRAWING NO.: C-18; SHEET: 19 of 22; CAMERON ENGINEERING & ASSOCIATES, LLP logo and contact info.



EXISTING PLANTING

	EXISTING EVERGREEN TREES TO REMAIN
	EXISTING DECIDUOUS TREES TO REMAIN
	EXISTING PLANTING WITHIN NYS DEC FRESHWATER WETLAND JURISDICTION TO BE REMOVED FOR INSTALLATION OF STORMWATER POND AND NATURE TRAIL. (TO BE MITIGATED 1:1 WITH NATIVE DECIDUOUS PLANTING AS SHOWN IN PROPOSED PLANTING SCHEDULE)

REFER TO DRAWINGS C-17 AND C-18 (TREE PRESERVATION & LAND CLEARING PLAN) FOR DETAILS.

PROPOSED PLANTING SCHEDULE

STREET TREES						
SYMBOL	QTY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	COMMENTS
	136	<i>Red Maple</i>	Red Maple	2.5"-3" CAL.	AS SHOWN	B&B
		<i>Aster saccharum</i>	Silver Maple	2.5"-3" CAL.	AS SHOWN	B&B
		<i>Liquidambar styraciflua</i>	Sweetgum	2.5"-3" CAL.	AS SHOWN	B&B
		<i>Quercus rubra</i>	Northern Red Oak	2.5"-3" CAL.	AS SHOWN	B&B
		<i>Quercus alba</i>	White Oak	2.5"-3" CAL.	AS SHOWN	B&B
		<i>Tilia americana</i>	American Basswood	2.5"-3" CAL.	AS SHOWN	B&B

FLOWERING / CLUMP FORM TREES						
SYMBOL	QTY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	COMMENTS
	56	<i>Amenlanche canadensis</i>	Shadbloß Sewickberry	2"-2.5" CAL.	AS SHOWN	B&B
		<i>Betula nigra</i>	River Birch	2"-2.5" CAL.	AS SHOWN	B&B
		<i>Cercis canadensis</i>	Eastern Redbud	2"-2.5" CAL.	AS SHOWN	B&B
		<i>Chionodoxa virginiana</i>	American Fringe Tree	2"-2.5" CAL.	AS SHOWN	B&B
		<i>Cornus florida</i>	Flowering Dogwood	2"-2.5" CAL.	AS SHOWN	B&B
		<i>Magnolia virginiana</i>	Sweetbay Magnolia	2"-2.5" CAL.	AS SHOWN	B&B

EVERGREEN TREES						
SYMBOL	QTY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	COMMENTS
	105	<i>Ilex opaca</i>	American Holly	7"-8" HEIGHT	AS SHOWN	B&B
		<i>Juniperus virginiana</i>	Eastern Red Cedar	7"-8" HEIGHT	AS SHOWN	B&B
		<i>Pinus rigida</i>	Pitch Pine	7"-8" HEIGHT	AS SHOWN	B&B
		<i>Pinus strobus</i>	White Pine	7"-8" HEIGHT	AS SHOWN	B&B
		<i>Thuja occidentalis</i>	Northern White Cedar	7"-8" HEIGHT	AS SHOWN	B&B
		<i>Thuja canadensis</i>	Canadian Hemlock	7"-8" HEIGHT	AS SHOWN	B&B

1:1 LANDSCAPE MITIGATION WITH NATIVE DECIDUOUS SHRUBS (WITHIN NYS DEC FRESHWATER WETLAND JURISDICTION)						
SYMBOL	QTY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	COMMENTS
		<i>Red Chokeberry</i>	Red Chokeberry	24"-36"	AS SHOWN	CONTAINER
		<i>Cornus sericea</i>	Red Twig Dogwood	24"-36"	AS SHOWN	CONTAINER
		<i>Hemodemia virginiana</i>	Common Witchhazel	24"-36"	AS SHOWN	CONTAINER
		<i>Hydrangea quercifolia</i>	Dotted Hydrangea	24"-36"	AS SHOWN	CONTAINER
		<i>Ilex verticillata</i>	Winterberry	24"-36"	AS SHOWN	CONTAINER
		<i>Vaccinium corymbosum</i>	Highbush Blueberry	24"-36"	AS SHOWN	CONTAINER
		<i>Viburnum corymbosum</i>	Koreanspice Viburnum	24"-36"	AS SHOWN	CONTAINER

Seeding at Right of way (ROW)

LOW GROWING WILDFLOWER & GRASS MIX

ERNST CONSERVATION MIX (ERNAK-105)
 UTILIZATION: RESTORATION OF DISTURBED AREAS, OTHER THAN THOSE INDICATED SPECIFICALLY

Mix Composition
 66.8% *Festuca ovina*, Variety Not Stated (Sheep Fescue, Variety Not Stated)
 17.0% *Lolium multiflorum* (Annual Ryegrass)
 0.0% *Linum perenne* spp. *lewisii* (Perennial Blue Flax)
 2.5% *Corynephorus canadensis* (Sheep Daisy)
 2.0% *Corynephorus lancofolia* (Lanceleaf Coreopsis)
 2.0% *Rudbeckia hirta*, Coastal Plain NC Ecotype (Black-eyed Susan, Coastal Plain NC Ecotype)
 1.0% *Chamaecrista fasciculata*, PA Ecotype (Partridge Pea, PA Ecotype)
 1.0% *Pappus rhoeas*, Shirley Milk Coreopsis/Shirley Milk
 0.4% *Achillea millefolium* (Common Yarrow)
 0.4% *Asclepias tuberosa* (Butterfly Milkweed)
 0.4% *Aster oblongifolius*, PA Ecotype (Aromatic Aster, PA Ecotype)
 0.3% *Spiranthes ciliolata*, VA Ecotype (Missillow, VA Ecotype)
 0.1% *Pycnanthemum tenuifolium* (Narrowleaf Mountainmint)
 0.1% *Zizia aurea*, PA Ecotype (Golden Alexanders, PA Ecotype)

RAIN GARDEN MIX

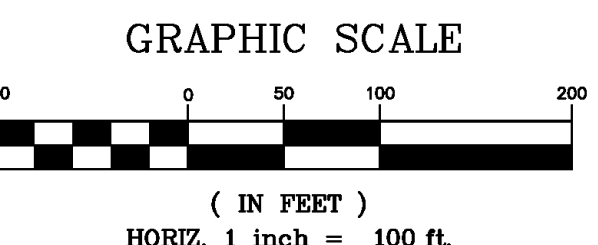
ERNST CONSERVATION MIX (ERNAK-106-1)
 UTILIZATION: ROADWAY SWALES AND PORTIONS OF DRAINAGE RETENTION AREAS

Mix Composition
 26.0% *Panicum clandestinum*, Tiggs (Dreventoug, Tiggs)
 25.0% *Panicum virgatum*, Shelter (Switchgrass, Shelter)
 20.0% *Carex vulpinoidea*, PA Ecotype (Fox Sedge, PA Ecotype)
 20.0% *Elymus virginicus*, Madison (Virginia Wildrye, Madison)
 4.0% *Agraria perennans*, Albany Pine Bush-NY Ecotype (Autumn Bentgrass, Albany Pine Bush NY Ecotype)
 5.0% *Juncus effusus* (Soft Rush)
 1.0% *Juncus tenuis*, PA Ecotype (Path Rush, PA Ecotype)
 1.0% *Panicum rigidulum*, PA Ecotype (Redtop Panicgrass, PA Ecotype)

NATIVE DETENTION AREA MIX

ERNST CONSERVATION MIX (ERNAK-103)
 UTILIZATION: PORTIONS OF DRAINAGE RETENTION AREAS

Mix Composition
 51.0% *Schizachyrium scoparium*, Camper (Little Bluestem, Camper)
 20.4% *Panicum clandestinum*, Tiggs (Dreventoug, Tiggs)
 20.0% *Carex vulpinoidea*, PA Ecotype (Fox Sedge, PA Ecotype)
 20.0% *Elymus virginicus*, PA Ecotype (Virginia Wildrye, PA Ecotype)
 5.0% *Chasmananthus latifolius*, WV Ecotype (River Oats, WV Ecotype)
 2.5% *Panicum rigidulum*, PA Ecotype (Redtop Panicgrass, PA Ecotype)
 0.5% *Carex acutata*, PA Ecotype (Blunt Broom Sedge, PA Ecotype)
 0.3% *Juncus effusus* (Soft Rush)
 0.3% *Juncus tenuis*, PA Ecotype (Path Rush, PA Ecotype)



NO.	DATE	REVISION DESCRIPTION	INT.
1	4/18/2020	REVISED PER TOWN ENGINEER AND SCANS	OC

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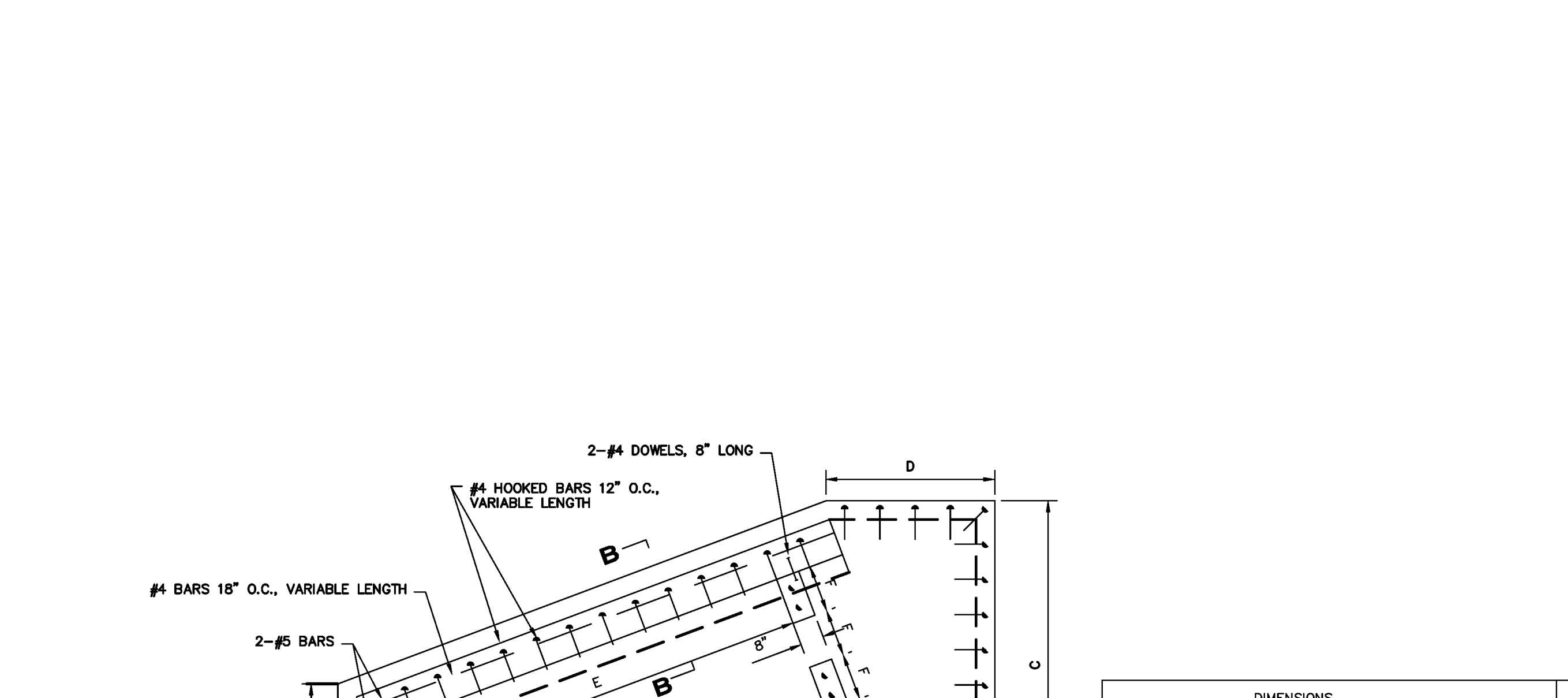
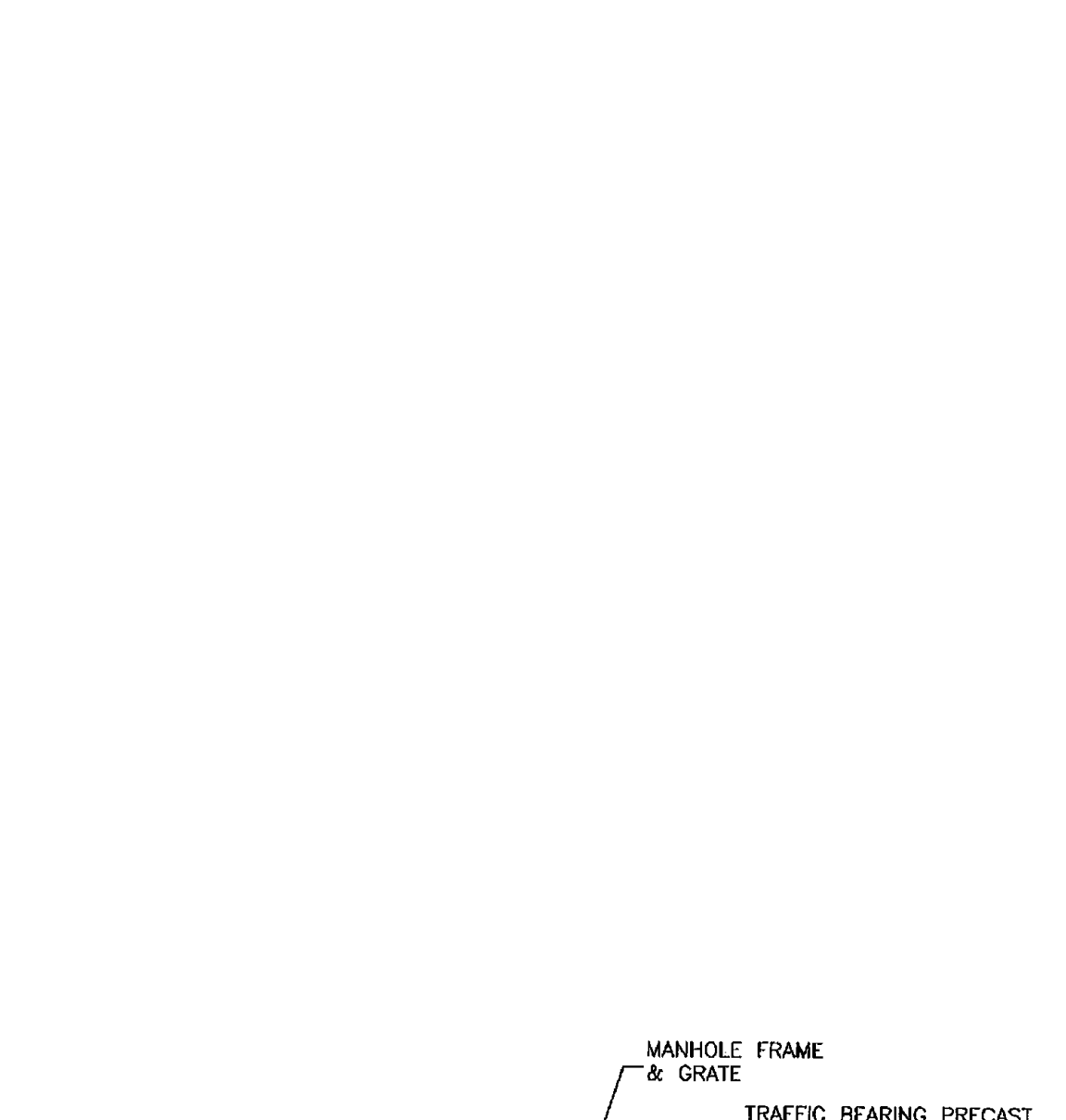
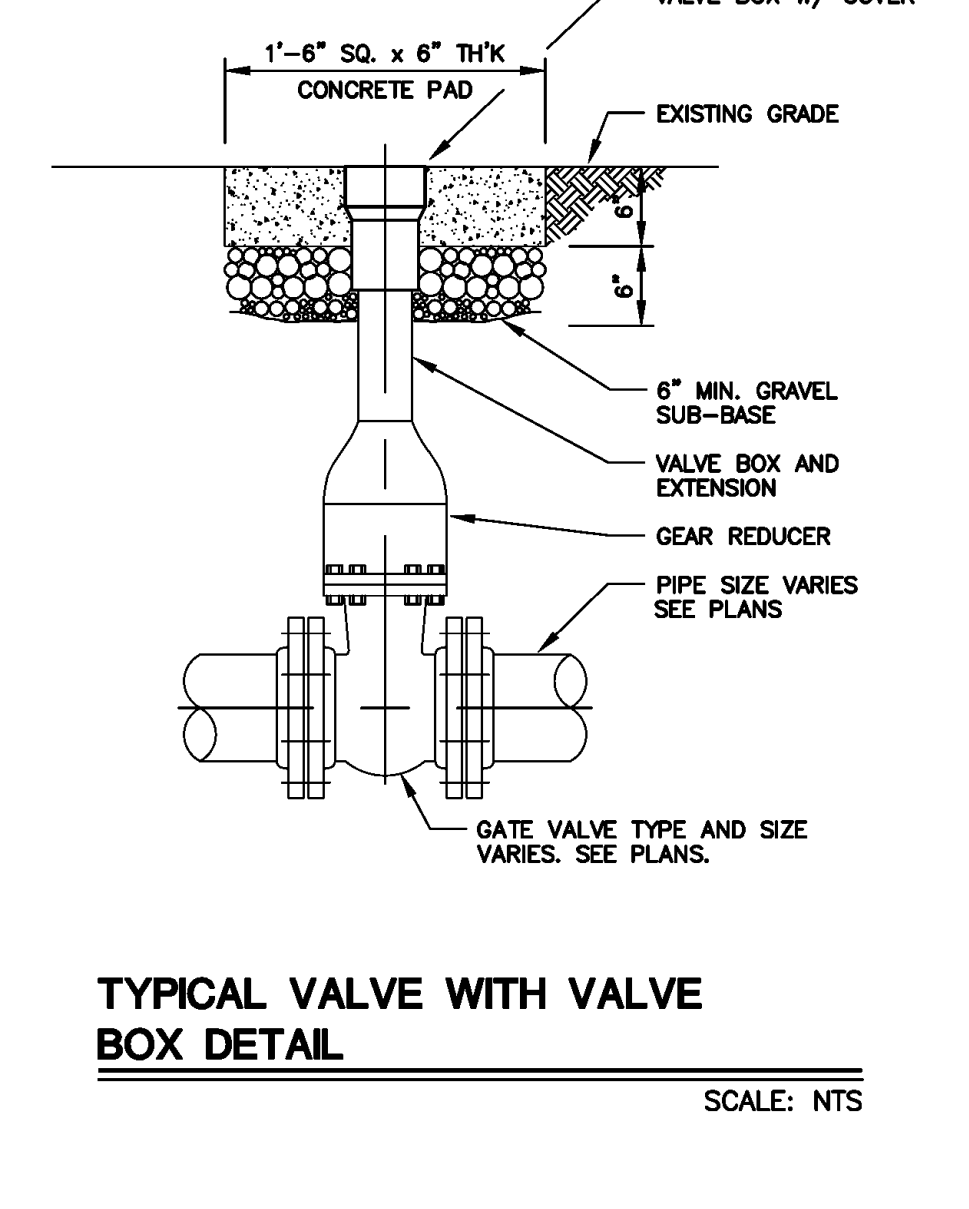
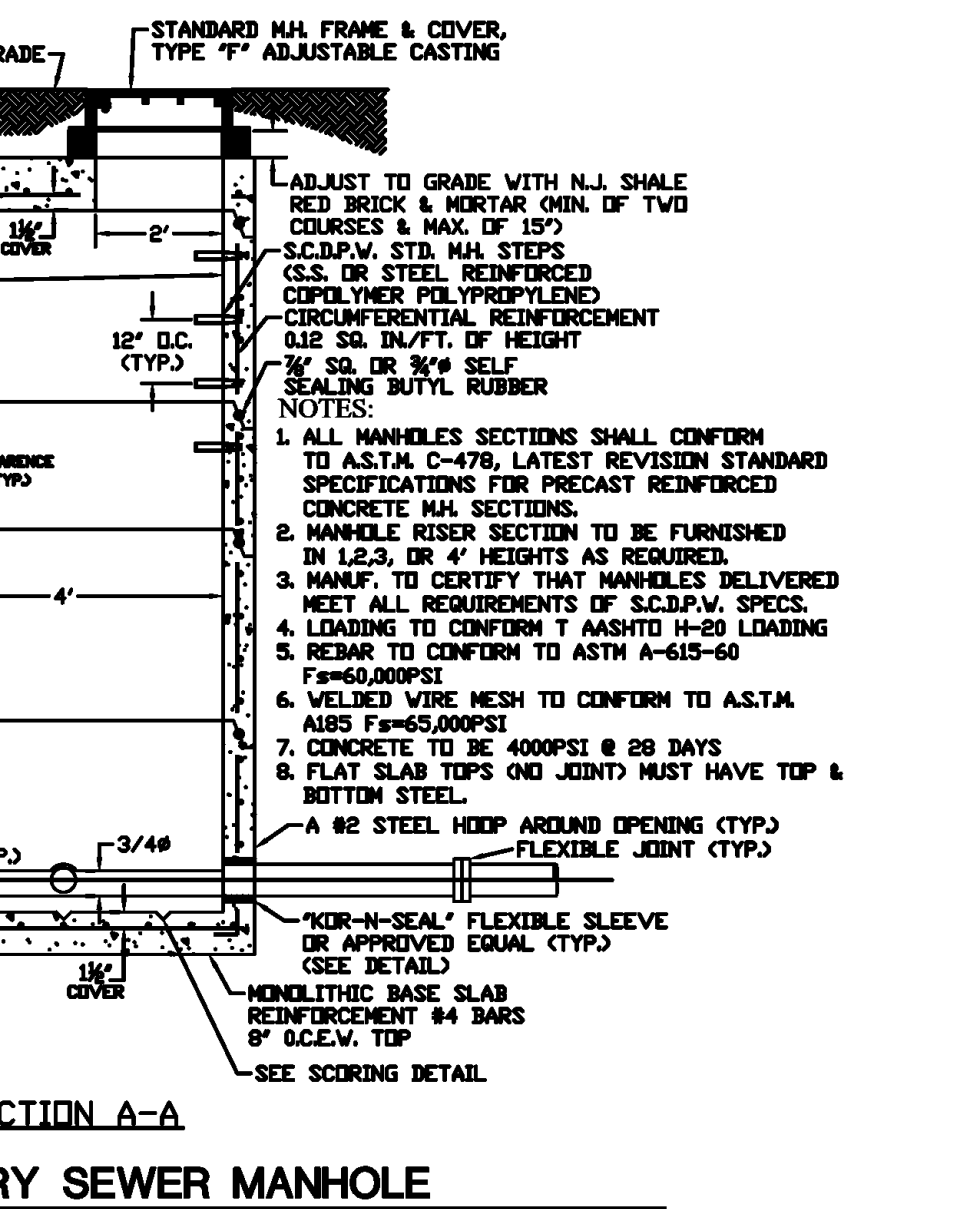
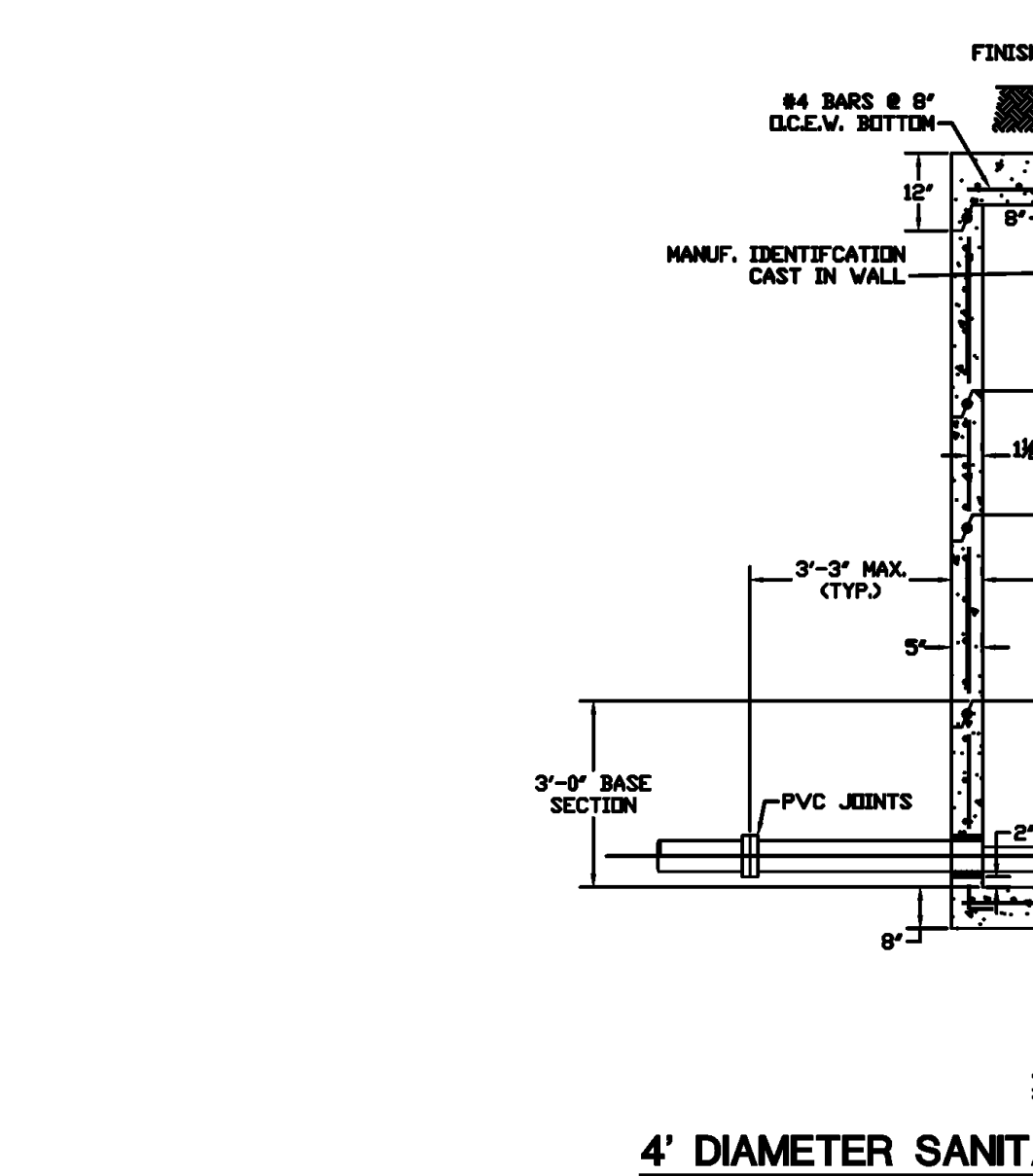
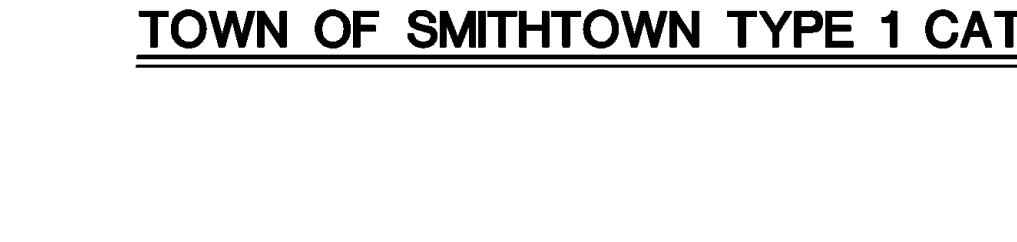
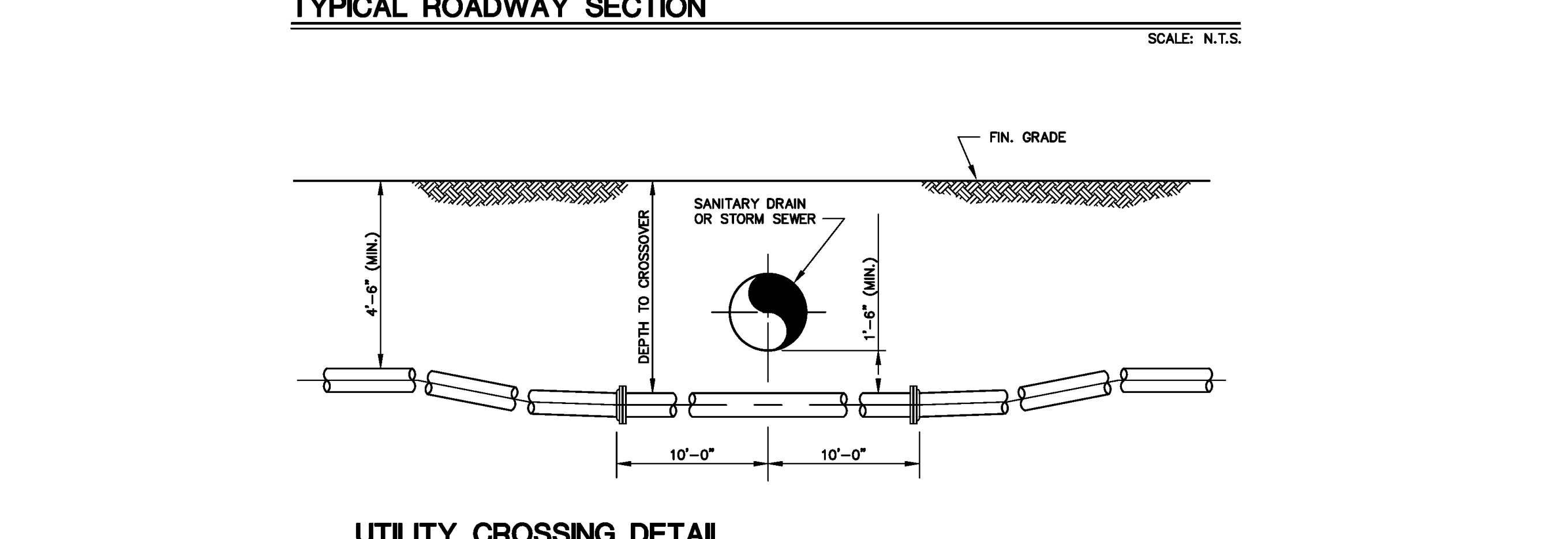
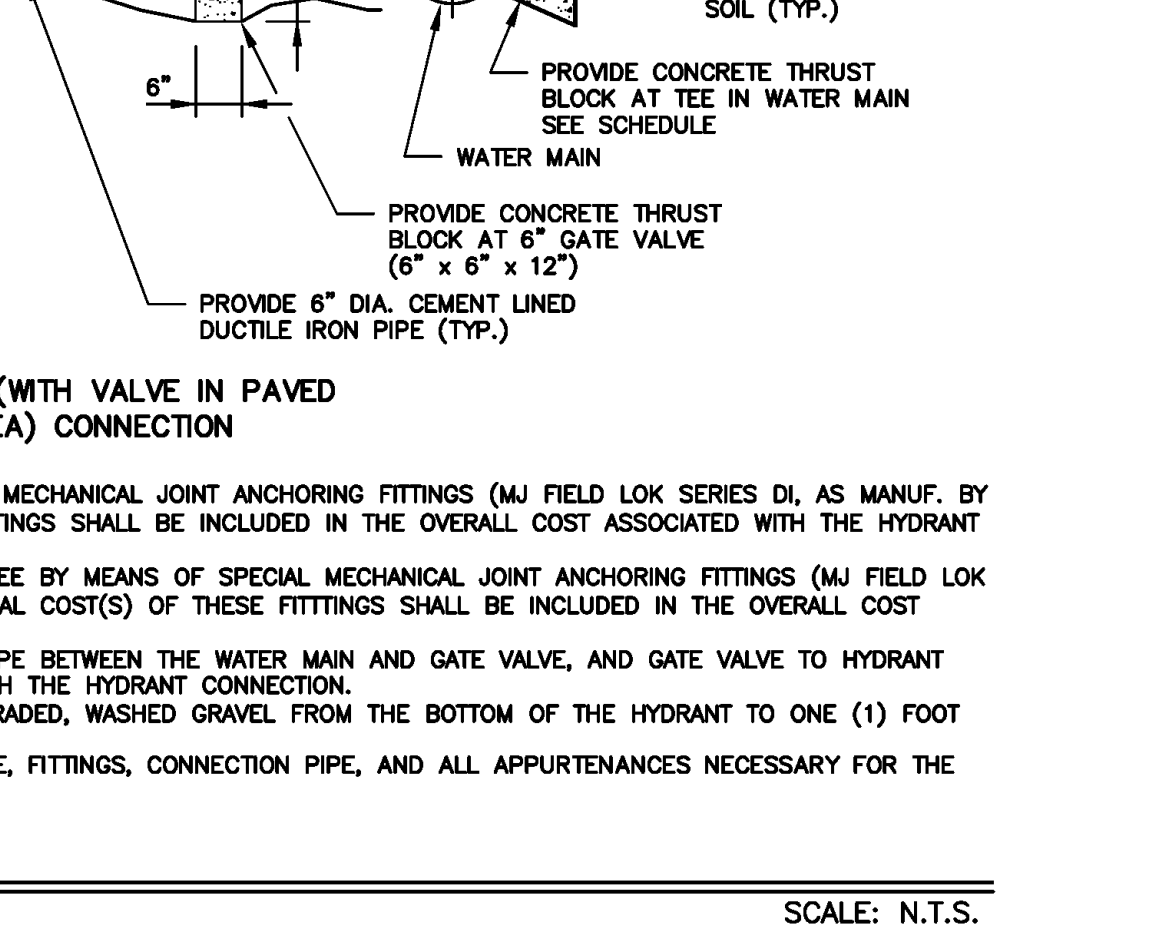
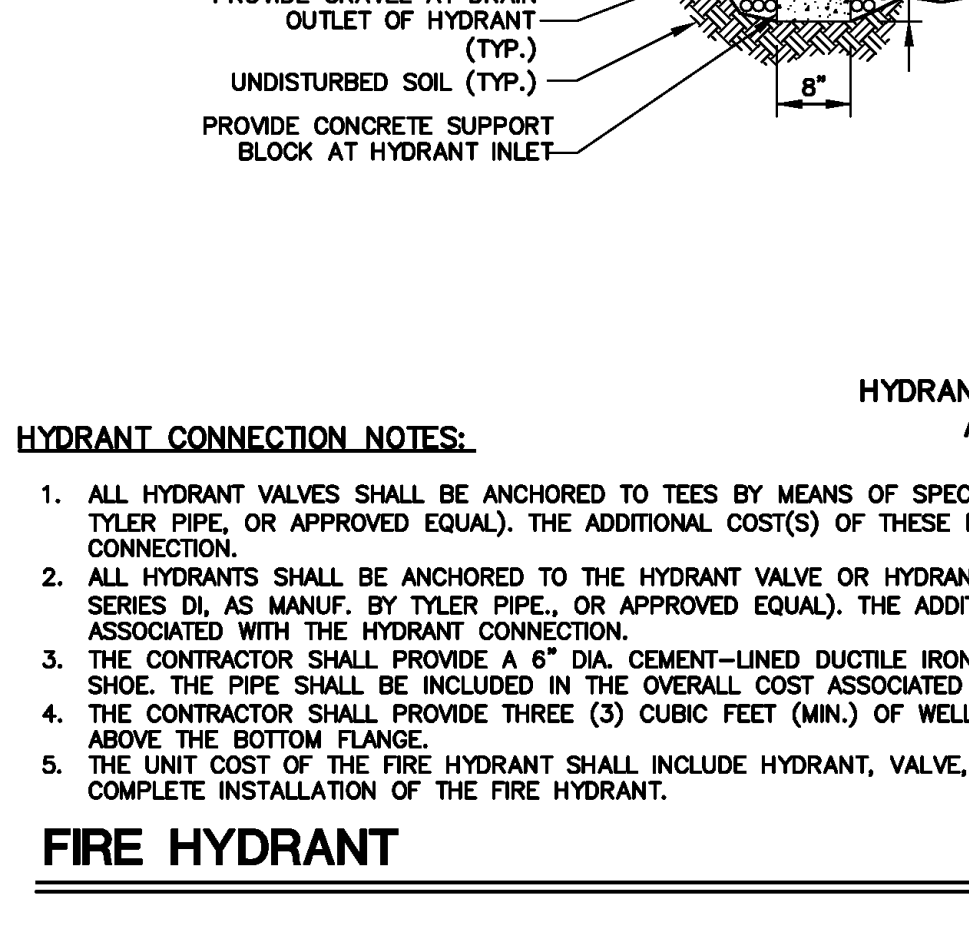
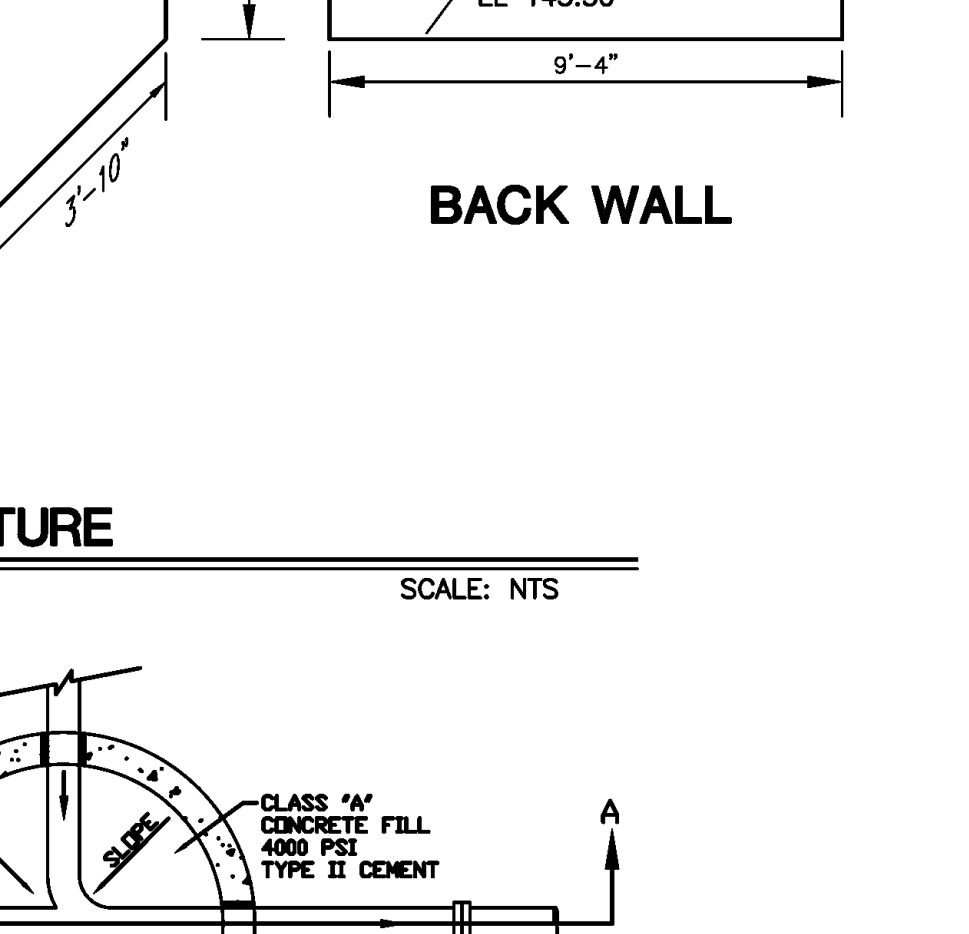
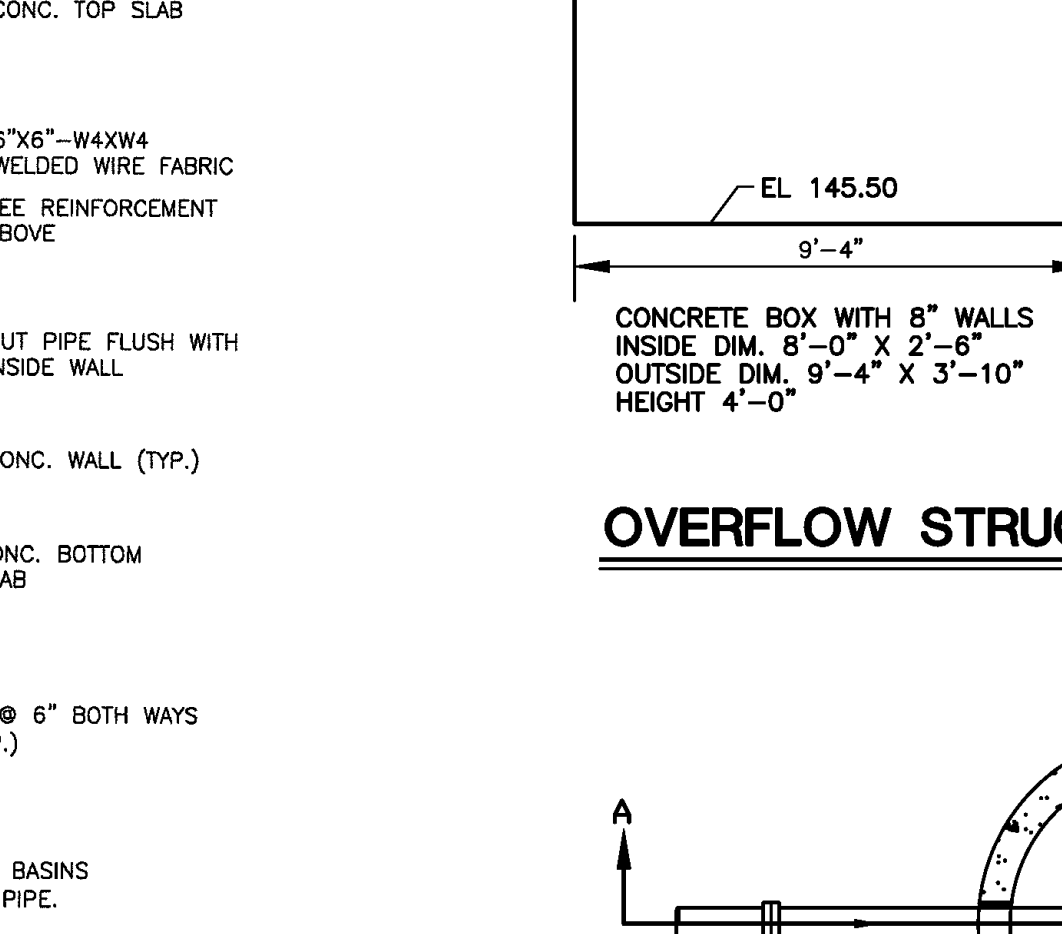
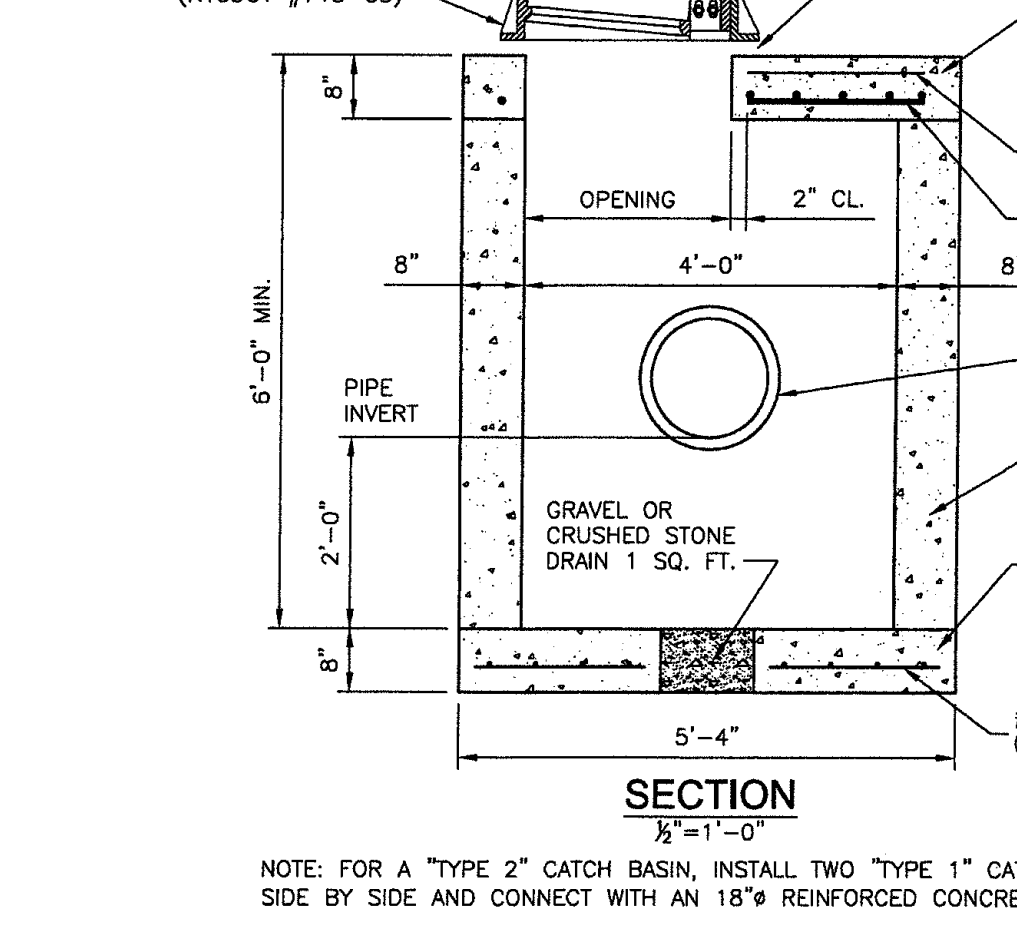
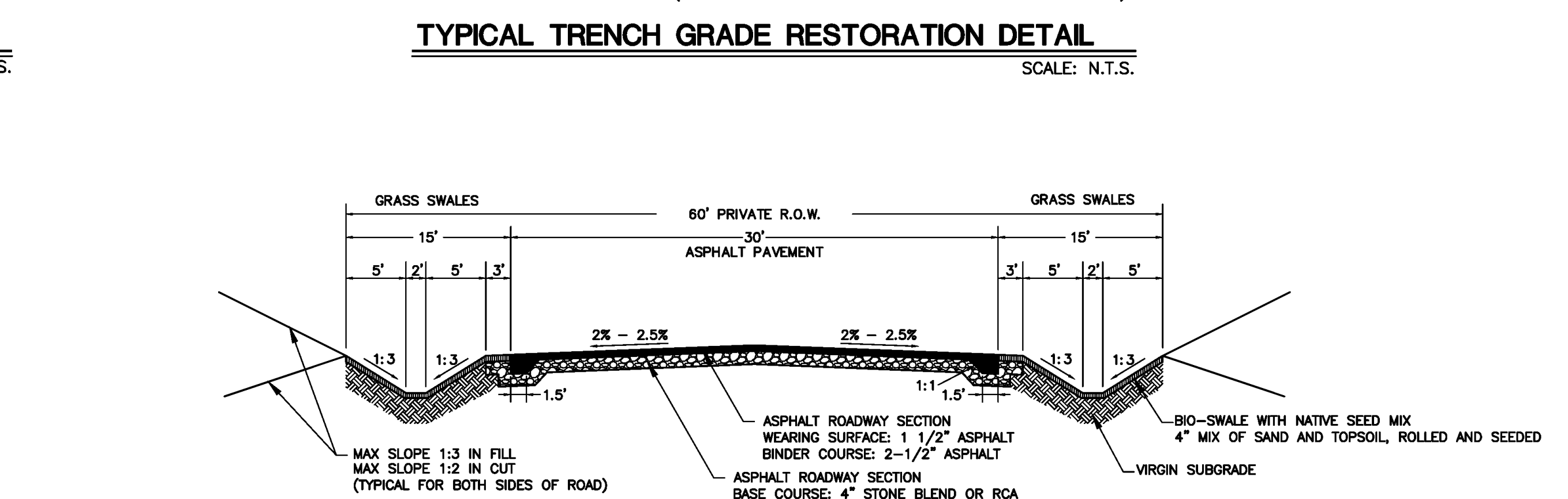
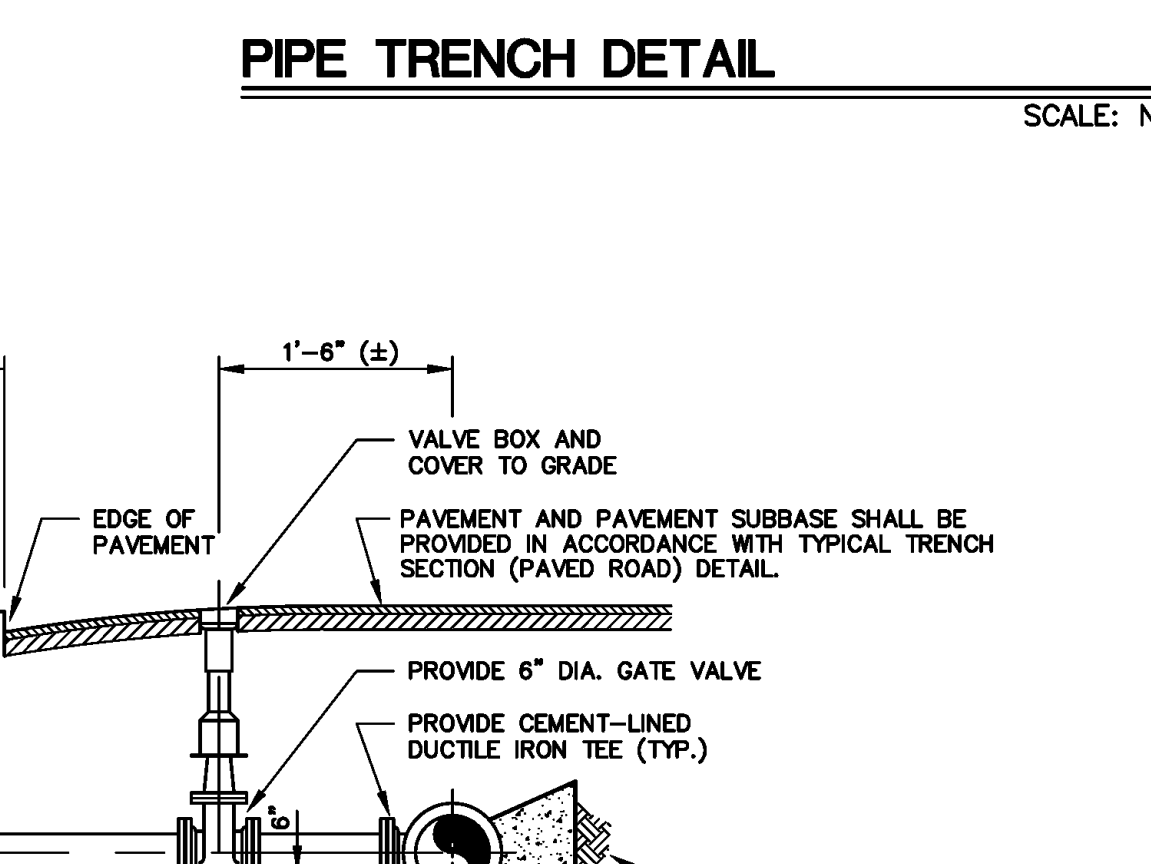
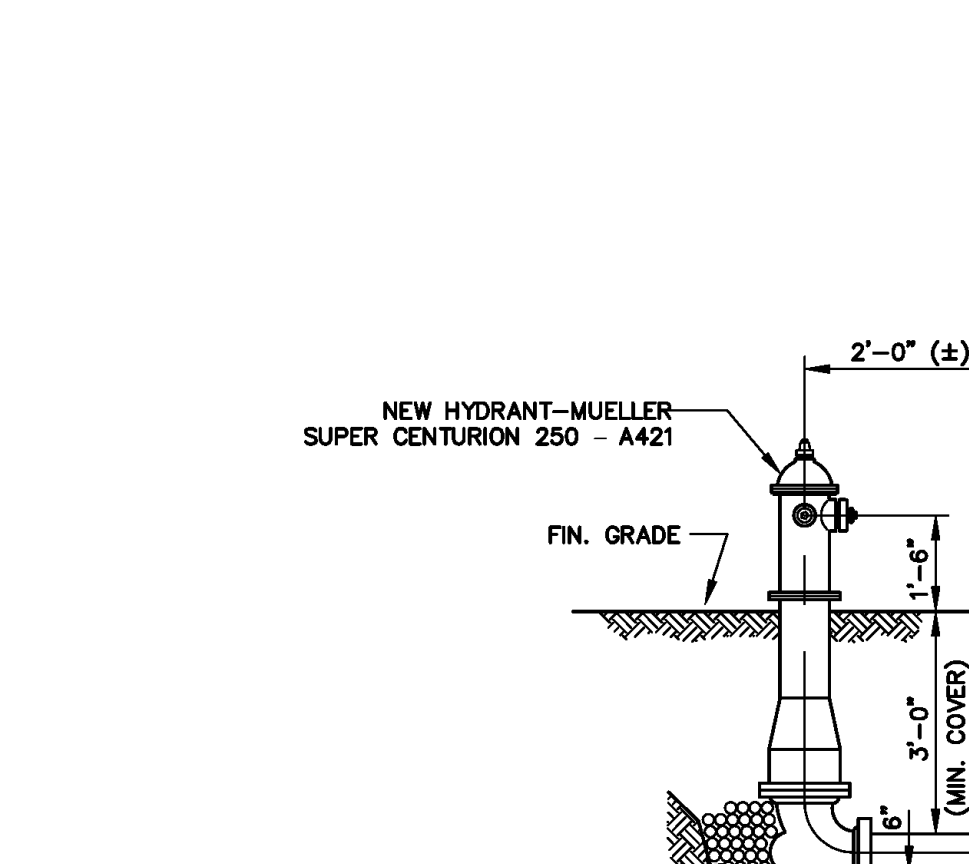
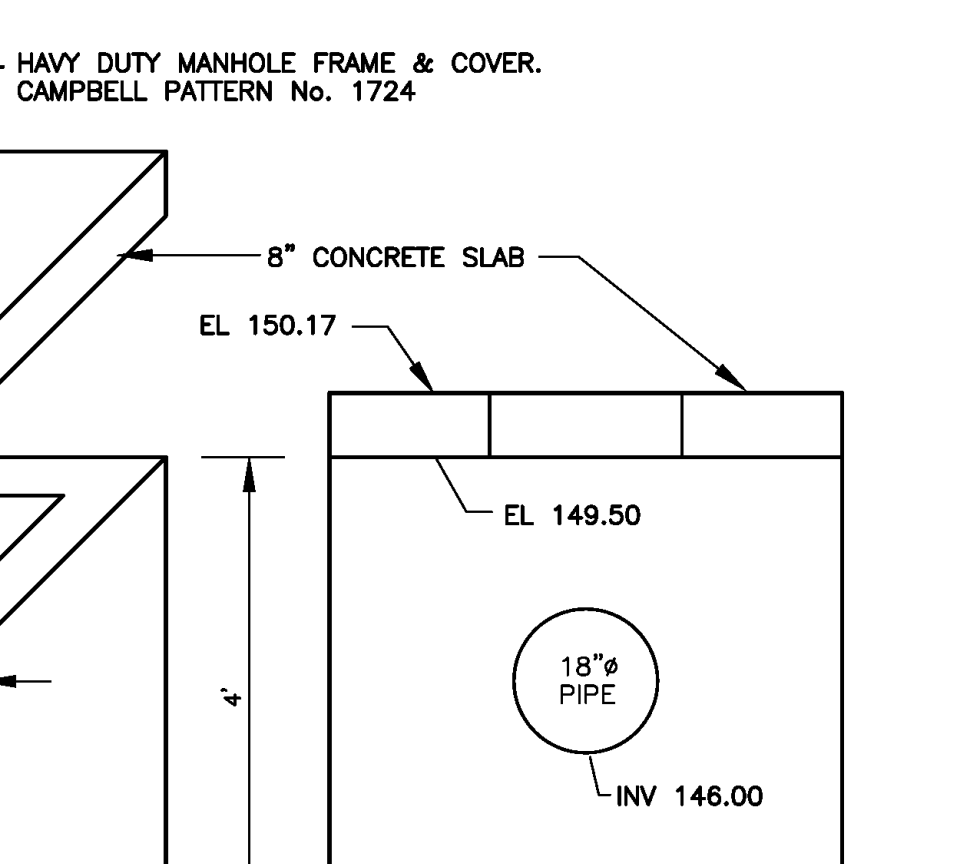
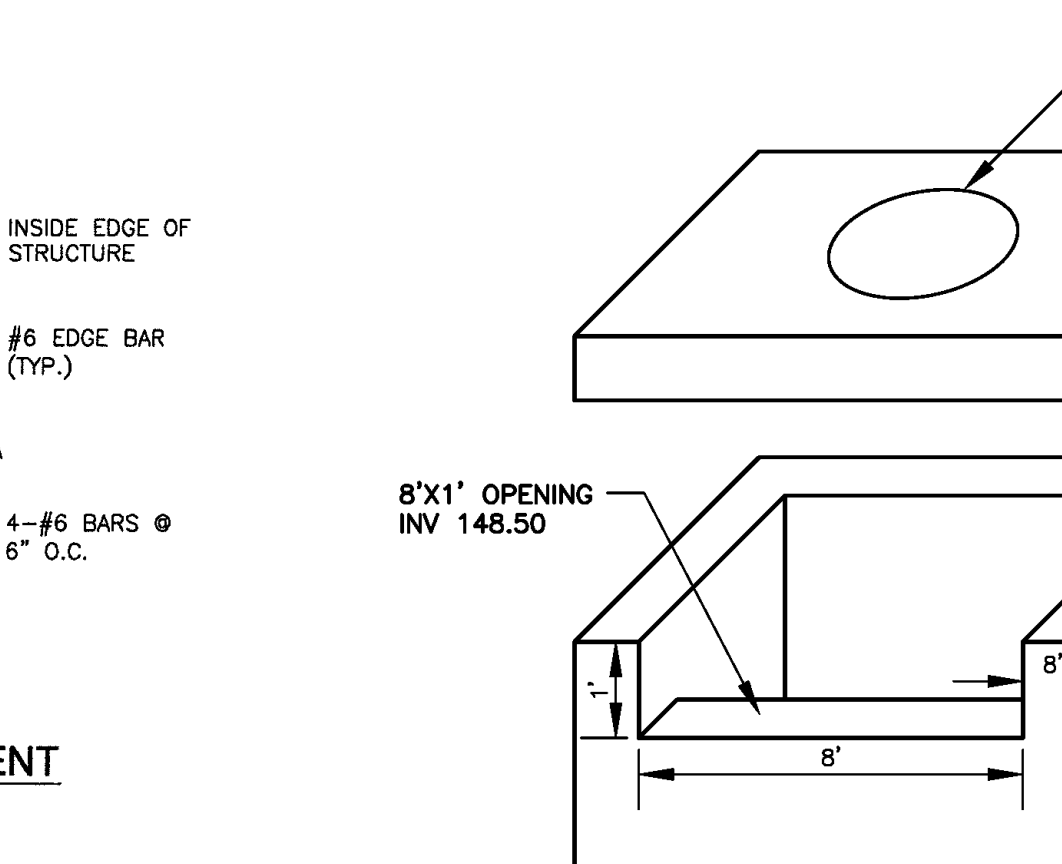
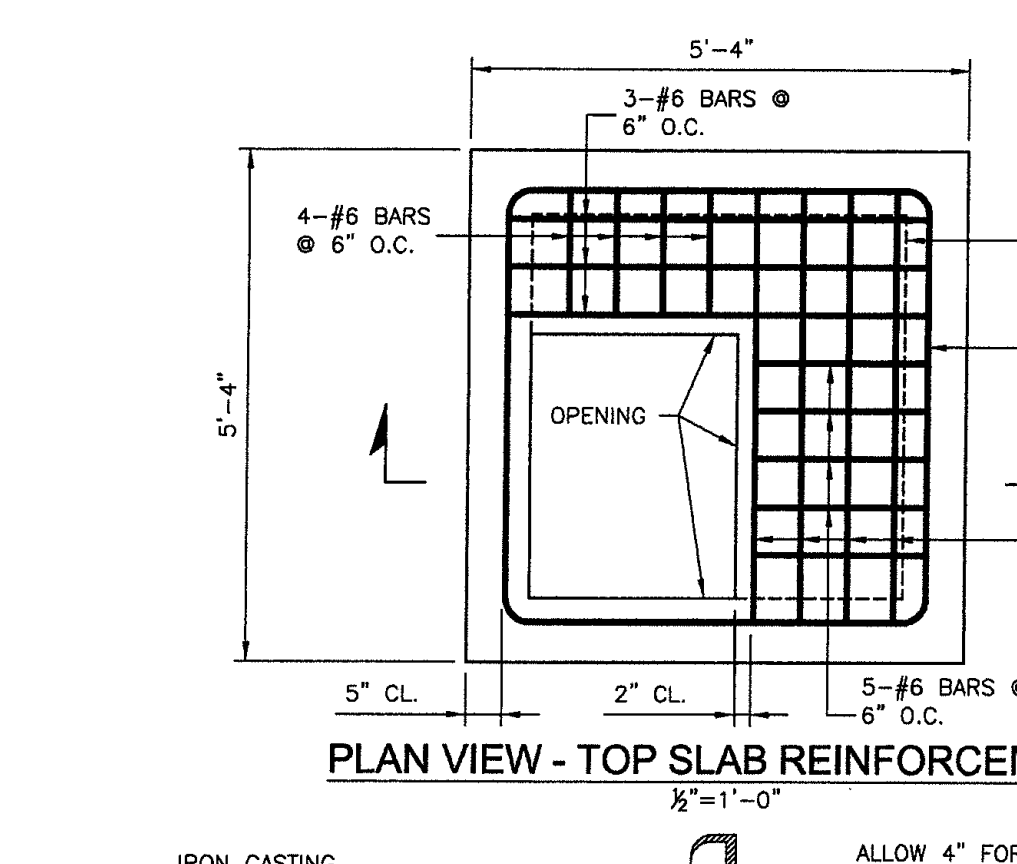
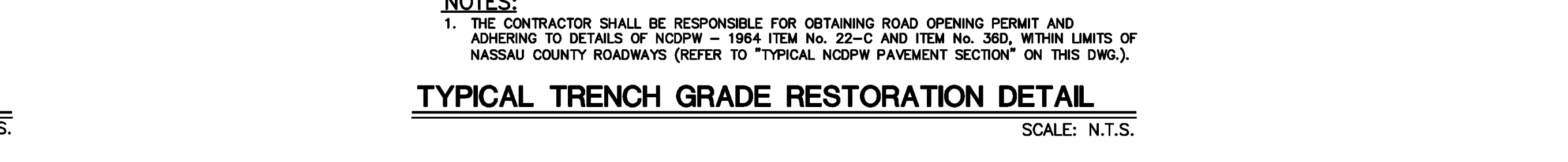
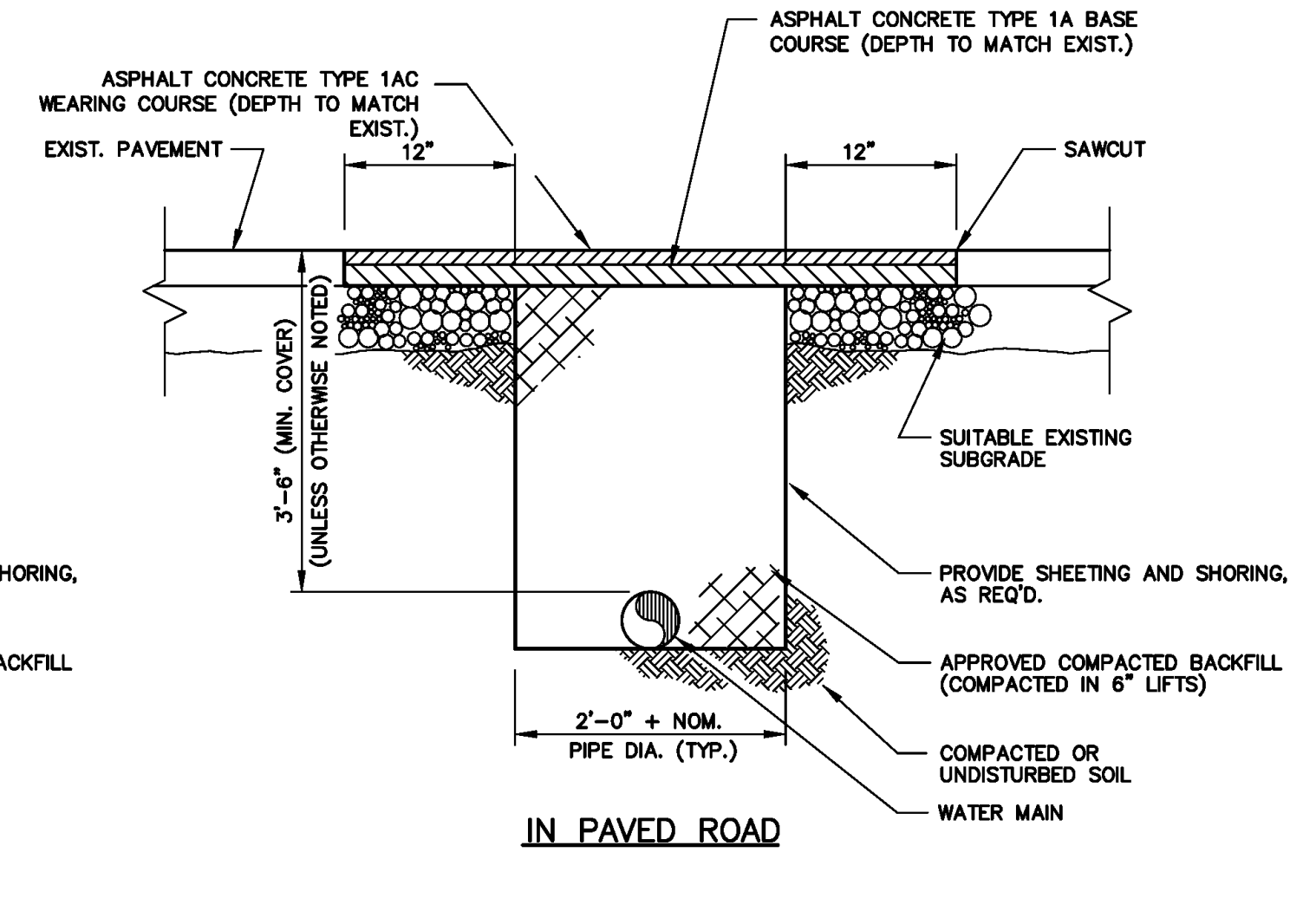
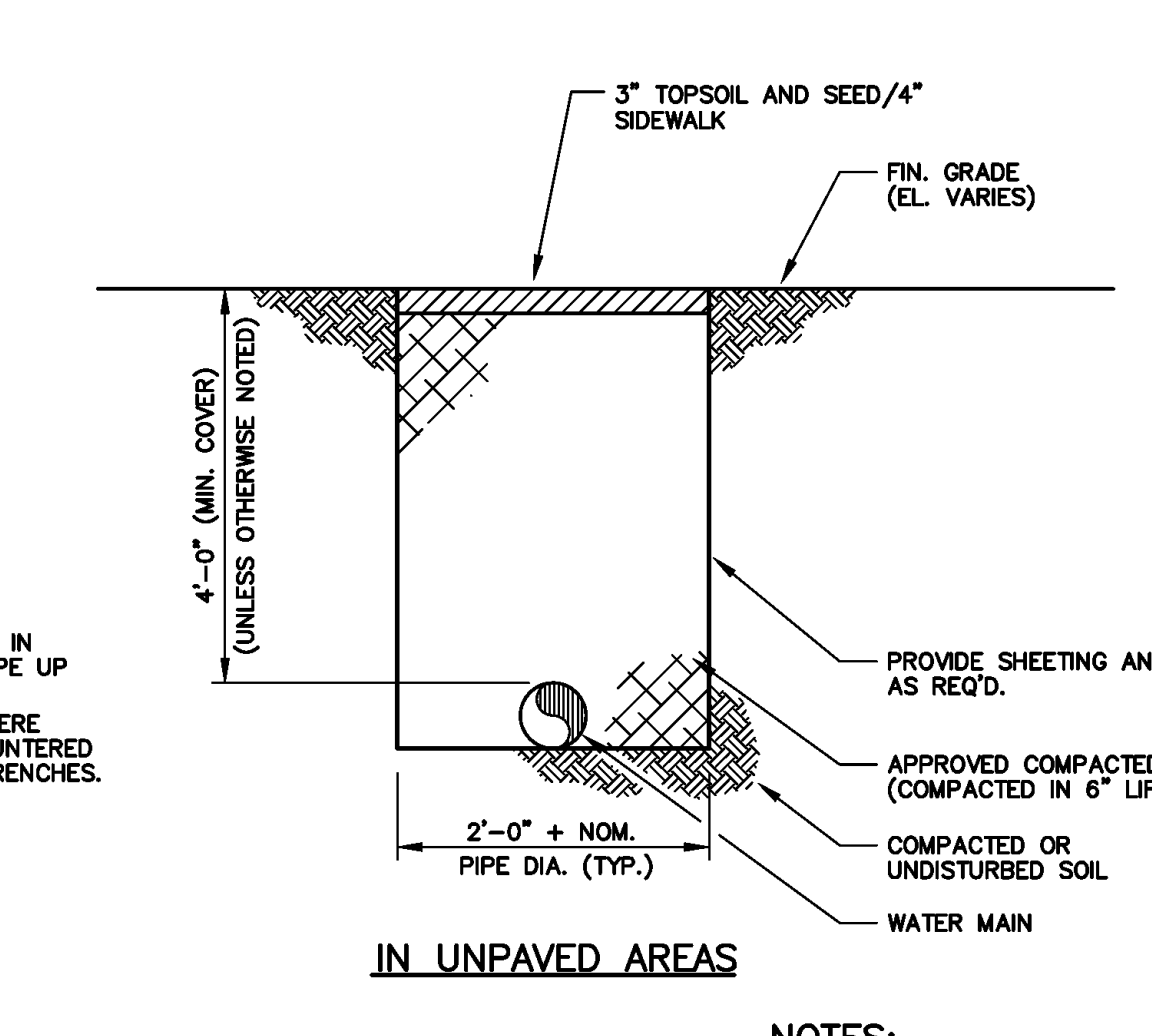
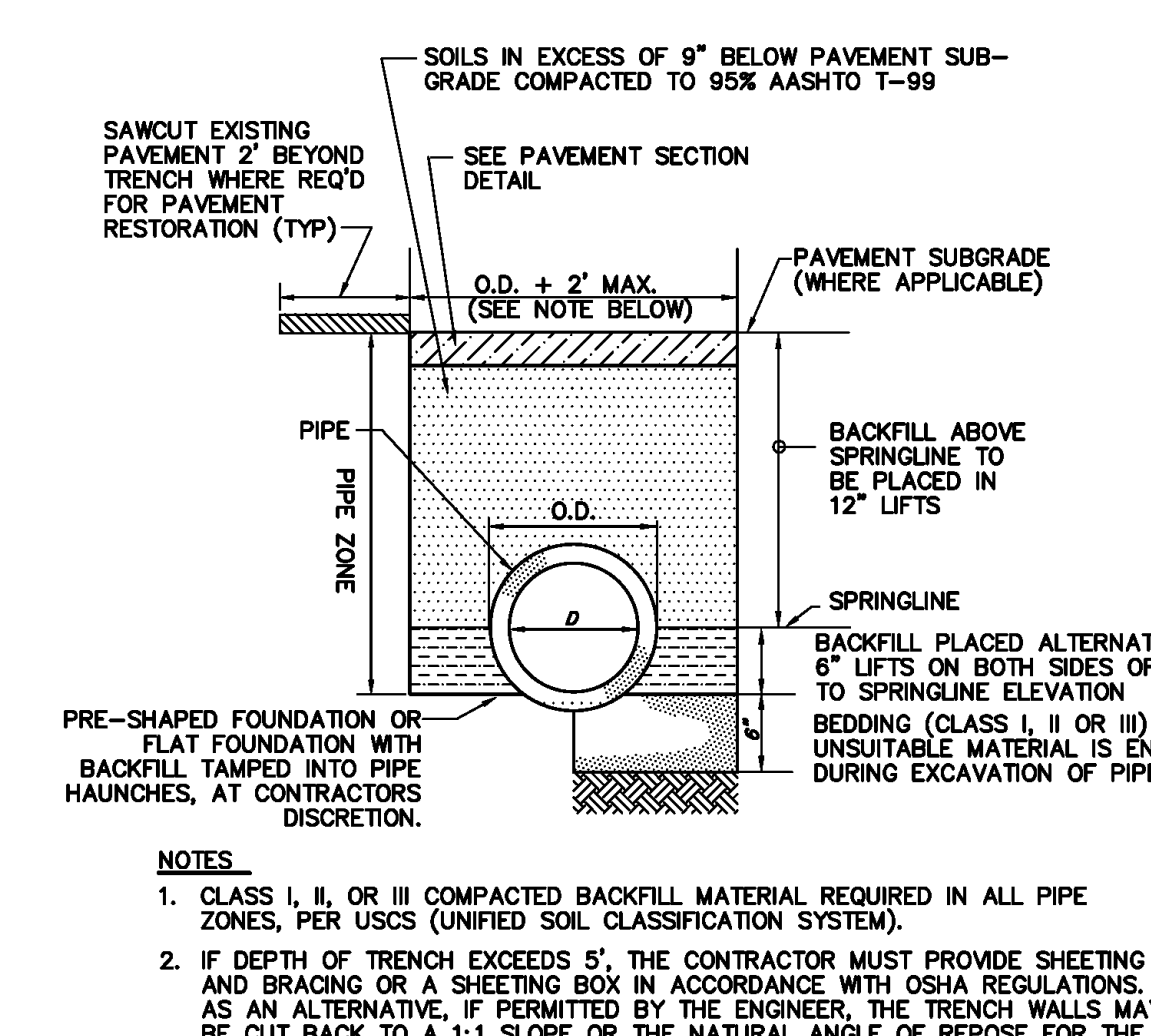
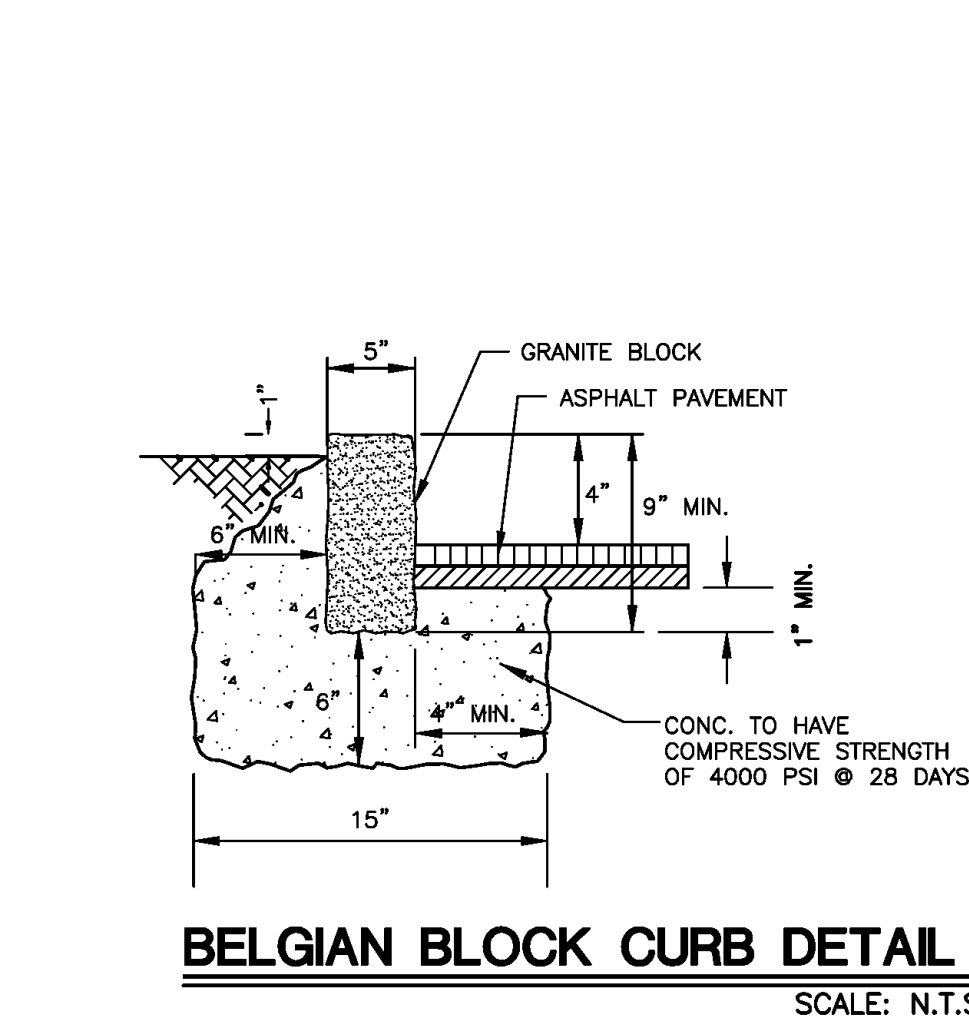
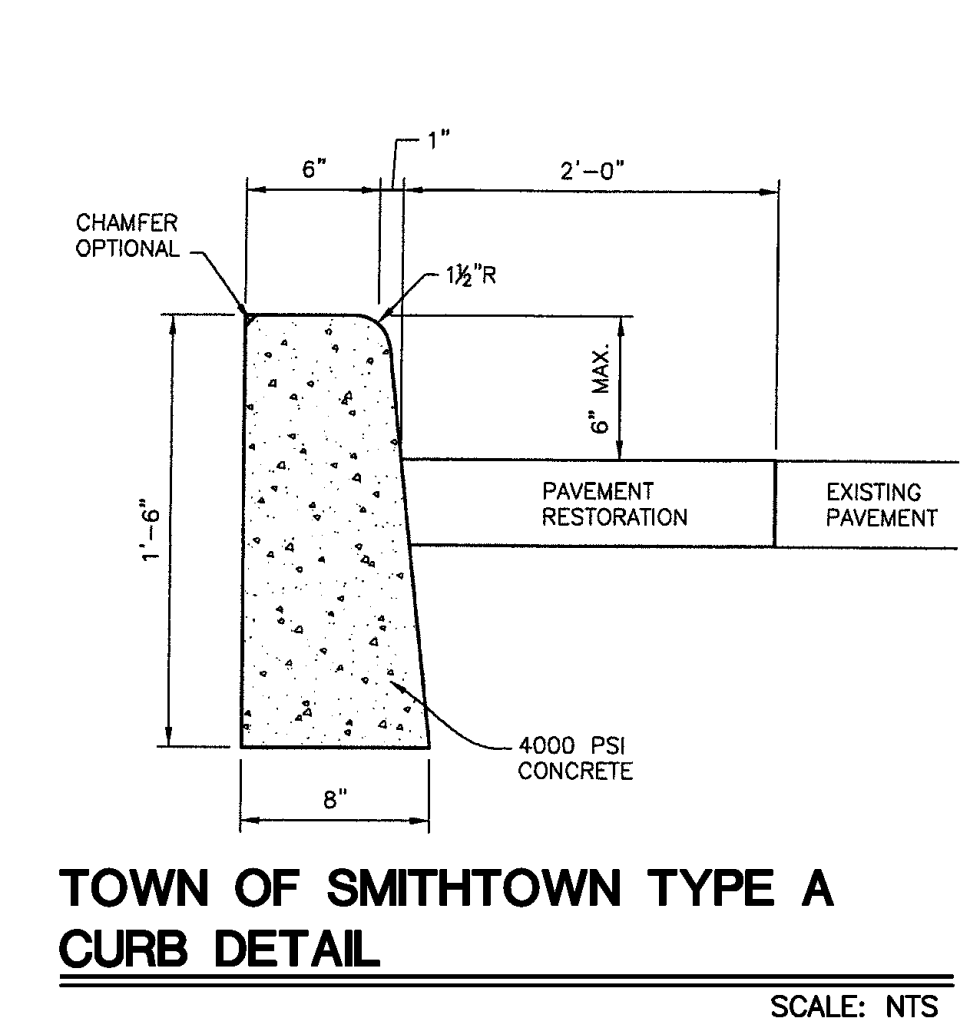
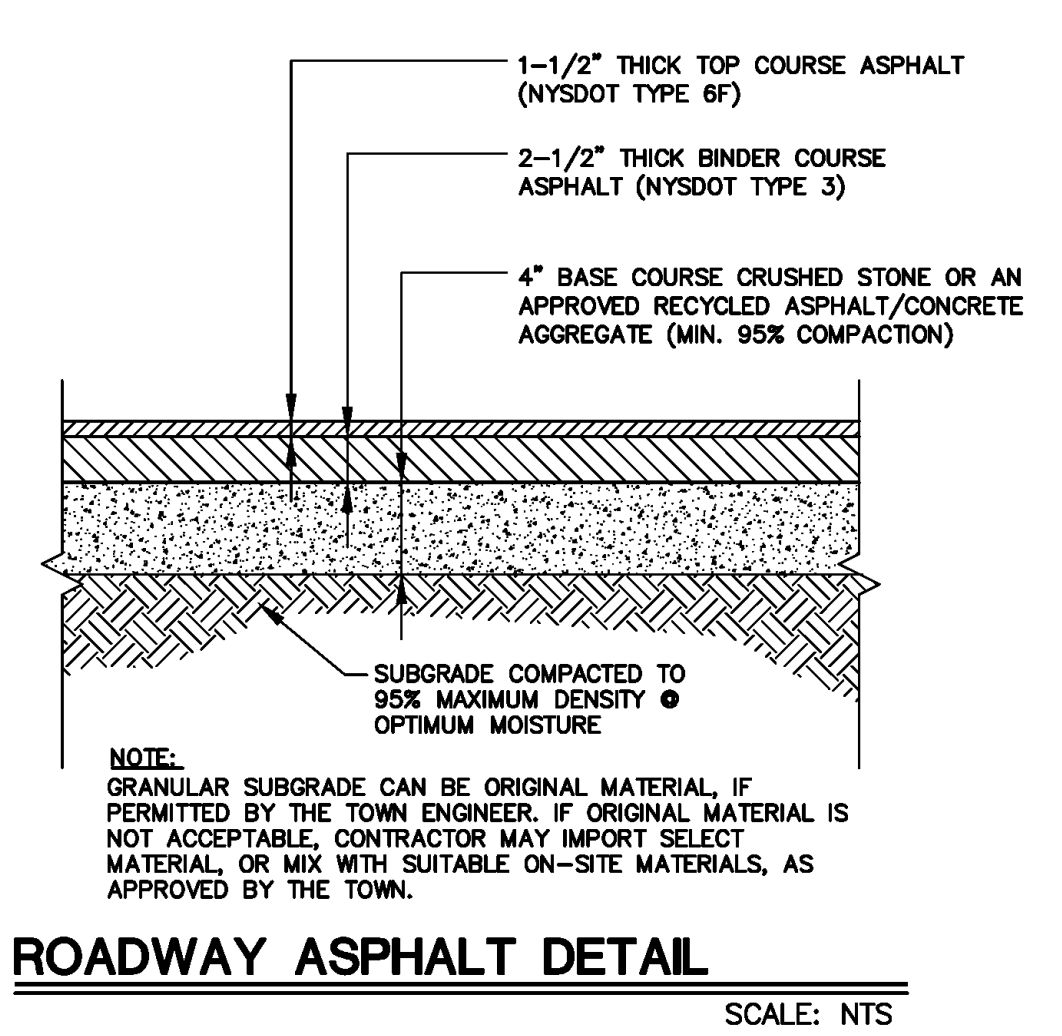
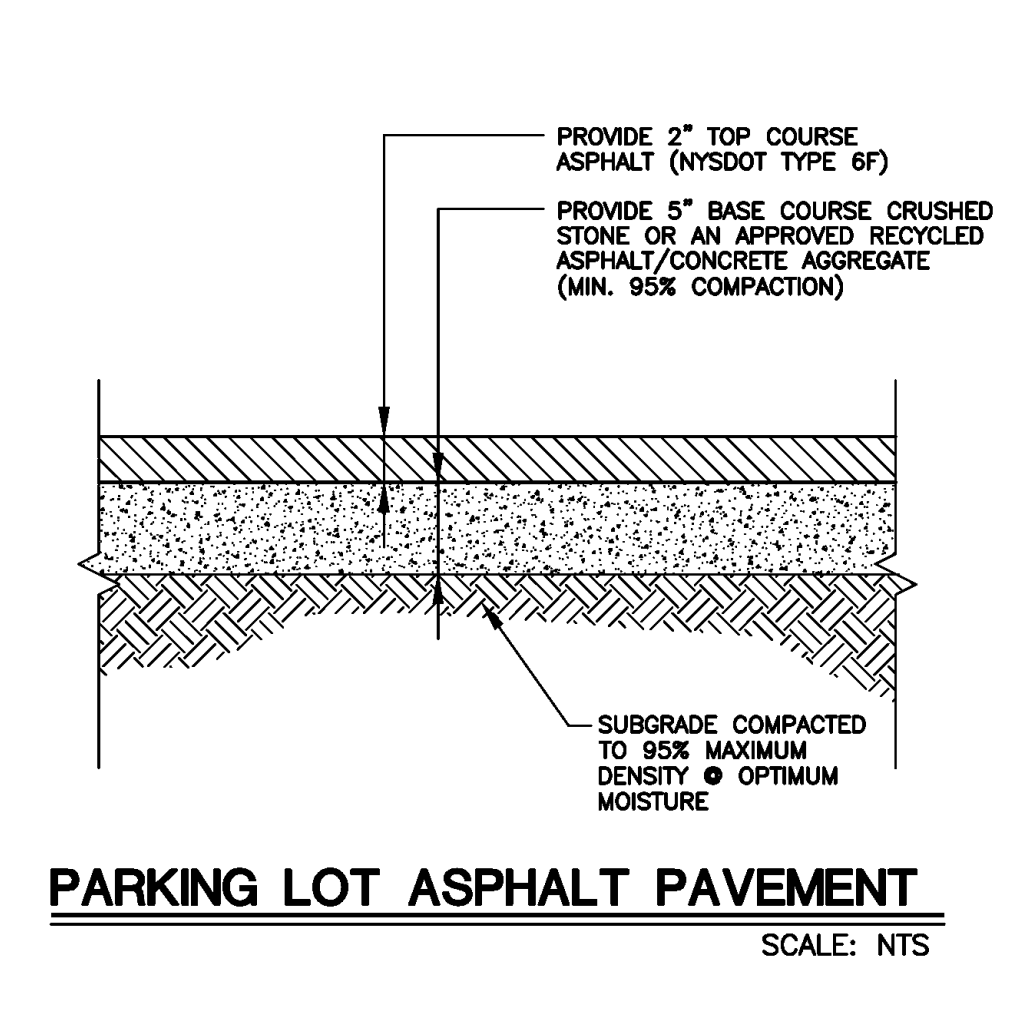
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 PROJECT NO: 1 FLOWERFIELD DRIVE ST. JAMES, NY 11780

TITLE: LANDSCAPE PLAN
 DISCIPLINE: CIVIL

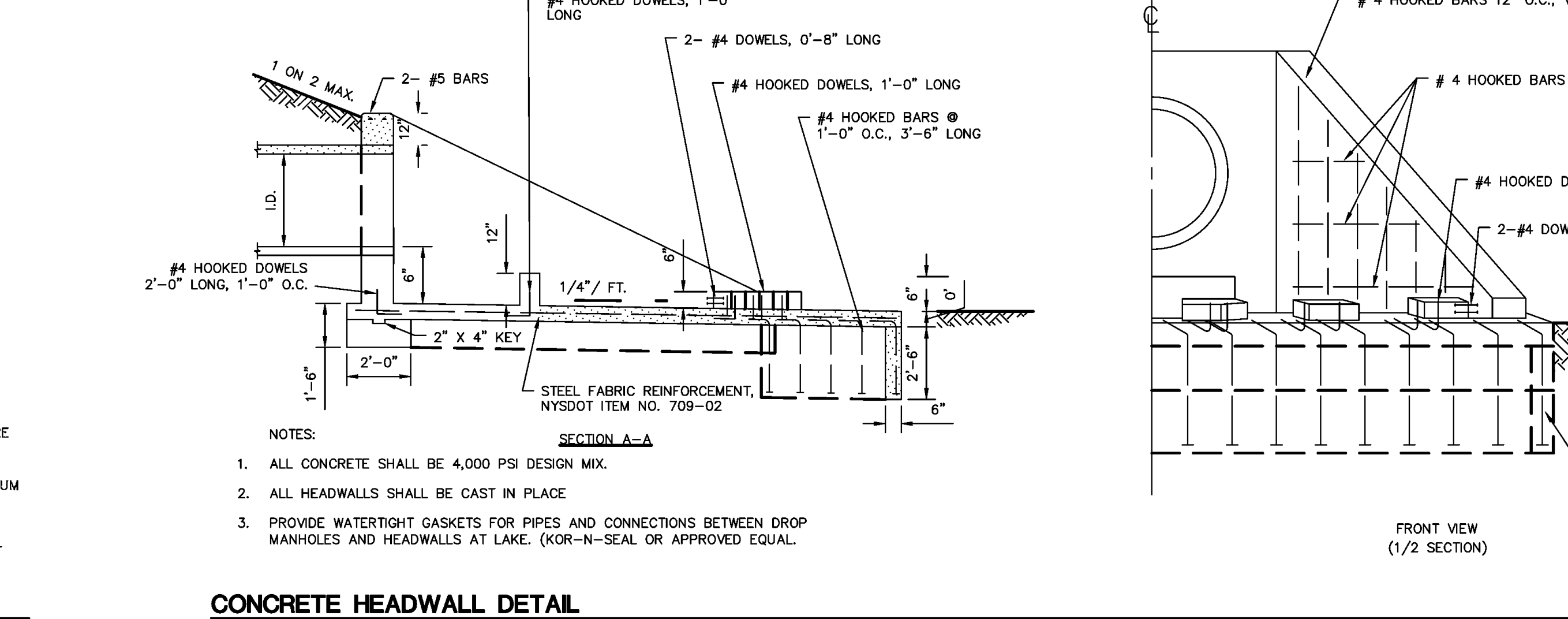
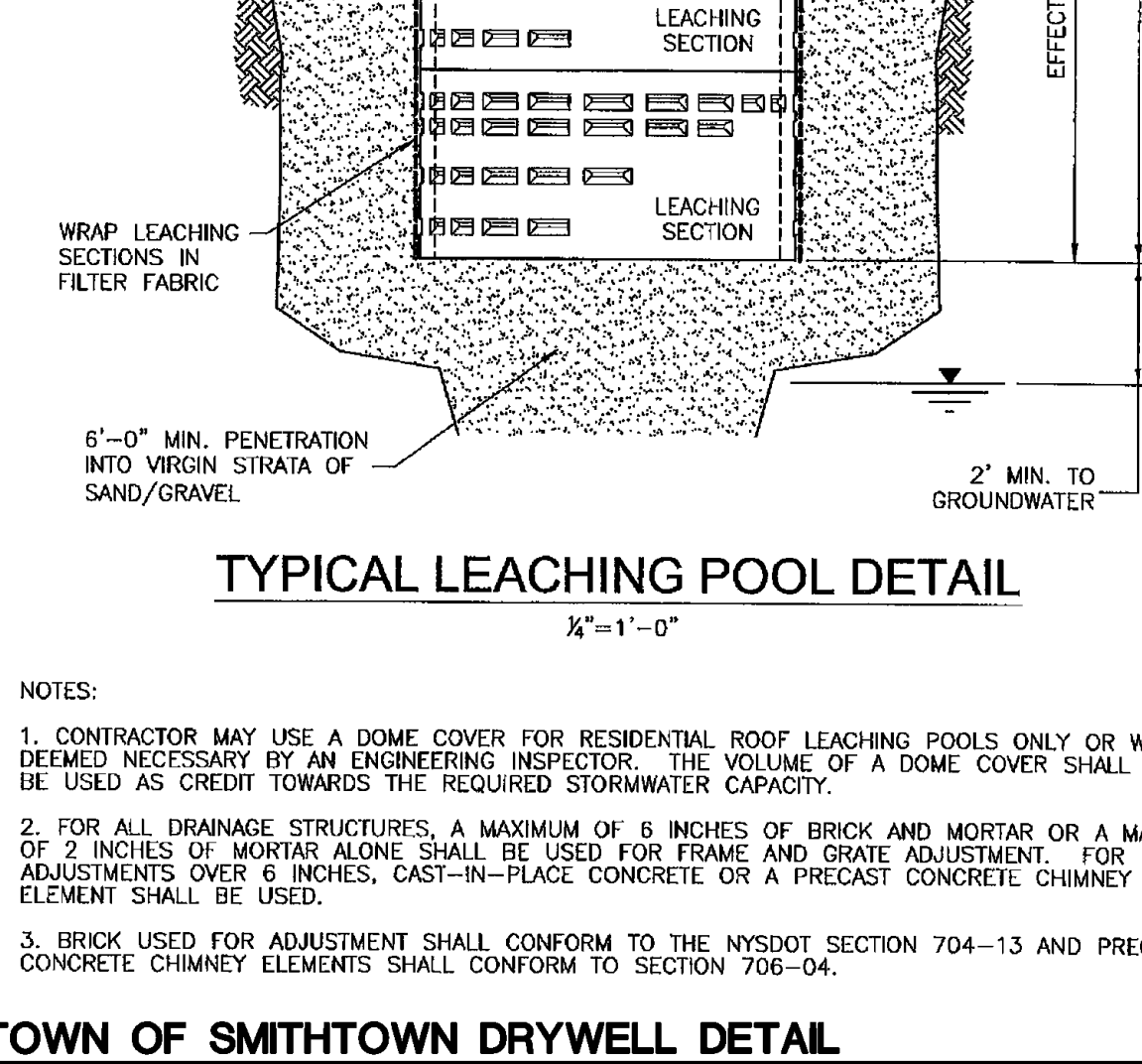
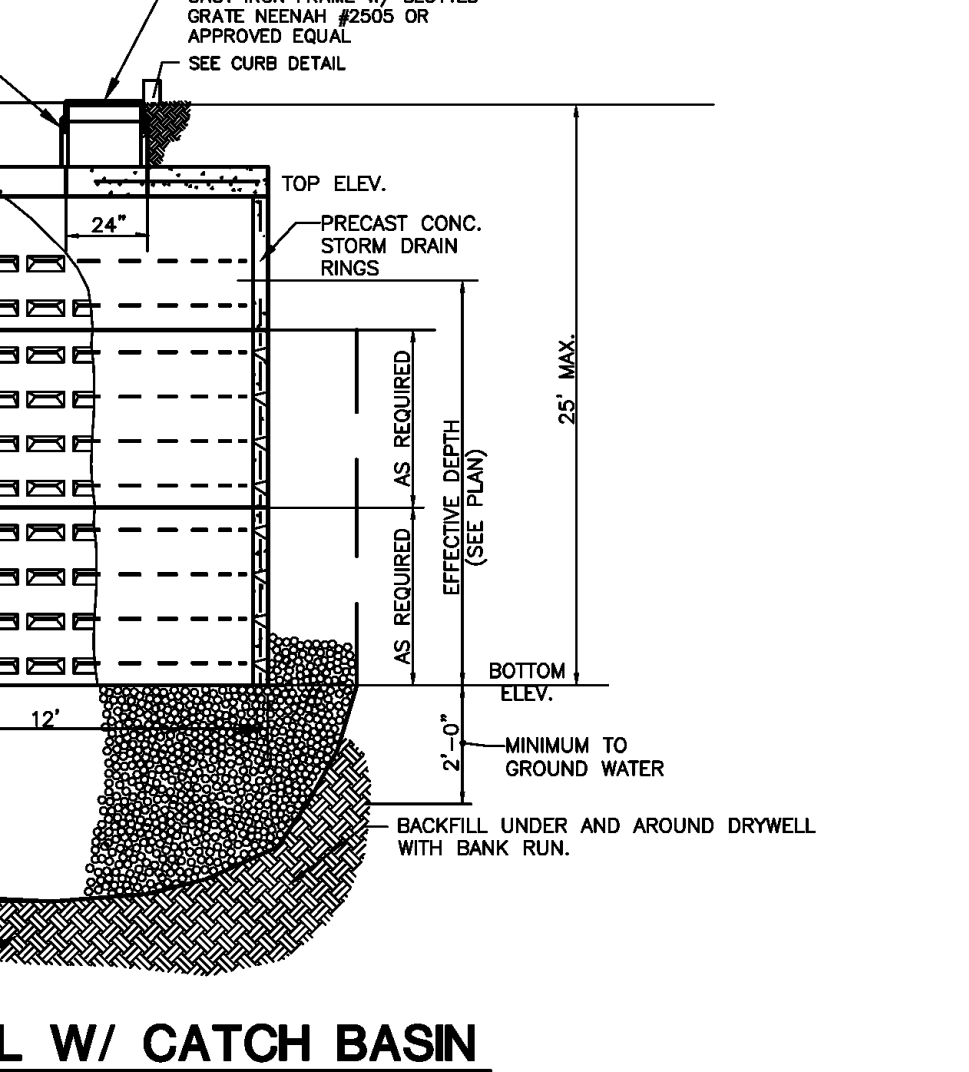
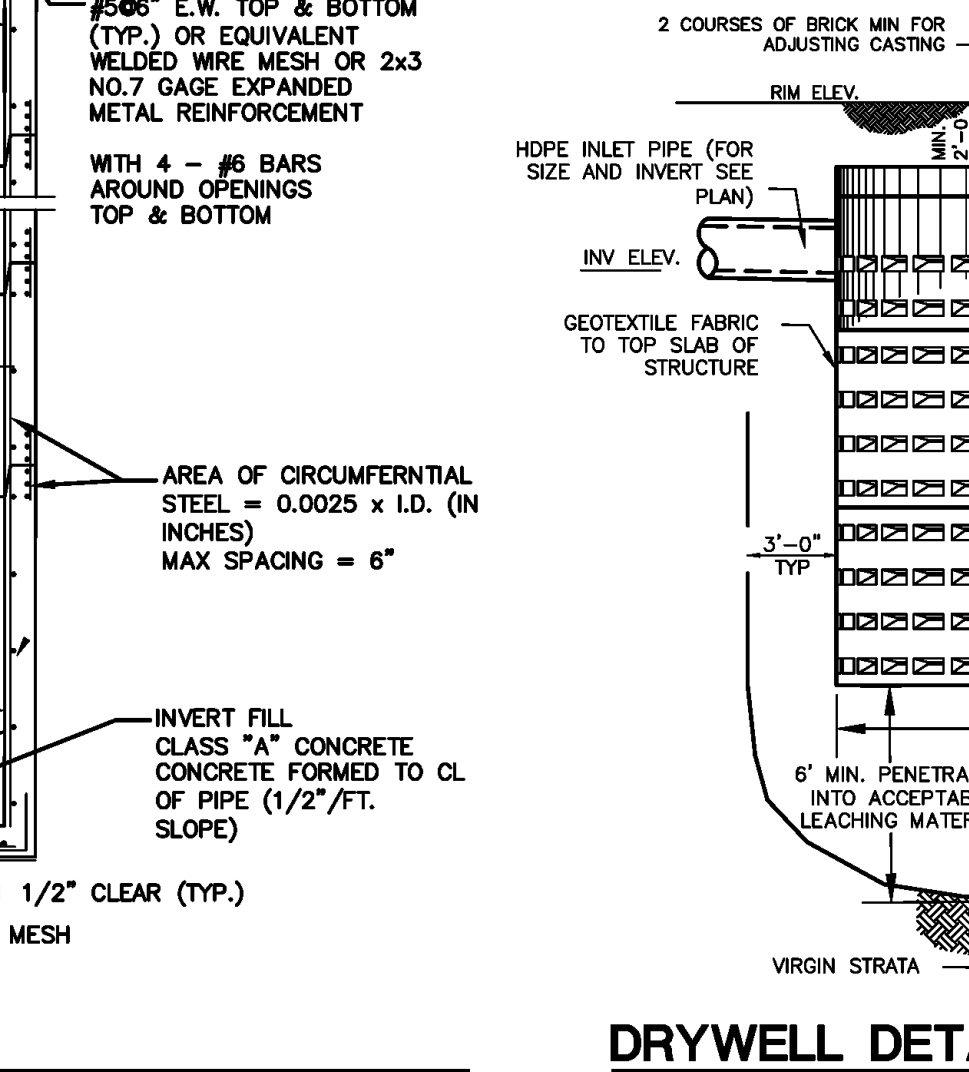
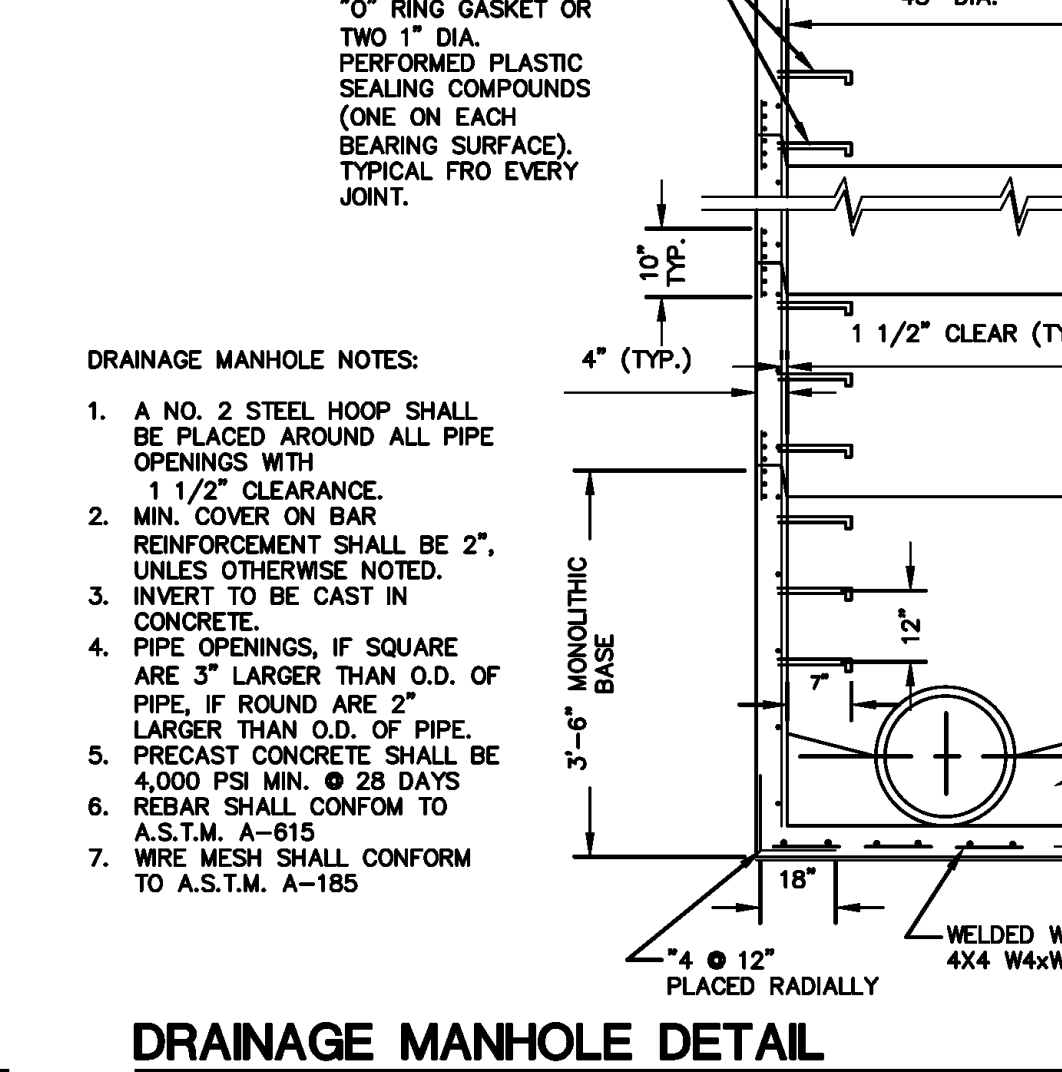
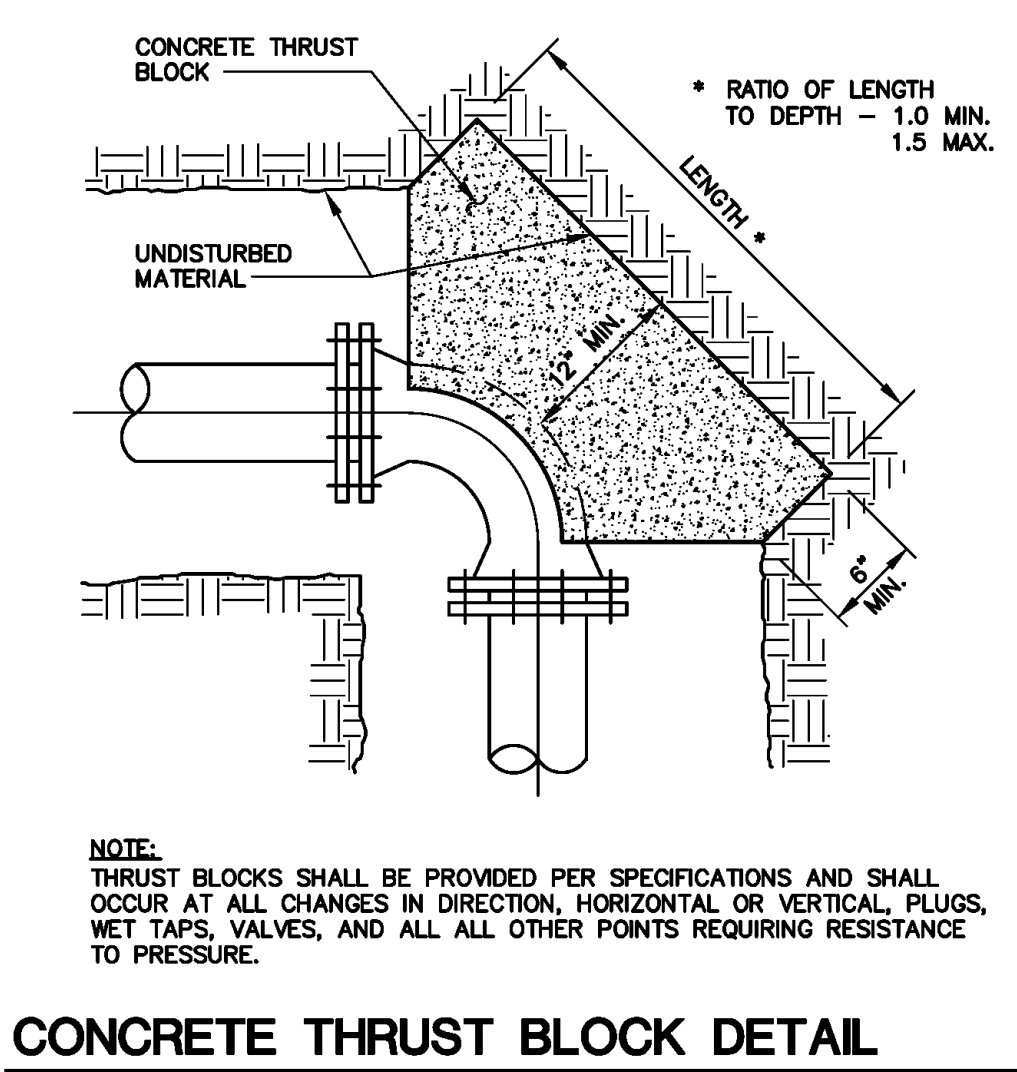
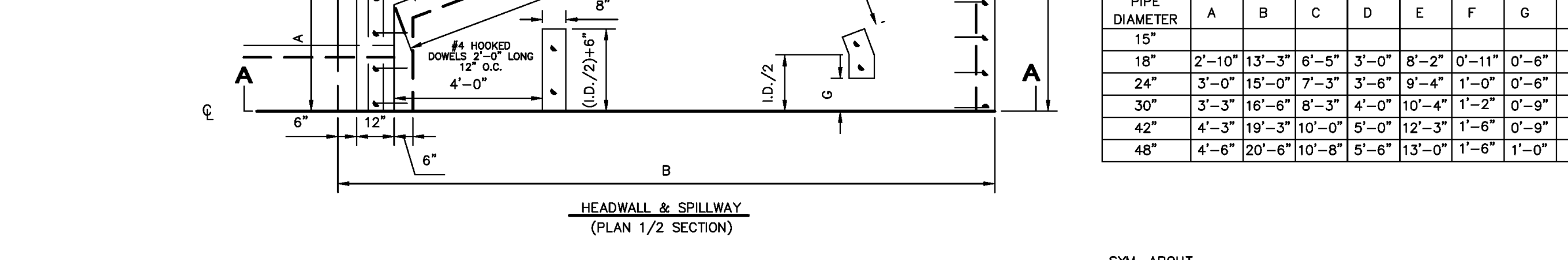
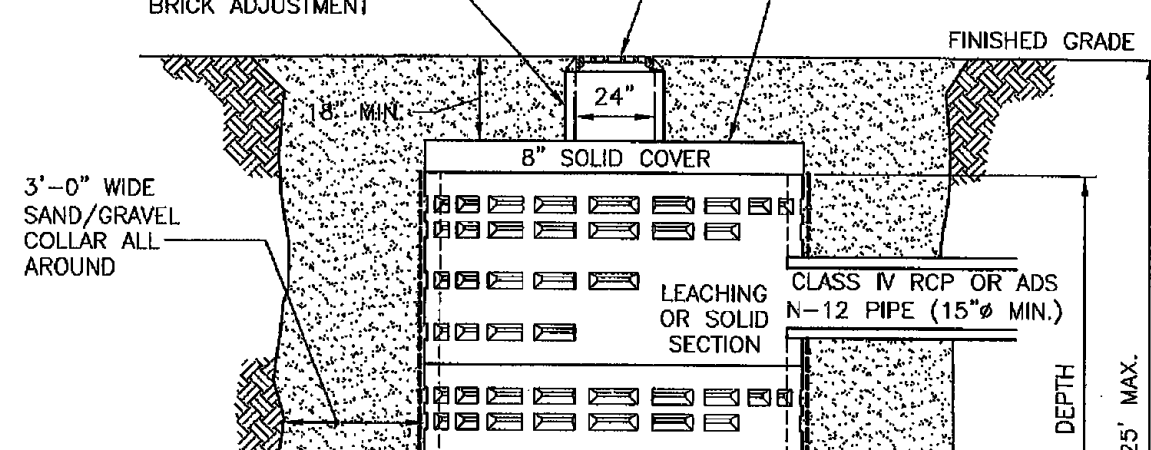
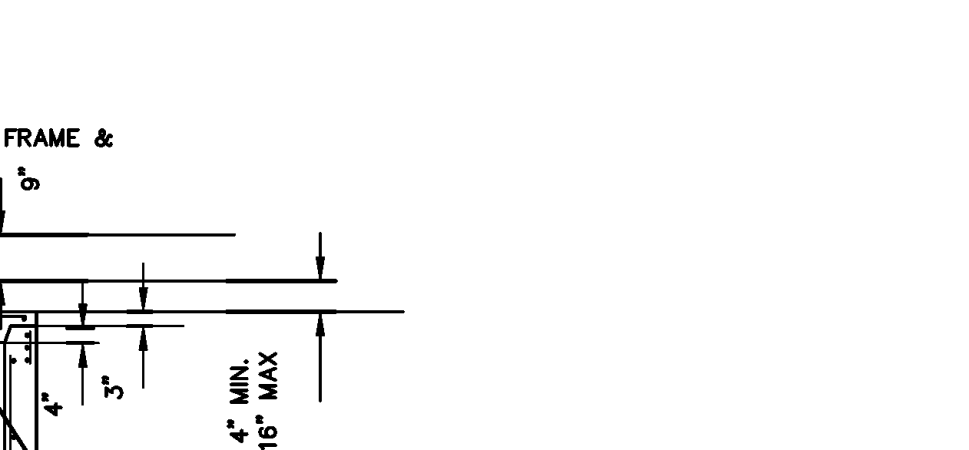
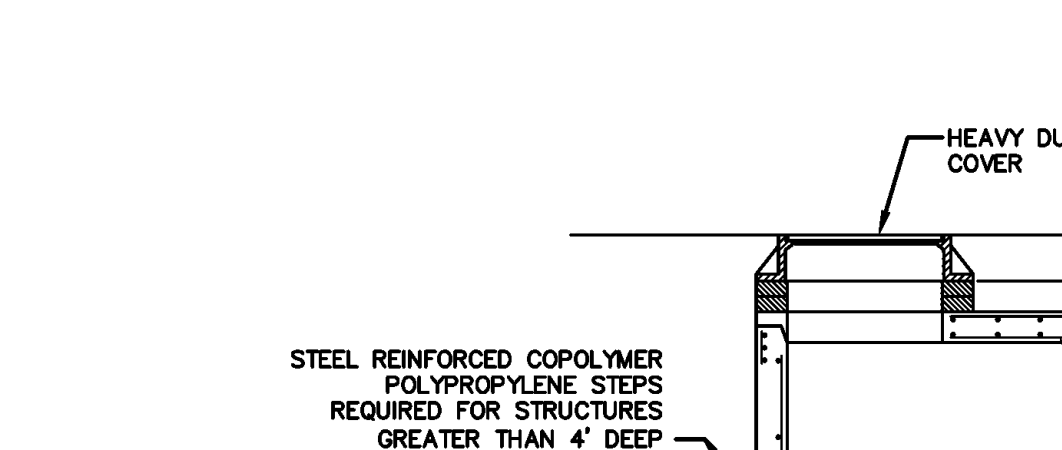
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 DESIGNED BY: OC
 DRAWN BY: OC
 CHECKED BY: RZ

PROJECT NO: CE0338A
 JOB NO: CE0338A
 DATE: APRIL 2020
 SCALE: AS SHOWN

DRAWING NO: C-19
 SHEET 20 OF 22



THRUST BLOCK BEARING AREA REQUIRED IN SQ. FT.	FITTING TYPE				
PIPE SIZE	11-1/4"	22-1/2"	45"	90"	TEE
4"-8"	1.0	1.0	2.0	3.0	2.0
8"	2.0	2.0	3.0	5.0	4.0
10"	2.5	2.5	4.5	8.5	6.0
12"	2.5	3.5	6.5	12.0	8.5



NO.	DATE	REVISION DESCRIPTION	INT.
1	01/18/2020	REVISED PER TOWN ENGINEER AND SCOH'S	OC

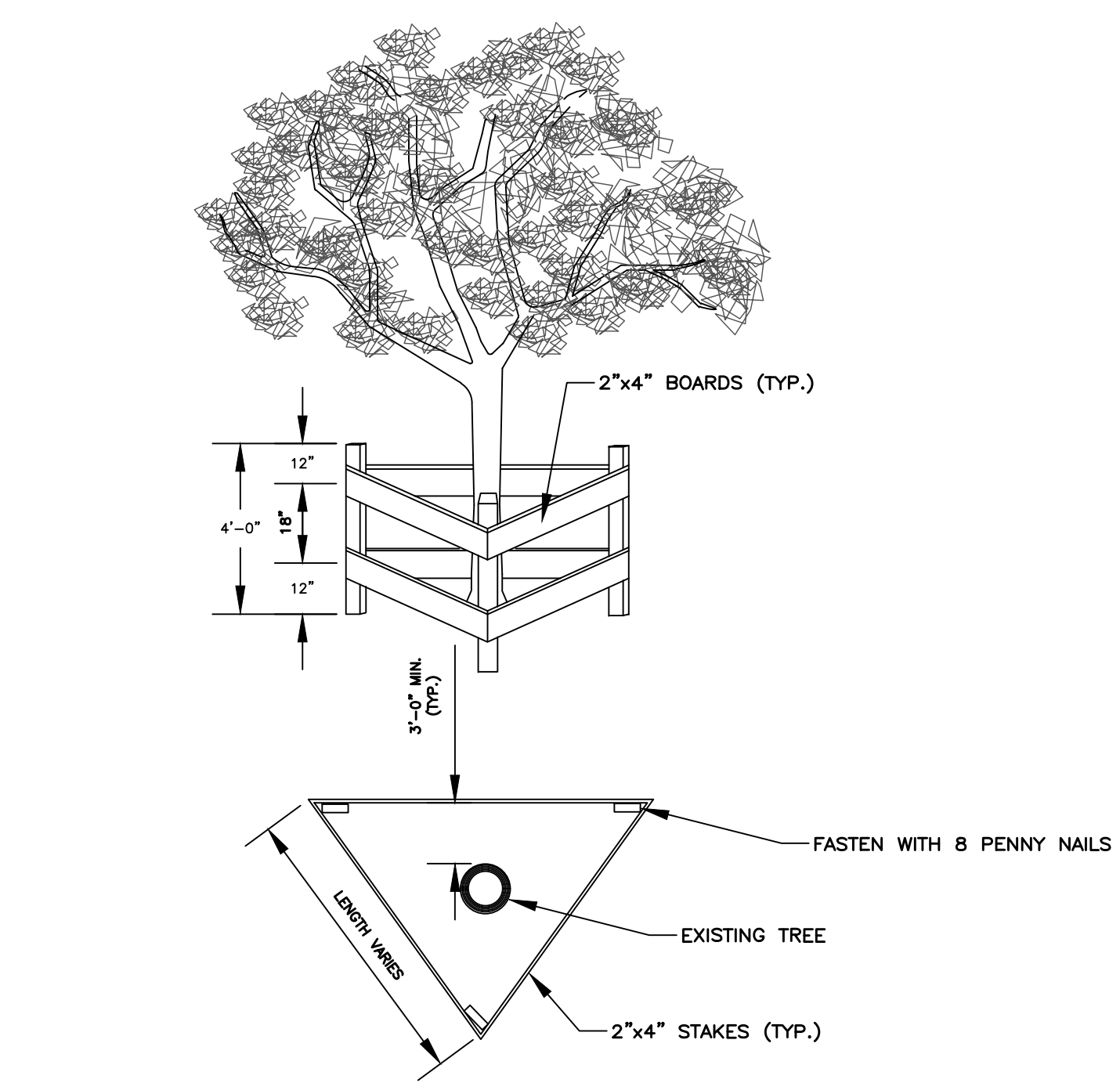
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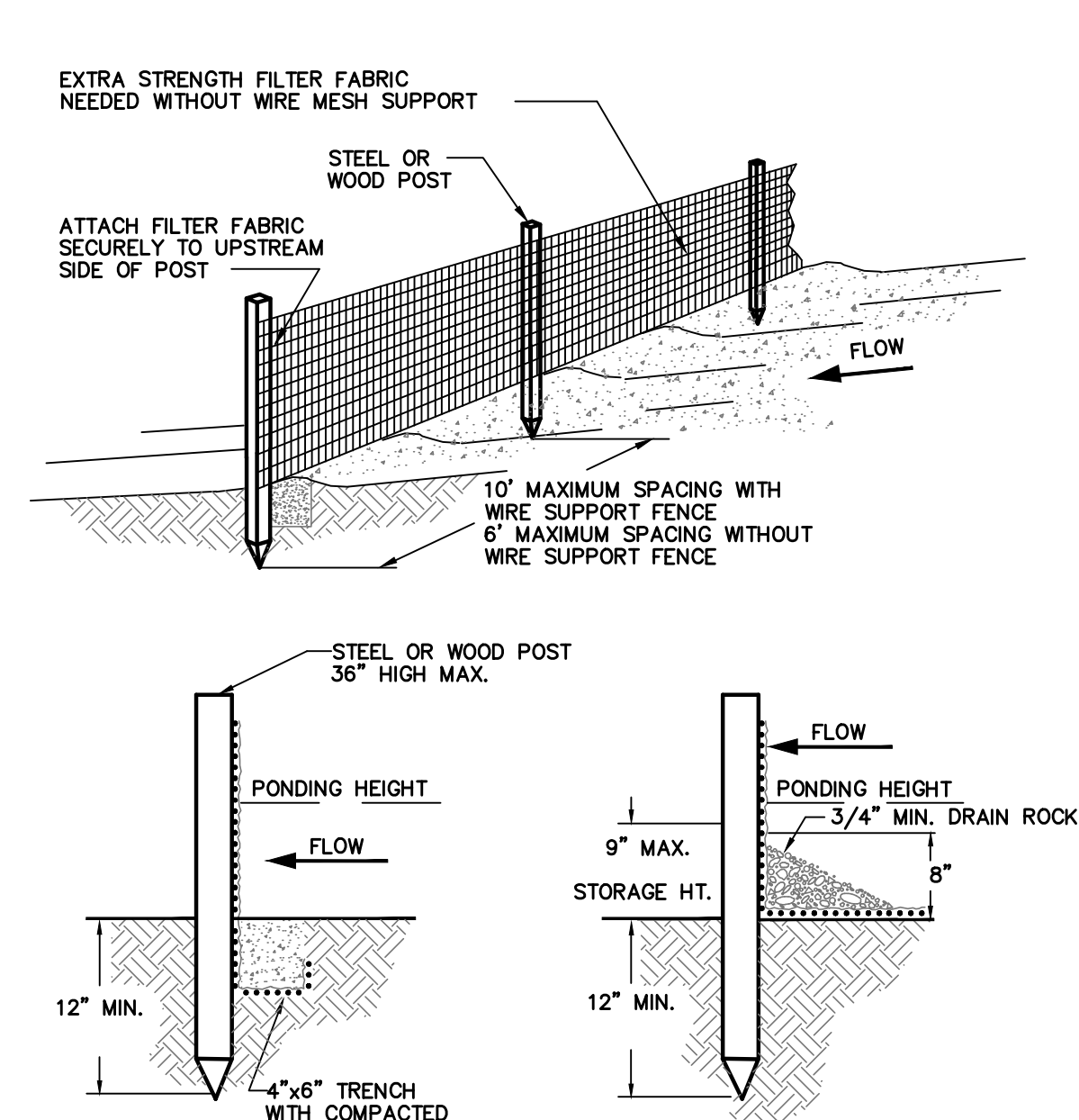
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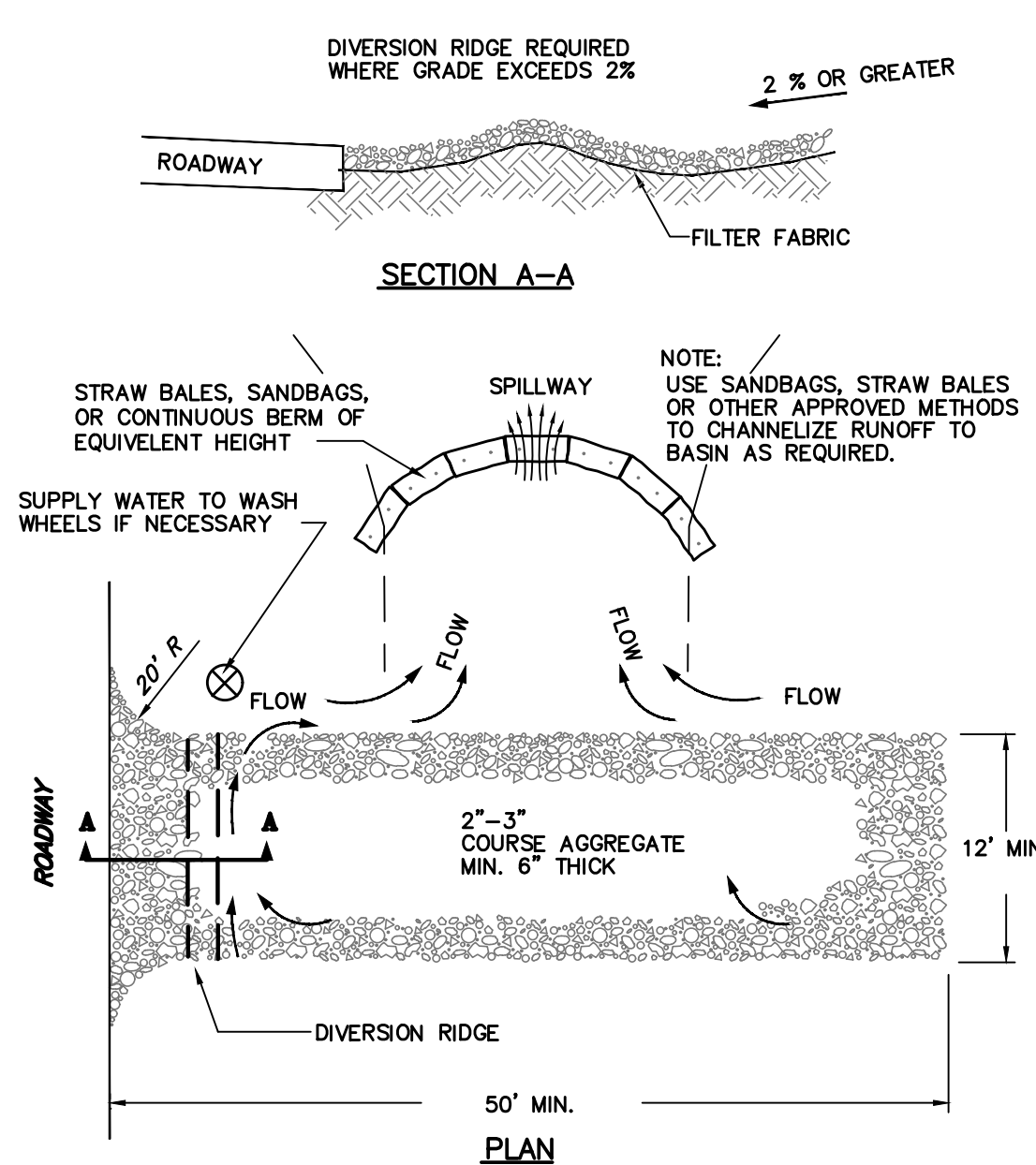
TREE PROTECTION

SCALE: NTS



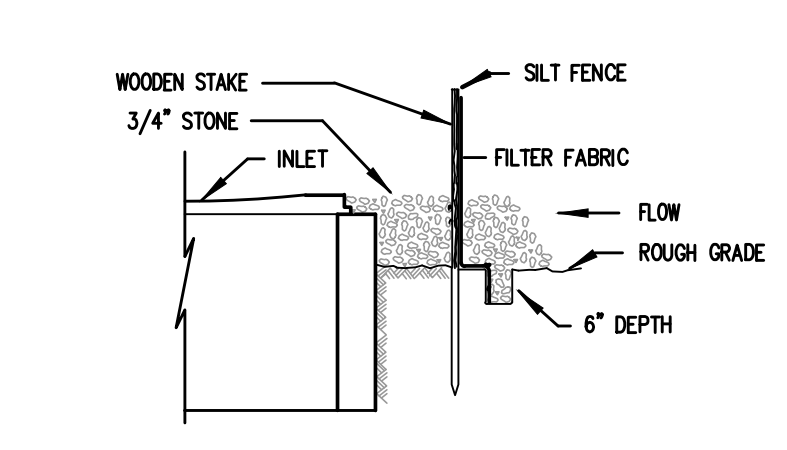
SILT FENCE DETAIL

SCALE: NTS



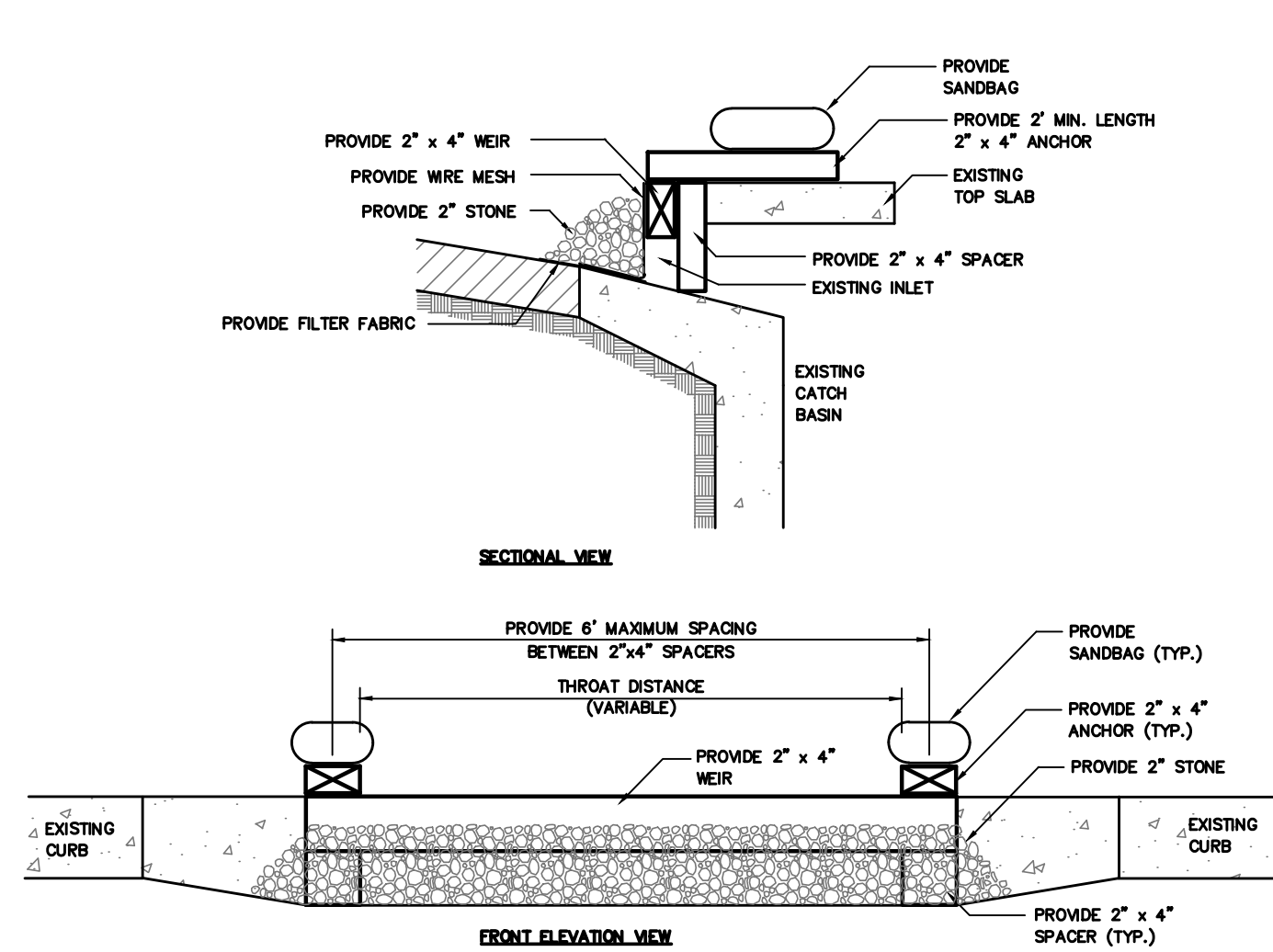
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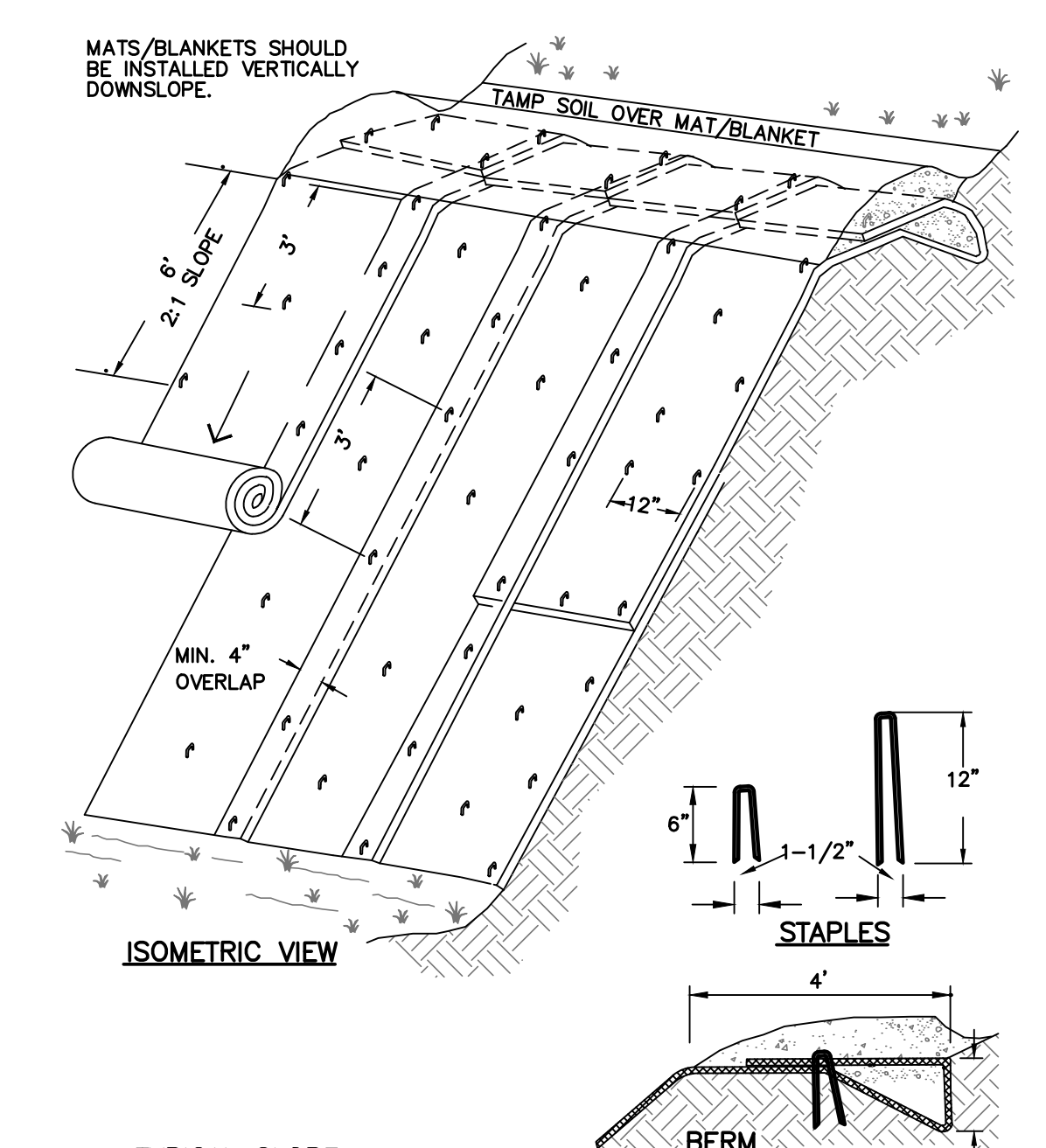
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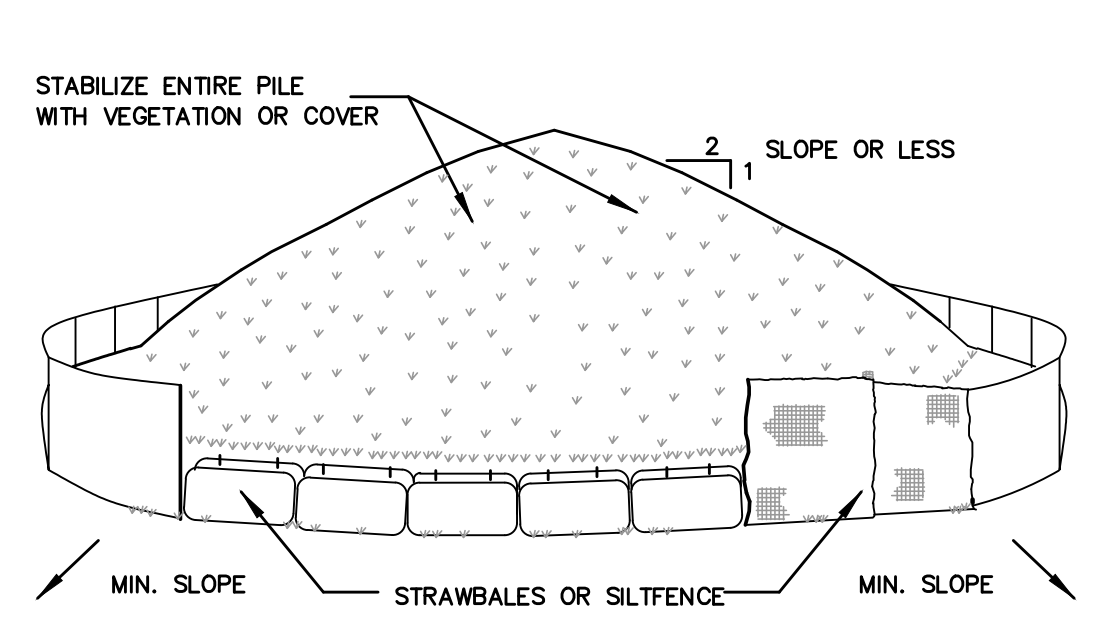
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SCALE: NTS



EROSION CONTROL BLANKETS ON SLOPE INSTALLATION

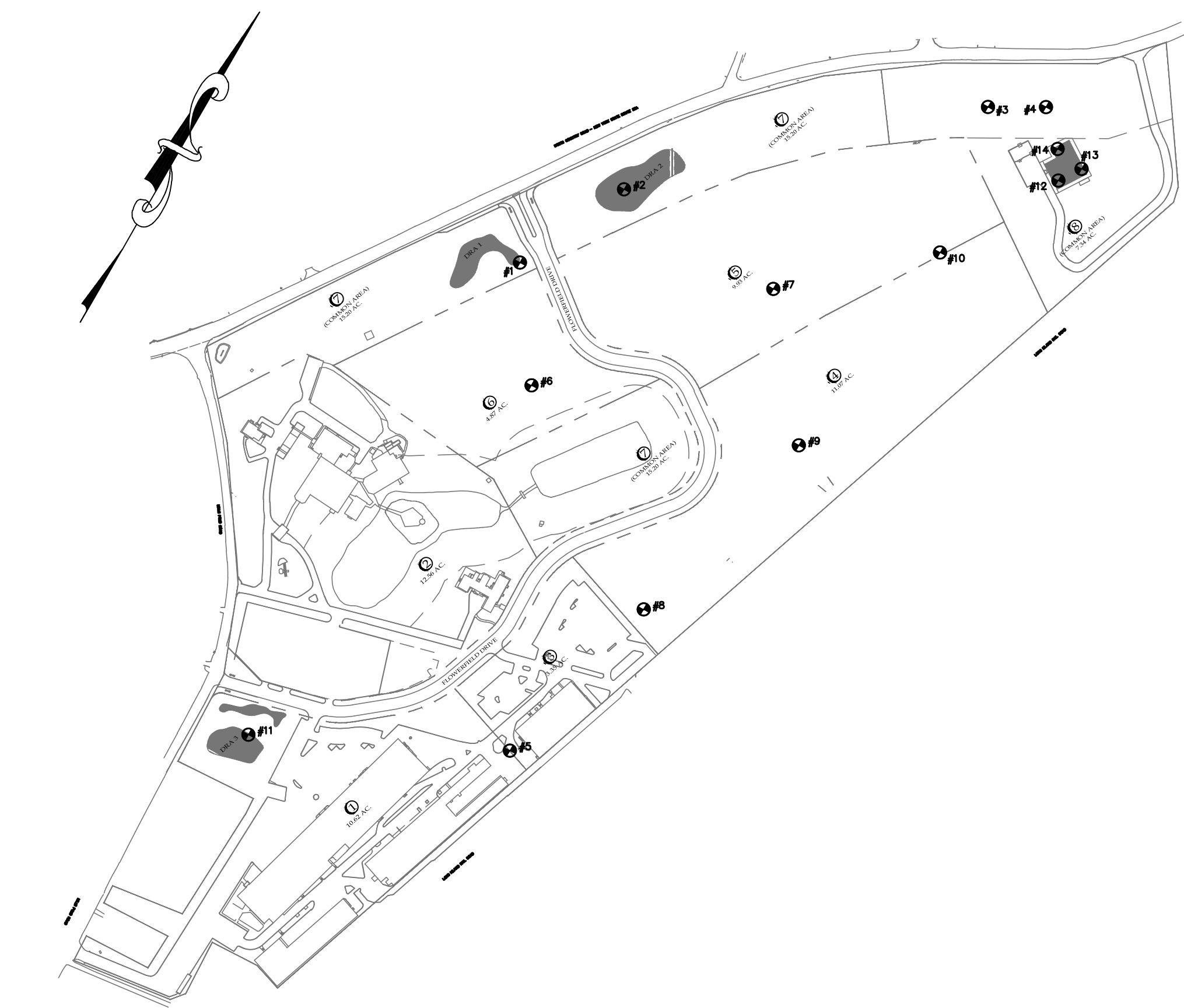
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SOIL STOCKPILING

SCALE: NTS

SOIL BORING DATA



BORING LOCATION MAP

SCALE: 1"=300'

Boring ID	Topsoil	1-2'	2-3'	3-4'	4-5'	5-6'	6-7'	7-8'	8-9'	9-10'	10-11'	11-12'	12-13'	13-14'	14-15'	15-16'	16-17'	17-18'	18-19'
B-1 EL. ±149.0	0.0-0.5	0.5-2.0	2.0-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-40.0	06-12-19											
B-2 EL. ±150.5	0.0-0.4	0.4-2.0	2.0-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-38.6	06-12-19											
B-3 EL. ±155.2	0.0-0.5	0.5-2.0	2.0-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-40.0	06-14-19											
B-4 EL. ±154.3	0.0-0.5	0.5-2.0	2.0-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-40.0	06-14-19											
B-5 EL. ±154.8	0.0-0.3	0.3-2.5	2.5-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-40.0	06-10-19											
B-6 EL. ±154.5	0.0-0.5	0.5-2.0	2.0-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-38.0	06-12-19											
B-7 EL. ±152.5	0.0-0.5	0.5-2.0	2.0-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-40.0	06-11-19											
B-8 EL. ±160.9	0.0-0.5	0.5-2.0	2.0-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-40.0	06-10-19											
B-9 EL. ±158.7	0.0-0.3	0.3-2.5	2.5-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-35.4	06-11-19											
B-10 EL. ±159.8	0.0-0.3	0.3-2.5	2.5-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-40.0	06-11-19											
B-11 EL. ±157.5	0.0-0.5	0.5-2.0	2.0-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-32.0	06-10-19											
B-12 EL. ±156.2	0.0-0.7	0.7-2.5	2.5-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-40.0	06-13-19											
B-13 EL. ±155.7	0.0-0.5	0.5-2.5	2.5-5.0	5.0-7.0	7.0-11.0	11.0-17.0	17.0-40.0	06-13-19											
B-14 EL. ±155.7	0.0-0.7	0.7-4.0	4.0-7.0	7.0-11.0	11.0-15.0	15.0-40.0	06-13-19												

NOTE: 1. ALL SOIL BORINGS WERE PERFORMED BY TERRACON CONSULTANTS, INC. 200 HAMMER HILL ROAD, ROCKY HILL, CT 06067, TEL: (860) 721-1900.

NO.	DATE	REVISION DESCRIPTION	INT.
1	01/18/2020	REVISED PER TOWN ENGINEER AND SCDS	OC

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 300 Corporate Road, St. James, NY 11780 | T: (609) 733-6700
 1000 Corporate Blvd., Suite 1000, St. James, NY 11780 | F: (609) 733-6700

PROJECT NAME: **MAP OF FLOWERFIELD FINAL ENGINEERING**

PROJECT FOR: **1 FLOWERFIELD DRIVE ST. JAMES, NY 11780**

TITLE: **DETAILS (2)**

DISCIPLINE: **CIVIL**

PROJECT ENGINEER: **OC**

DESIGNED BY: **OC**

DRAWN BY: **OC**

CHECKED BY: **RZ**

PROJECT NO.: **CE0338A**

JOB NO.: **CE0338A**

DATE: **APRIL 2020**

SCALE: **AS SHOWN**

C-21

SHEET 22 OF 22

FINAL
Environmental
Impact Statement

For the:

Gyrodyne, LLC

**Map of Flowerfield
Subdivision Application**

Hamlet of St. James, Town of Smithtown
Suffolk County, New York

VOLUME 4 OF 4

December 2020



Cameron Engineering & Associates, LLP

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COUNTY OF SUFFOLK



STEVEN BELLONE
SUFFOLK COUNTY EXECUTIVE

DEPARTMENT OF HEALTH SERVICES

JAMES L. TOMARKEN, MD, MPH, MBA, MSW
Commissioner

October 28, 2019

Gyrodyne, LLC
1 Flowerfield, Suite 24
Saint James, NY 11780
Attn: Marigene Gallicchio

Re: Flowerfield Industrial Campus / Gyrodyne 1 Flowerfield, St. James, NY
SCFR# 04458

Dear Sir or Madam:

This office has reviewed the closure documentation regarding the remediation work performed at the above-referenced location.

Based upon review of the information provided, it has been determined that a satisfactory remediation of the on-site sanitary and storm-water leaching systems has been accomplished. All endpoint samples and waste disposal manifests have been deemed acceptable and no further action is required by the department at this time.

If you have any questions regarding this matter please feel free to contact the undersigned.

Sincerely,

Edward Roe
Senior Public Health Sanitarian
Bureau of Environmental Investigation and Remediation
Office of Pollution Control
(631) 854-2534



Public Health
Prevent. Promote. Protect.

GYRODYNE PROPERTY (INDUSTRIAL AREA)
1 FLOWERFIELD
ST. JAMES, NEW YORK

Page G-3

REMEDIATION REPORT

SUBMITTED TO:



Suffolk County Department of Health Services
Office of Pollution Control
15 Horseblock Place
Farmingville, New York 11738

ON BEHALF OF:

Gyrodyne, LLC
1 Flowerfield
St. James, New York 11780

PREPARED BY:



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630 Johnson Avenue, Suite 7
Bohemia, New York 11716
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Thomas Melia, PG, Sr. Project Manager
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thomasm@pwgrosner.com
niannucci@pwgrosner.com

PWGC Project Number: GCA1704

APRIL 2018



**REMEDIATION REPORT
1 FLOWERFIELD, ST. JAMES, NEW YORK (INDUSTRIAL AREA)**

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1.0 INTRODUCTION

This Remediation Report has been prepared by P.W. Grosser Consulting Inc. (PWGC), on behalf of Gyrodyne, LLC, for the property located at 1 Flowerfield (Industrial Area) in St. James, New York (subject property). This report documents the results of remedial activities performed at the above-referenced site. The scope of work was based upon PWGC's Phase II Environmental Site Assessment (ESA) for the site, additional sampling results, and the requirements of the Suffolk County Department of Health Services (SCDHS) for the subject site.

1.1 Site Description

The subject property is located at 1 Flowerfield in the Hamlet of St. James, New York. The site is located in the Town of Smithtown and Suffolk County. The property is identified in the Suffolk County Tax Map as:

- 0800-040.00-02.00-004.000
- 0800-040.00-02.00-013.003
- 0800-040.00-02.00-014.000
- 0800-040.00-02.00-015.000

The subject property measures approximately 62.4 acres and is occupied by multiple commercial/industrial buildings, identified as follows:

- Building 1 – Two story building with no basement. Used as office space.
- Building 2 – Single story building with no basement. Used as office and commercial space.
- Building 7 - Single story building (partial second story) with no basement. Used as office and commercial space.
- Building 8 - Single story building with no basement. Used as office and commercial space.
- Bus Depot – Fenced parking lot used for storage of school busses. No permanent structures are present.
- Fairgrounds – Empty fields located on the northern portion of the property. No permanent structures are present. Several portable toilets were present at the Fairgrounds during site reconnaissance.

A Vicinity Map is included as **Figure 1**; a site plan is included as **Figure 2**.

1.1 Environmental History

1.1.1 Phase I ESA

PWGC performed a Phase I ESA for the site in June 2017. PWGC's Phase I ESA identified the following Recognized Environmental Concerns (REC) for the Industrial Area portion of the subject property:

- The site is currently and has historically been used for industrial purposes, including woodworking,



auto repair, machine shop, and HVAC, from the 1960s to present. The usages are commonly associated with the storage and use of hazardous substances and petroleum products at the site; usage/storage of such materials was confirmed in several locations during site reconnaissance. The presence of onsite sanitary systems, floor drains, and storm water drywells associated with the industrial portion of the property, represent pathways for such substances to potentially have been released to the environment. Such releases have been documented at the site in the past.

- Multiple current and past tenants within the industrial portion of the site are identified as RCRA hazardous waste generators, dating back to at least 1987 based on database records. As such, it can be assumed that hazardous wastes have been generated and stored at the site for at least 30 years. The presence of onsite sanitary systems, floor drains, and storm water drywells associated with the industrial portion of the property, represent pathways for such substances to potentially have been released to the environment. Such releases have been documented at the site in the past.
- Several chemical storage areas were observed at the site during site reconnaissance. Chemicals identified included waste oil and automotive fluids, paints, stains, solvents, and landscaping and maintenance supplies. The presence of onsite sanitary systems, floor drains, and storm water drywells associated with the property, represent pathways for such substances to potentially have been released to the environment. Such releases have been documented at the site in the past.
- Multiple floor drains, sanitary systems and storm drains were identified at the site. Floor drains were identified within the Building 7 boiler rooms. Each active building at the site is equipped with at least one onsite sanitary system. Multiple sanitary systems were observed at Building 1 (two systems), and Building 7 (five systems). Multiple storm drains and catch basins were observed throughout the paved portions of the site. Industrial area sanitary systems and storm drains were sampled in 2011. At that time, multiple structures were determined to be impacted, and remediated under the oversight of SCDHS. Remediation was successful and SCDHS issued a No Further Action letter to the site. As the industrial usage of the site, which presumably caused the impact identified in 2011, has continued, it is possible that additional discharges have occurred since remediation was completed.

1.1.2 Phase II ESA

Based on the findings of the Phase I ESA, PWGC conducted a Phase II ESA for the subject property in September 2017. The Phase II ESA consisted of the following:

- Characterization sampling of the Industrial Area sanitary systems and storm drains.



Based on conversations with SCDHS, characterization sampling included primary sanitary structures (e.g., septic tanks and primary cesspools), storm drains remediated in 2011, and additional storm drains identified by field screening.

Based on the results of the Phase II ESA, PWGC offered the following conclusions:

- A total of 17 soil samples were collected from sanitary structures within the Industrial Area.
- Of the 17 sanitary structures sampled, 13 contained VOC, SVOC, or metals impact in excess of SCDHS Action Levels.
- A total of four soil samples were collected from onsite storm water drains.
- Of the four storm drains sampled, two contained SVOC impact in excess of SCDHS Action Levels.
- Based on analytical results a total of 13 sanitary structures and 2 storm drains required remediation in accordance with SCDHS procedures.

1.1.3 Remediation Work Plan

The September 2017 Phase II ESA was submitted to SCDHS. Based upon their review of the Phase II ESA, SCDHS required characterization sampling of four additional secondary sanitary structures and four additional storm drains. Sample results for these structures were below SCDHS Action Levels. Following completion of additional sampling, SCDHS issued a Notice to Remediate (NTR) indicating that:

- Impacted structures identified in the Phase II ESA be properly remediated.
- Collection of characterization samples from storm drain SD19, and the final discharge point of two white PVC pipes on the south side of Building 2 (later determined to be storm drain SD20).
- Permanent disconnection of the interior sink effluent sources from storm drains SD10, SD15, and SD18.

In December 2017, PWGC submitted a Work Plan to SCDHS documenting the additional characterization sampling described above, and detailing a remedial scope of work to address the requirements of the NTR. The Work Plan was approved by SCDHS in January 2018.

A copy of the NTR is included in **Appendix A**.



2.0 REMEDIAL ACTIVITIES

Based on the Phase II ESA, and SCDHS directive PWGC performed remedial activities at the site consisting of the following:

- Remediation of onsite sanitary structures 7ST, 9ST, 9ST1, 9SLPC, 9PLP, 10ST, 12ST, 12PLP, 12PLP1 (MH-1), 13ST, 13PLP, 11ST, 11SLP, AND 14ST.
- Remediation of storm drains SD13 and SD17.
- Characterization sampling of storm drains SD19 and SD20.
- Permanent disconnection of interior sink effluent sources from storm drains SD10, SD15 and SD18.

2.1 Remediation

Based on the findings of PWGC's Phase II ESA, onsite sanitary structures 7ST, 9ST, 9ST1, 9SLPC, 9PLP, 10ST, 12ST, 12PLP, 12PLP1 (MH-1), 13ST, 13PLP, 11ST, 11SLP, AND 14ST were remediated from February 26, 2018 to March 5, 2018 in accordance with SCDHS requirements. Remedial activities were performed by Clearbrook of Deer Park, New York under the oversight of PWGC personnel. It should be noted that initially, structure 13ST was believed to be a solid bottom septic tank, however during remediation, PWGC discovered that the floor of this structure is compromised (cracked and open to the soil below), and as such, was treated as a leaching structure.

2.1.1 Waste Removal

A vacuum powered pump truck was used to remove liquids from the previously mentioned structures. Following removal of liquids, a Guzzler was used to remove impacted sludge and sediment from the impacted structures until all solid material was removed or until clean native material was encountered, dependent on the nature of the structure.

2.1.2 Endpoint Sample Collection

Following removal of sediments from the remediated structures, a confirmatory endpoint soil sample was collected from the base of cesspools 9PLP, 9SLPC, 12PLP1(MH-1), 12PLP, 13PLP, and 11SLP, septic tank 13ST, and storm drains SD13 and SD17 to document the effectiveness of the cleanout. Endpoint samples were collected using a properly decontaminated hand auger, placed in laboratory supplied glassware, and stored in a cooler on ice for transport to the laboratory. SCDHS personnel were onsite to inspect each structure following remediation and to observe sampling of the structures.

As septic tanks 7ST, 9ST, 9ST1, 10ST, 12ST, 11ST, and 14ST are solid bottom structures, no confirmatory

endpoint samples were required.

2.1.3 Laboratory Analysis

Samples were collected in pre-cleaned laboratory supplied glassware and stored in a cooler packed with ice for shipment to the analytical laboratory. Samples were delivered under proper chain-of-custody procedures to Alpha Analytical, Inc. of Westborough, Massachusetts, a NYSDOH ELAP certified laboratory.

Endpoint sample analysis was targeted based upon which compounds exceeded SCDHS Action Levels in each structure. As such, samples were collected for these structures based on the following:

Impacted Structures	Contaminants Targeted
SD-13, SD-17	SVOCs
9PLP, 9SLPC, 12PLP, 13ST, 13PLP	VOCs
12PLP1(MH-1)	VOCs, Metals
11SLP	VOCs, SVOCs

2.1.4 Analytical Results

Endpoint soil sample results were compared to the Cleanup Objectives specified in SCDHS SOP 9-95, Pumpout and Soil Cleanup Criteria. Analytical data is summarized in **Tables 1, 2, and 3**; laboratory analytical reports are included in **Appendix B**.

Endpoint soil samples collected from the structures did not contain impact above SCDHS Cleanup Objectives. As such, it appears the remedial effort on these structures were successful.

2.2 Waste Disposal

Wastes generated during UIC remediation were disposed of at properly permitted facilities, as detailed below. Copies of waste manifests and disposal recipes are included as **Appendix C**.

2.2.1 Soil Disposal

An estimated total of 95.43 tons of non-hazardous sludge and sediment were generated during remediation. Non-hazardous solids were disposed of at Clearbrook of Deer Park New York.

2.2.2 Liquid Disposal

An estimated of 23,000 gallons of non-hazardous liquids were generated during remediation. Liquids were disposed of at Clear Flo Technologies, Inc. of North Lindenhurst, New York.

2.3 Additional Characterization Sampling

In accordance with the NTR, PWGC collected characterization samples from storm drains SD19 and SD20. Storm drain SD20 was confirmed via dye testing to be the discharge point for the two white PVC pipes on the south side of Building 2.

2.3.1 Sample Collection

Samples were collected using a properly decontaminated hand auger, placed in laboratory supplied glassware, and stored in a cooler on ice for transport to the laboratory.

2.3.2 Laboratory Analysis

Samples were collected in pre-cleaned laboratory supplied glassware and stored in a cooler packed with ice for shipment to the analytical laboratory. Samples were delivered under proper chain-of-custody procedures to Alpha Analytical, Inc. of Westborough, Massachusetts, a NYSDOH ELAP certified laboratory. Samples were analyzed for SCDHS List VOCs, SVOCs, and metals.

2.3.3 Analytical Results

Sample results were compared to the Action Levels specified in SCDHS SOP 9-95, Pumpout and Soil Cleanup Criteria. Analytical data is summarized in **Tables 1, 2, and 3**; laboratory analytical reports are included in **Appendix B**.

Samples collected from storm drains SD19 and SD20 did not contain impact above SCDHS Action Levels. As such, no remediation of these structures was necessary.

2.4 Effluent Source Removal

As directed by SCDHS, sinks within Building 2 and Building 8 discharging to exterior storm drains in violation of the Suffolk County Sanitary Code, have been permanently disconnected. Photo documentation is included in **Appendix D**.

3.0 CONCLUSIONS AND RECOMMENDATIONS

PWGC implemented a remediation program for UIC structures at the property located at 1 Flowerfield (Industrial Area), St. James, New York. The scope of work was based upon PWGC's Phase II ESA for the site and the requirements of SCDHS for the subject site, and consisted of:

- Remediation of onsite sanitary structures 7ST, 9ST, 9ST1, 9SLPC, 9PLP, 10ST, 12ST, 12PLP, 12PLP1 (MH-1), 13ST, 13PLP, 11ST, 11SLP, AND 14ST.
- Remediation of storm drains SD13 and SD17.
- Permanent disconnection of interior sink effluent sources from storm drains SD10, SD15 and SD18.

3.1 Remedial Activities

The scope of work for remediation consisted of the removal of liquids and sediment from seven septic tanks, six cesspools, and two storm drains containing impact exceeding SCDHS Action Levels. Remedial activities were performed by Clearbrook of Deer Park, New York under the oversight of PWGC personnel. An estimated total of 95.43 tons of non-hazardous soils were generated during remediation. Non-hazardous soils were disposed of at Clearbrook of Deer Park New York. An estimated total of 23,000 gallons of non-hazardous liquids were generated during remediation. Liquids were disposed of at Clear Flo Technologies, Inc. of North Lindenhurst, New York, New York.

3.2 Endpoint Sample Data

Confirmatory endpoint soil samples were collected from the base of structures 9PLP, 9SLPC, 12PLP1 (MH-1), 12PLP, 13ST, 13PLP, 11SLP, SD13, AND SD17 to document the effectiveness of the cleanout. As septic tank 7ST, 9ST, 10ST, 12ST, 11ST, and 14ST are solid bottom (non-leaching) structures, no endpoint sample was necessary. Endpoint sample analysis was targeted based upon which compounds exceeded SCDHS Action Levels in each structure.

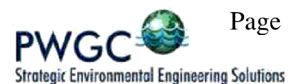
Contaminant concentrations in the endpoint soil samples collected from these structures were below SCDHS Cleanup Objectives.

3.3 Additional Characterization Sampling

In accordance with the NTR, PWGC collected characterization samples from storm drains SD19 and SD20. Samples collected from storm drains SD19 and SD20 did not contain impact above SCDHS Action Levels.

3.4 Effluent Source Removal

As directed by SCDHS, sinks within Building 2 and Building 8 discharging to exterior storm drains in violation of

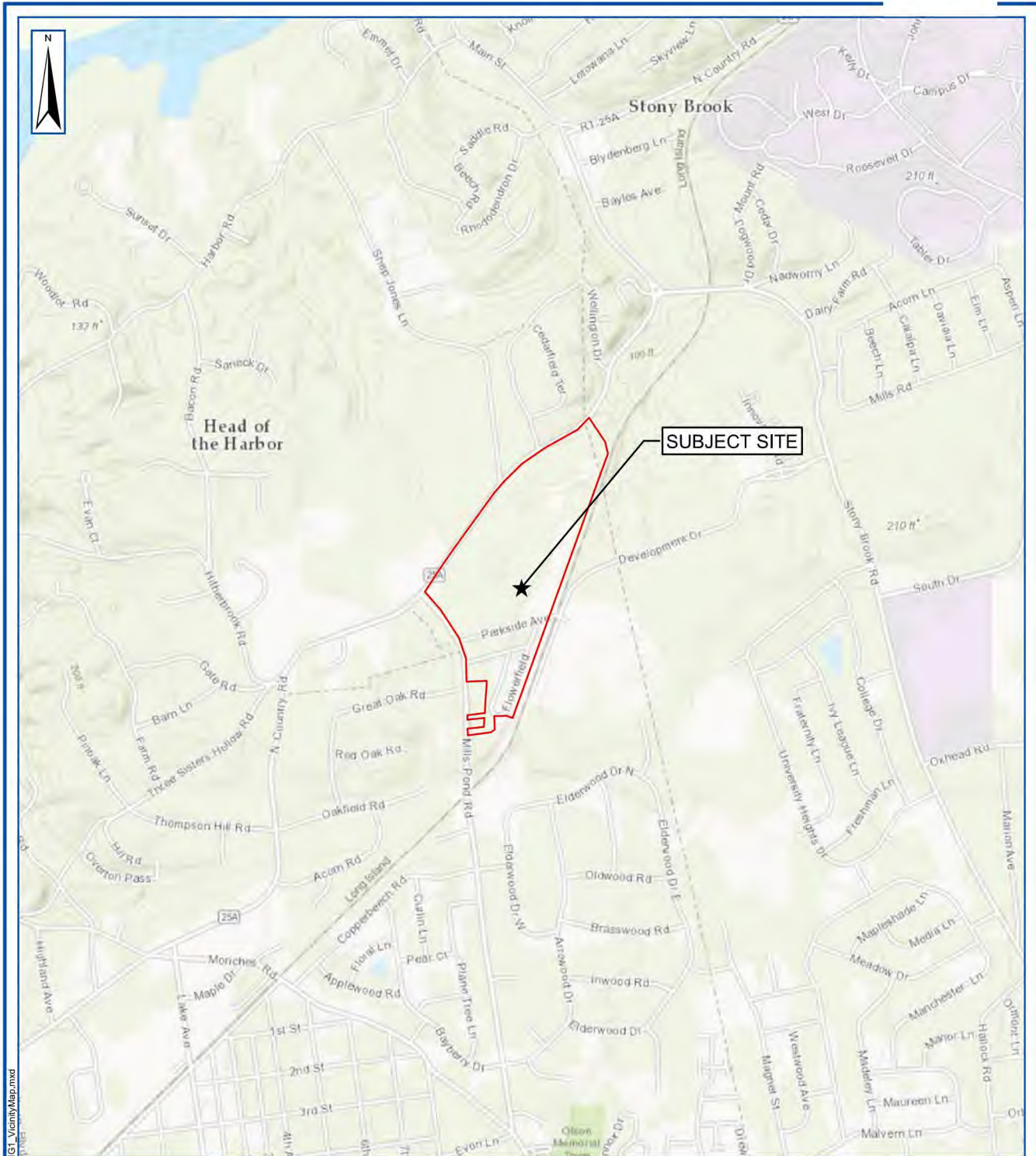


the Suffolk County Sanitary Code, have been permanently disconnected.

3.5 Recommendations

Based on endpoint sample results, it appears that the remedial effort was successful, and PWGC recommends that a No Further Action letter be issued for the site.

FIGURES

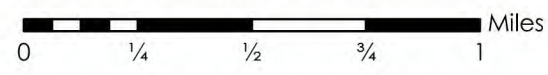


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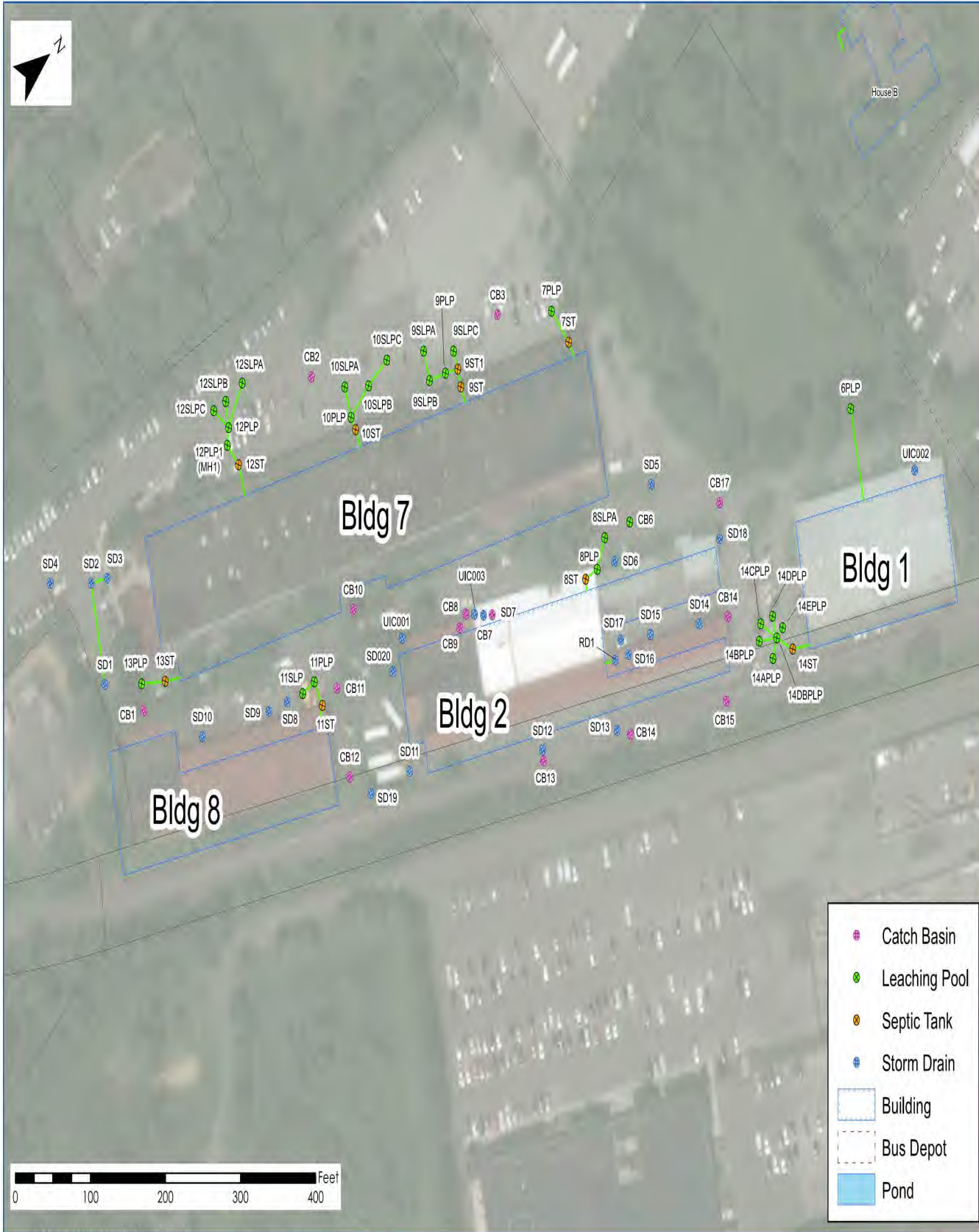


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SUBJECT SITE VICINITY
FLOWERFIELD INDUSTRIAL PARK
ST JAMES, NY



Project:	GCA1701
Date:	8/17/2017
Designed by:	TM
Drawn by:	JCG
Approved by:	TM
Figure No:	1



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Date:	1/26/2018	Drawn by:	JCG
Scale:	AS SHOWN	Approved by:	TM

SITE PLAN
 FLOWERFIELD INDUSTRIAL
 ST JAMES, NY

FIGURE NO:
 2

TABLES

CLIENT SAMPLE ID: LABORATORY ID: SAMPLING DATE	CAS Number	SCDHS Action Level	SCDHS Cleanup Objective	EP-9PLP L1806744-02 2/27/2018	EP-9SLPC L1806744-01 2/27/2018	EP-11SLP L1807506-01 3/5/2018	EP-12PLP L1807506-04 3/5/2018	EP-12PLP1 (MH-1) L1806744-03 2/27/2018	EP-13ST L1807506-02 3/5/2018	EP-13PLP L1807506-03 3/5/2018	SD-19 1/25/2018 L1802820-02	SD-20 1/25/2018 L1802820-01
Volatle Organic Compounds												
1,1,1,2-Tetrachloroethane	630-20-6	600	300	0.3 U	0.34 U	0.27 U	0.33 U	0.38 U	0.4 U	0.31 U	2.1 U	1.2 U
1,1,1-Trichloroethane	71-55-6	1,400	700	0.33 U	0.37 U	0.3 U	0.36 U	0.42 U	0.44 U	0.34 U	2.1 U	1.2 U
1,1,2,2-Tetrachloroethane	79-34-5	800	400	0.28 U	0.32 U	0.25 U	0.3 U	0.36 U	0.38 U	0.29 U	2.1 U	1.2 U
1,1,2-Trichloroethane	79-00-5	200	100	0.29 U	0.33 U	0.26 U	0.32 U	0.37 U	0.39 U	0.3 U	3.2 U	1.8 U
1,1-Dichloroethane	75-34-3	600	300	0.25 U	0.28 U	0.23 U	0.28 U	0.32 U	0.34 U	0.26 U	3.2 U	1.8 U
1,1-Dichloroethene	75-35-4	600	300	0.35 U	0.39 U	0.32 U	0.38 U	0.44 U	0.47 U	0.36 U	2.1 U	1.2 U
1,1-Dichloropropene	563-58-6	200	100	0.31 U	0.35 U	0.28 U	0.34 U	0.39 U	0.41 U	0.32 U	10 U	6 U
1,2,3-Trichlorobenzene	87-61-6	17,000	8,300	0.23 U	0.26 U	0.21 U	0.26 U	0.3 U	0.32 U	0.24 U	10 U	6 U
1,2,3-Trichloropropane	96-18-4	100	50	0.16 U	0.19 U	0.15 U	0.18 U	0.21 U	0.22 U	0.17 U	21 U	12 U
1,2,4,5-Tetramethylbenzene	95-93-2	18,000	8,800	0.14 U	0.16 U	0.13 U	0.17 U	1.4 J	0.2 U	0.15 U	0.72 J	0.75 J
1,2,4-Trichlorobenzene	120-82-1	17,000	8,300	0.2 U	0.23 U	0.18 U	0.22 U	0.26 U	0.27 U	0.21 U	10 U	6 U
1,2,4-Trimethylbenzene	95-63-6	7,200	3,600	0.17 U	0.2 U	0.16 U	0.2 U	1.8 J	0.23 U	0.18 U	0.79 J	1.3 J
1,2-Dibromo-3-chloropropane	96-12-8	100	50	0.37 U	0.42 U	0.34 U	0.41 U	0.47 U	0.5 U	0.38 U	10 U	6 U
1,2-Dibromoethane	106-93-4	600	300	0.18 U	0.21 U	0.17 U	0.2 U	0.24 U	0.25 U	0.19 U	8.4 U	4.8 U
1,2-Dichlorobenzene	95-50-1	2,200	1,100	0.17 U	0.19 U	0.15 U	0.29 J	0.72 J	0.23 U	0.18 U	10 U	6 U
1,2-Dichloroethane	107-06-2	100	50	0.23 U	0.26 U	0.21 U	0.25 U	0.29 U	0.31 U	0.24 U	2.1 U	1.2 U
1,2-Dichloropropane	78-87-5	100	50	0.21 U	0.24 U	0.19 U	0.23 U	0.27 U	0.29 U	0.22 U	7.4 U	4.2 U
1,3,5-Trimethylbenzene	108-67-8	16,800	8,400	0.15 U	0.17 U	0.14 U	2.2 J	2.5 J	0.2 U	0.16 U	0.53 J	0.69 J
1,3-Dichlorobenzene	541-73-1	4,800	2,400	0.2 U	0.23 U	0.18 U	0.22 U	0.26 U	0.28 U	0.21 U	10 U	6 U
1,3-Dichloropropane	142-28-9	600	300	0.17 U	0.19 U	0.16 U	0.19 U	0.22 U	0.23 U	0.18 U	10 U	6 U
1,4-Dichlorobenzene	106-46-7	3,600	1,800	0.5 J	0.8 J	0.42 J	1 J	2.3 J	0.24 J	0.18 U	10 U	1.9 J
2,2-Dichloropropane	594-20-7	600	300	0.42 U	0.48 U	0.38 U	0.46 U	0.54 U	0.57 U	0.44 U	10 U	6 U
2-Butanone	78-93-3	400	200	0.64 U	0.73 U	2.6 J	0.71 U	0.82 U	0.87 U	0.67 U	70	27
4-Methyl-2-pentanone	108-10-1	1,400	700	0.23 U	0.26 U	0.21 U	0.25 U	0.29 U	0.31 U	0.24 U	21 U	12 U
Acetone	67-64-1	**	**	250	11	12	6.1 J	22	54	10	170	110
Benzene	71-43-2	120	60	0.18 U	0.2 U	0.16 U	0.92 J	0.76 J	0.24 U	0.19 U	2.1 U	1.2 U
Bromobenzene	108-86-1	2,800	1,400	0.2 U	0.23 U	0.18 U	0.22 U	0.26 U	0.28 U	0.21 U	10 U	6 U
Bromochloromethane	74-97-5	400	200	0.33 U	0.38 U	0.3 U	0.37 U	0.43 U	0.45 U	0.35 U	10 U	6 U
Bromodichloromethane	75-27-4	4,600	2,300	0.29 U	0.33 U	0.26 U	0.32 U	0.37 U	0.39 U	0.3 U	2.1 U	1.2 U
Bromoform	75-25-2	13,000	6,300	0.22 U	0.25 U	0.2 U	0.24 U	0.28 U	0.3 U	0.23 U	8.4 U	4.8 U
Carbon tetrachloride	56-23-5	1,600	800	0.32 U	0.36 U	0.29 U	0.35 U	0.41 U	0.44 U	0.34 U	2.1 U	1.2 U
Chlorobenzene	108-90-7	2,200	1,100	0.36 J	0.41 J	0.41 J	4.1	11	0.44 U	0.34 U	2.1 U	1.2 U
Chloroethane	75-00-3	400	200	0.3 U	0.33 U	0.27 U	0.32 U	0.38 U	0.4 U	0.31 U	4.2 U	2.4 U
Chloroform	67-66-3	800	400	0.34 U	0.39 U	0.31 U	0.38 U	0.44 U	0.47 U	0.36 U	3.2 U	1.8 U
cis-1,2-Dichloroethene	156-59-2	500	250	0.32 U	0.36 U	0.29 U	0.35 U	0.41 U	0.43 U	0.33 U	2.1 U	1.2 U
cis-1,3-Dichloropropene	10061-01-5	100	50	0.22 U	0.24 U	0.2 U	0.24 U	0.28 U	0.29 U	0.22 U	2.1 U	1.2 U
Dibromochloromethane	124-48-1	6,200	3,100	0.16 U	0.19 U	0.15 U	0.18 U	0.21 U	0.22 U	0.17 U	2.1 U	1.2 U
Dibromomethane	74-95-3	400	200	0.22 U	0.25 U	0.2 U	0.24 U	0.28 U	0.3 U	0.23 U	21 U	12 U
Dichlorodifluoromethane	75-71-8	600	300	0.47 U	0.53 U	0.42 U	0.51 U	0.6 U	0.63 U	0.49 U	21 U	12 U
Ethylbenzene	100-41-4	2,000	1,000	0.16 U	0.18 U	0.14 U	2.3	0.2 U	0.42 J	0.16 U	0.49 J	1.2 U
Freon-113	76-13-1	12,000	6,000	0.48 U	0.54 U	NA	NA	0.61 U	NA	NA	NA	NA
Hexachlorobutadiene	87-68-3	54,000	27,000	0.32 U	0.37 U	0.29 U	0.36 U	0.42 U	0.44 U	0.34 U	10 U	6 U
Isopropylbenzene	98-82-8	9,400	4,700	0.18 U	0.2 U	0.16 U	0.39 J	0.23 U	0.41 J	0.19 U	2.1 U	1.2 U
Methyl tert butyl ether	1634-04-4	200	100	0.14 U	0.16 U	0.13 U	0.16 U	0.18 U	0.19 U	0.15 U	4.2 U	2.4 U
Methylene chloride	75-09-2	100	50	1.5 U	1.7 U	1.4 U	1.7 U	2 U	2.1 U	1.6 U	21 U	12 U
n-Butylbenzene	104-51-8	12,000	5,900	0.21 U	0.24 U	0.19 U	0.23 U	1.2	0.29 U	0.22 U	0.82 J	0.54 J
n-Propylbenzene	103-65-1	8,000	4,000	0.2 U	0.23 U	0.18 U	0.91 J	0.69 J	0.27 U	0.21 U	0.64 J	0.44 J
Naphthalene	91-20-3	24,000	12,000	0.13 U	0.15 U	0.12 U	0.92 J	2.1 J	0.17 U	0.13 U	1.6 J	1.7 J
o-Chlorotoluene	95-49-8	5,200	2,600	0.21 U	0.23 U	0.19 U	0.23 U	0.26 U	0.28 U	0.22 U	10 U	6 U
o-Xylene	95-47-6	NS	1,600	0.32 U	0.36 U	0.29 U	6	0.72 J	0.43 U	0.33 U	2.2 J	2.4 U
p-Chlorotoluene	106-43-4	5,200	2,600	0.17 U	0.19 U	0.16 U	0.19 U	0.22 U	0.23 U	0.18 U	10 U	6 U
p-Diethylbenzene	105-05-5	26,000	13,000	3.7 U	4.2 U	3.4 U	4.1 U	5.8	5 U	3.9 U	8.4 U	4.8 U
p-Ethyltoluene	622-96-8	9,000	4,500	0.22 U	0.25 U	0.2 U	3.4 J	3.3 J	0.3 U	0.23 U	8.4 U	0.59 J
p-Isopropyltoluene	99-87-6	22,000	11,000	0.48 J	0.5 J	0.17 U	0.38 J	1.5	1.5	0.2 U	1.1 J	2.2
p/m-Xylene	179601-23-1	NS	1,600	0.33 U	0.37 U	0.3 U	3.9	1.1 J	0.44 U	0.34 U	1.3 J	2.4 U
sec-Butylbenzene	135-98-8	12,000	5,900	0.2 U	0.23 U	0.18 U	0.22 U	0.75 J	0.27 U	0.21 U	0.83 J	0.83 J
Styrene	100-42-5	9,200	4,600	0.37 U	0.42 U	0.34 U	0.41 U	0.48 U	0.51 U	0.39 U	4.2 U	2.4 U
tert-Butylbenzene	98-06-6	12,000	5,900	0.23 U	0.26 U	0.21 U	0.25 U	0.3 U	0.31 U	0.24 U	10 U	6 U
Tetrachloroethene	127-18-4	2,600	1,300	0.28 U	0.32 U	0.26 U	0.31 U	0.36 U	0.38 U	0.29 U	2.1 U	1.2 U
Toluene	108-88-3	3,000	1,500	0.9 J	6.9	0.37 J	1.3 J	0.53 J	4.9	0.19 U	2.2 J	3.6
trans-1,2-Dichloroethene	156-60-5	400	200	0.22 U	0.26 U	0.2 U	0.25 U	0.29 U	0.3 U	0.23 U	3.2 U	1.8 U
trans-1,3-Dichloropropene	10061-02-6	100	50	0.19 U	0.22 U	0.18 U	0.21 U	0.25 U	0.26 U	0.2 U	2.1 U	1.2 U
Trichloroethene	79-01-6	1,000	500	0.28 U	0.32 U	0.26 U	0.31 U	0.36 U	0.38 U	0.29 U	2.1 U	1.2 U
Trichlorofluoromethane	75-69-4	1,600	800	0.39 U	0.44 U	0.35 U	0.43 U	0.5 U	0.53 U	0.4 U	10 U	6 U
Vinyl chloride	75-01-4	100	50	0.29 U	0.33 U	0.27 U	0.32 U	0.38 U	0.4 U	0.31 U	4.2 U	2.4 U

Notes:
 All concentrations are ug/kg (ppb)
 (1) Action Levels & Cleanup Objectives, SCDHS Article 12 - SOP 9-95, August 2010.
 ** - Standard is determined on a case by case basis
 Highlighted text denotes concentrations exceeding SCDHS Action Levels.
 NS - No Standard
 U - Indicates that the analyte was not detected above the laboratory MDL
 J - Estimated value

Table 1
 Sample Analytical Results - Volatile Organic Compounds
 Gyrodyne Property (Industrial Area)
 St. James, New York

Table 2

Soil Sample Analytical Results - Semi-Volatile Organic Compounds
Gyrodyne Property (Industrial Area)
St. James, New York

CLIENT SAMPLE ID: LABORATORY ID: SAMPLING DATE	CAS Number	SCDHS Action Level	SCDHS Cleanup Objective	EP-11SLP L1807506-01 3/5/2018	EP-SD13 L1807506-05 3/5/2018	EP-SD17 L1807506-06 3/5/2018	SD-19 1/25/2018 L1802820-02	SD-20 1/25/2018 L1802820-01
Semi-Volatile Organic Compounds								
Acenaphthene	83-32-9	200,000	98,000	54,000 U	170 U	140 U	290 U	190 U
Anthracene	120-12-7	200,000	100,000	40,000 U	120 U	110 U	210 U	140 U
Benzo(a)anthracene	56-55-3	2,000	1,000	40,000 U	29 J	110 U	330	75 J
Benzo(a)pyrene	50-32-8	44,000	22,000	54,000 U	170 U	140 U	460	89 J
Benzo(b)fluoranthene	205-99-2	3,400	1,700	40,000 U	55 J	110 U	830	170
Benzo(ghi)perylene	191-24-2	200,000	100,000	54,000 U	33 J	140 U	400	96 J
Benzo(k)fluoranthene	207-08-9	3,400	1,700	40,000 U	120 U	110 U	240	49 J
Chrysene	218-01-9	2,000	1,000	40,000 U	38 J	110 U	560	130 J
Dibenzo(a,h)anthracene	53-70-3	200,000	100,000	40,000 U	120 U	110 U	95 J	140 U
Fluoranthene	206-44-0	200,000	100,000	40,000 U	62 J	110 U	760	190
Fluorene	86-73-7	200,000	100,000	68,000 U	210 U	180 U	36 J	230 U
Indeno(1,2,3-cd)pyrene	193-39-5	16,000	8,000	54,000 U	34 J	140 U	410	93 J
Phenanthrene	85-01-8	200,000	100,000	40,000 U	120 U	110 U	240	91 J
Pyrene	129-00-0	200,000	100,000	40,000 U	47 J	110 U	690	190

Notes:

- All concentrations are ug/kg (ppb)
- (1) Action Levels & Cleanup Objectives, SCDHS Article 12 - SOP 9-95, August 2010.
- ** - Standard is determined on a case by case basis
- Highlighted text denotes concentrations exceeding SCDHS Action Levels.
- NS - No Standard
- U - Indicates that the analyte was not detected above the laboratory MDL
- J - Estimated value

Table 3

Soil Sample Analytical Results - Total Metals
 Gyrodyne Property (Industrial Area)
 St. James, New York

CLIENT SAMPLE ID: LABORATORY ID: SAMPLING DATE	CAS Number	SCDHS Action Level	SCDHS Cleanup Objective	EP-12PLP1(MH-1) 2/27/2018 L1806744-03	SD-19 1/25/2018 L1802820-02	SD-20 1/25/2018 L1802820-01
Total Metals						
Arsenic, Total	7440-38-2	30	6	1.51	2.14	10.8
Barium, Total	7440-39-3	4,000	820	49	46.6	165
Beryllium, Total	7440-41-7	240	47	0.052 J	0.226 J	0.106 J
Cadmium, Total	7440-43-9	40	8	0.406 J	1.2	19.5
Chromium, Total	7440-47-3	100	20	14.7	23.4	90.3
Copper, Total	7440-50-8	8,500	1,700	99.1	152	367
Lead, Total	7439-92-1	2,000	450	24.7	66.5	1240
Mercury, Total	7439-97-6	3.7	0.7	1.8	0.26	0.17
Nickel, Total	7440-02-0	650	130	5.08	10.7	68.6
Selenium, Total	7782-49-2	NS	NS	0.804 J	NA	NA
Silver, Total	7440-22-4	50	10	0.484 J	0.871 U	0.475 J

Notes:

All concentrations are mg/kg (ppm)

(1) Action Levels & Cleanup Objectives, SCDHS Article 12 - SOP 9-95, August 2010.

** - Standard is determined on a case by case basis

Highlighted text denotes concentrations exceeding SCDHS Action Levels.

NS - No Standard

U - Indicates that the analyte was not detected above the laboratory MDL

J - Estimated value

APPENDIX A CORRESPONDENCE

COUNTY OF SUFFOLK

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STEVEN BELLONE
SUFFOLK COUNTY EXECUTIVE

DEPARTMENT OF HEALTH SERVICES

JAMES L. TOMARKEN, MD, MPH, MBA, MSW
COMMISSIONER

December 15, 2017

Gyrodyne LLC
1 Flowerfield, Suite 24
Saint James, NY 11780

Re: Flowerfield Industrial Park and Catering
1 Flowerfield, Saint James, NY

SCFR# 04458

Dear Sir or Madam,

This letter is to advise you that our office has reviewed the environmental assessment performed by PWG Consulting at the above-referenced location. Review of the laboratory analyses provided found one or more of the following compounds present in the sample(s) collected: *volatile organic compounds, semi-volatile organic compounds, and/or heavy metals*. The compounds found are present at concentrations indicative of unpermitted discharges of industrial waste.

Compounds that have exceeded the Suffolk County SOP-9-95 guidelines are considered toxic or hazardous and are not to be discharged to the ground surface, sanitary systems, storm drains, or any other leaching system. Please be advised that the discharge of any liquid from an industrial process without having first obtained a SPDES permit for that discharge is a violation of the New York State Environmental Conservation Law and Article 12 of the Suffolk County Sanitary Code. These regulations were promulgated to protect the groundwater, the drinking water resource in Suffolk County.

Due to the elevated levels found, **YOU ARE DIRECTED** to have all contaminated solids/sludge and liquids pumped from all contaminated structures as indicated below, **including all structures connected to them not previously sampled**. Please be advised that the remediation activity can only be accomplished by a **licensed industrial waste transporter**. The New York State Department of Environmental Conservation can verify the permit status of an industrial waste transporter. NYSDEC can be reached at (518) 402-8792 or by e-mail at transport@dec.ny.gov.

Flowerfield Catering

<u>Impacted Structure(s):</u>	<u>Contaminant(s) Found:</u>	<u>Endpoint(s) for:</u>
Sanitary Systems GT1, GT2, ST1, ST2, ST3, CP4, CP10, CP11	VOCs	VOCs
CP1, CP2	VOCs, Heavy Metals	VOCs, Heavy Metals

over-

Flowerfield Industrial Campus

<u>Impacted Structure(s):</u>	<u>Contaminant(s) Found:</u>	<u>Endpoint(s) for:</u>
Storm Water Drywells SD-13, SD-17	SVOCs	SVOCs
Sanitary Systems 9ST, 9PLP, 9SLPC, 10ST, 12PLP, 13ST, 13PLP, 14ST	VOCs	VOCs
7ST, 11ST, 12ST, 12PLP1	VOCs, Heavy Metals	VOCs, Heavy Metals
11SLP	VOCs, SVOCs	VOCs, SVOCs

Additional Requirements:

- Soil sample analysis from SD19 and the final discharge location of the two white PVC pipes on the south side of Building 2.
- Permanently disconnect the interior sink influent sources from SD10, SD15 and SD18 at the industrial campus as this is in violation of Suffolk County Sanitary Code.

Following the extraction of the contaminated soils from the leaching structures, confirmatory endpoint sample collection will be required to prove the remediation satisfactory. If endpoint samples or the nature of the contaminants indicate that further environmental contamination may be present, additional remedial measures including, but not limited to, a ground water investigation and/or soil vapor intrusion investigation will be required by the Department.

Failure to comply with the directives set forth in this letter by January 26, 2018 will result in this matter being scheduled for a formal administrative hearing at which time the Department will be seeking the imposition of the maximum penalties of \$2000.00 per day for each and every violation of the Suffolk County Sanitary Code including, but not limited to, failure to comply with the directives set forth in this letter. Your immediate attention to this matter is, therefore, expected.

Scope of Work to be Performed:

- Submit application and check for the remaining fee in the amount of \$550.00 made out to The Suffolk County Department of Health Services (SCDHS). *Fee covers Closure Review.*
- Retain the services of a licensed industrial waste transporter and/or environmental consultant. *Contact the NYSDEC regarding the permitting status of the waste transporter and disposal facility.*
- Provide a work plan describing the proposed remedial action.
- Contact the undersigned to schedule a remediation inspection/oversee additional requirements.
- Provide post-excavation end-point sample analyses for all impacted structures.
- Provide waste-disposal manifest(s) for all contaminated liquid and soil.
- Contact the United States Environmental Protection Agency at <http://www.epa.gov/safewater/uic> regarding their Underground Injection Control (UIC) program requirements.

All field activities must be scheduled at mutually agreeable times with the Department. If you have any questions concerning these matters or to schedule an appointment, please contact the undersigned.

Sincerely,



Peter Priolo
Public Health Sanitarian
Bureau of Environmental Investigation and Remediation
(631) 854-2545
CC: T. Melia, PWGC; NYSDEC, USEPA



Edward Roe
Senior Public Health Sanitarian

(631) 854-2534

APPENDIX B LABORATORY ANALYTICAL REPORTS

Serial_No:02011815:33

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ANALYTICAL REPORT

Lab Number:	L1802820
Client:	P. W. Grosser 630 Johnson Avenue Suite 7 Bohemia, NY 11716
ATTN:	Thomas Melia
Phone:	(631) 589-6353
Project Name:	GCA1704
Project Number:	GCA1704
Report Date:	02/01/18

The original project report/data package is held by Alpha Analytical. This report/data package is paginated and should be reproduced only in its entirety. Alpha Analytical holds no responsibility for results and/or data that are not consistent with the original.

Certifications & Approvals: MA (M-MA086), NH NELAP (2064), NJ NELAP (MA935), CT (PH-0574), IL (200077), ME (MA00086), MD (348), NY (11148), NC (25700/666), PA (68-03671), RI (LAO00065), TX (T104704476), VT (VT-0935), VA (460195), USDA (Permit #P330-14-00197).

Eight Walkup Drive, Westborough, MA 01581-1019
508-898-9220 (Fax) 508-898-9193 800-624-9220 - www.alphalab.com



Serial_No:02011815:33

Project Name: GCA1704
Project Number: GCA1704

Lab Number: L1802820
Report Date: 02/01/18

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Alpha Sample ID	Client ID	Matrix	Sample Location	Collection Date/Time	Receive Date
L1802820-01	SD-20	SOIL	ST. JAMES, NY	01/25/18 09:50	01/25/18
L1802820-02	SD-19	SOIL	ST. JAMES, NY	01/25/18 10:15	01/25/18
L1802820-03	11SLP	SOIL	ST. JAMES, NY	01/25/18 11:11	01/25/18
L1802820-04	7ST	SOIL	ST. JAMES, NY	01/25/18 11:35	01/25/18
L1802820-05	12PLP1	SOIL	ST. JAMES, NY	01/25/18 11:50	01/25/18
L1802820-06	12ST	SOIL	ST. JAMES, NY	01/25/18 12:00	01/25/18
L1802820-07	11ST	SOIL	ST. JAMES, NY	01/25/18 14:30	01/25/18
L1802820-08	CP001	SOIL	ST. JAMES, NY	01/25/18 13:30	01/25/18
L1802820-09	CP002	SOIL	ST. JAMES, NY	01/25/18 13:40	01/25/18
L1802820-10	CP010	SOIL	ST. JAMES, NY	01/25/18 13:50	01/25/18



Serial_No:02011815:33

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Project Name: GCA1704**Lab Number:** L1802820**Project Number:** GCA1704**Report Date:** 02/01/18

Case Narrative

The samples were received in accordance with the Chain of Custody and no significant deviations were encountered during the preparation or analysis unless otherwise noted. Sample Receipt, Container Information, and the Chain of Custody are located at the back of the report.

Results contained within this report relate only to the samples submitted under this Alpha Lab Number and meet NELAP requirements for all NELAP accredited parameters unless otherwise noted in the following narrative. The data presented in this report is organized by parameter (i.e. VOC, SVOC, etc.). Sample specific Quality Control data (i.e. Surrogate Spike Recovery) is reported at the end of the target analyte list for each individual sample, followed by the Laboratory Batch Quality Control at the end of each parameter. Tentatively Identified Compounds (TICs), if requested, are reported for compounds identified to be present and are not part of the method/program Target Compound List, even if only a subset of the TCL are being reported. If a sample was re-analyzed or re-extracted due to a required quality control corrective action and if both sets of data are reported, the Laboratory ID of the re-analysis or re-extraction is designated with an "R" or "RE", respectively. When multiple Batch Quality Control elements are reported (e.g. more than one LCS), the associated samples for each element are noted in the grey shaded header line of each data table. Any Laboratory Batch, Sample Specific % recovery or RPD value that is outside the listed Acceptance Criteria is bolded in the report. All specific QC information is also incorporated in the Data Usability format of our Data Merger tool where it can be reviewed along with any associated usability implications. Soil/sediments, solids and tissues are reported on a dry weight basis unless otherwise noted. Definitions of all data qualifiers and acronyms used in this report are provided in the Glossary located at the back of the report.

In reference to questions H (CAM) or 4 (RCP) when "NO" is checked, the performance criteria for CAM and RCP methods allow for some quality control failures to occur and still be within method compliance. In these instances the specific failure is not narrated but noted in the associated QC table. The information is also incorporated in the Data Usability format of our Data Merger tool where it can be reviewed along with any associated usability implications.

Please see the associated ADEx data file for a comparison of laboratory reporting limits that were achieved with the regulatory Numerical Standards requested on the Chain of Custody.

HOLD POLICY

For samples submitted on hold, Alpha's policy is to hold samples (with the exception of Air canisters) free of charge for 21 calendar days from the date the project is completed. After 21 calendar days, we will dispose of all samples submitted including those put on hold unless you have contacted your Client Service Representative and made arrangements for Alpha to continue to hold the samples. Air canisters will be disposed after 3 business days from the date the project is completed.

Please contact Client Services at 800-624-9220 with any questions.

Serial_No:02011815:33

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Project Name: GCA1704
Project Number: GCA1704

Lab Number: L1802820
Report Date: 02/01/18

Case Narrative (continued)

Report Submission

All non-detect (ND) or estimated concentrations (J-qualified) have been quantitated to the limit noted in the MDL column.

Volatile Organics

L1802820-01 and -02: Any reported concentrations that are below 200 ug/kg may be biased low due to the sample not being collected according to 5035-L/5035A-L low-level specifications.

Total Metals

The WG1085576-3 MS recovery, performed on L1802820-01, is outside the acceptance criteria for arsenic (208%). A post digestion spike was performed and was within acceptance criteria.

The WG1085576-3 MS recoveries, performed on L1802820-01, are outside the acceptance criteria for cadmium (225%) and nickel (126%). A post digestion spike was performed and yielded unacceptable recoveries for cadmium (61%) and nickel (70%). This has been attributed to sample matrix.

The WG1085576-3 MS recoveries for chromium (49%), copper (0%) and lead (270%), performed on L1802820-01, do not apply because the sample concentrations are greater than four times the spike amounts added.

The WG1085576-4 Laboratory Duplicate RPDs for chromium (46%) and copper (25%), performed on L1802820-01, are outside the acceptance criteria. The elevated RPDs have been attributed to the non-homogeneous nature of the native sample.

I, the undersigned, attest under the pains and penalties of perjury that, to the best of my knowledge and belief and based upon my personal inquiry of those responsible for providing the information contained in this analytical report, such information is accurate and complete. This certificate of analysis is not complete unless this page accompanies any and all pages of this report.

Authorized Signature:  Michelle M. Morris

Title: Technical Director/Representative

Date: 02/01/18



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ORGANICS



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VOLATILES



Project Name: GCA1704
Project Number: GCA1704

Serial_No:02011815:33

Lab Number: L1802820
Report Date: 02/01/18 Page G-30

SAMPLE RESULTS

Lab ID: L1802820-01
 Client ID: SD-20
 Sample Location: ST. JAMES, NY
 Sample Depth:
 Matrix: Soil
 Analytical Method: 1,8260C
 Analytical Date: 01/30/18 12:09
 Analyst: MV
 Percent Solids: 69%

Date Collected: 01/25/18 09:50
 Date Received: 01/25/18
 Field Prep: Not Specified

Parameter	Result	Qualifier	Units	RL	MDL	Dilution Factor
Volatile Organics by GC/MS - Westborough Lab						
Methylene chloride	ND		ug/kg	12	2.0	1
1,1-Dichloroethane	ND		ug/kg	1.8	0.32	1
Chloroform	ND		ug/kg	1.8	0.44	1
Carbon tetrachloride	ND		ug/kg	1.2	0.41	1
1,2-Dichloropropane	ND		ug/kg	4.2	0.27	1
Dibromochloromethane	ND		ug/kg	1.2	0.21	1
1,1,2-Trichloroethane	ND		ug/kg	1.8	0.38	1
Tetrachloroethene	ND		ug/kg	1.2	0.36	1
Chlorobenzene	ND		ug/kg	1.2	0.42	1
Trichlorofluoromethane	ND		ug/kg	6.0	0.50	1
1,2-Dichloroethane	ND		ug/kg	1.2	0.30	1
1,1,1-Trichloroethane	ND		ug/kg	1.2	0.42	1
Bromodichloromethane	ND		ug/kg	1.2	0.37	1
trans-1,3-Dichloropropene	ND		ug/kg	1.2	0.25	1
cis-1,3-Dichloropropene	ND		ug/kg	1.2	0.28	1
1,1-Dichloropropene	ND		ug/kg	6.0	0.39	1
Bromoform	ND		ug/kg	4.8	0.28	1
1,1,2,2-Tetrachloroethane	ND		ug/kg	1.2	0.36	1
Benzene	ND		ug/kg	1.2	0.23	1
Toluene	3.6		ug/kg	1.8	0.23	1
Ethylbenzene	ND		ug/kg	1.2	0.20	1
Vinyl chloride	ND		ug/kg	2.4	0.38	1
Chloroethane	ND		ug/kg	2.4	0.38	1
1,1-Dichloroethene	ND		ug/kg	1.2	0.45	1
trans-1,2-Dichloroethene	ND		ug/kg	1.8	0.29	1
Trichloroethene	ND		ug/kg	1.2	0.36	1
1,2-Dichlorobenzene	ND		ug/kg	6.0	0.22	1
1,3-Dichlorobenzene	ND		ug/kg	6.0	0.26	1
1,4-Dichlorobenzene	1.9	J	ug/kg	6.0	0.22	1



Serial_No:02011815:33

Project Name: GCA1704

Lab Number: L1802820

Project Number: GCA1704

Report Date: 02/01/18

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SAMPLE RESULTS

Lab ID: L1802820-01
 Client ID: SD-20
 Sample Location: ST. JAMES, NY
 Sample Depth:

Date Collected: 01/25/18 09:50
 Date Received: 01/25/18
 Field Prep: Not Specified

Parameter	Result	Qualifier	Units	RL	MDL	Dilution Factor
Volatile Organics by GC/MS - Westborough Lab						
Methyl tert butyl ether	ND		ug/kg	2.4	0.18	1
p/m-Xylene	ND		ug/kg	2.4	0.42	1
o-Xylene	ND		ug/kg	2.4	0.41	1
Xylenes, Total	ND		ug/kg	2.4	0.41	1
cis-1,2-Dichloroethene	ND		ug/kg	1.2	0.41	1
Dibromomethane	ND		ug/kg	12	0.29	1
Styrene	ND		ug/kg	2.4	0.48	1
Dichlorodifluoromethane	ND		ug/kg	12	0.60	1
Acetone	110		ug/kg	12	2.8	1
2-Butanone	27		ug/kg	12	0.83	1
4-Methyl-2-pentanone	ND		ug/kg	12	0.29	1
1,2,3-Trichloropropane	ND		ug/kg	12	0.21	1
Bromochloromethane	ND		ug/kg	6.0	0.43	1
2,2-Dichloropropane	ND		ug/kg	6.0	0.54	1
1,2-Dibromoethane	ND		ug/kg	4.8	0.24	1
1,3-Dichloropropane	ND		ug/kg	6.0	0.22	1
1,1,1,2-Tetrachloroethane	ND		ug/kg	1.2	0.38	1
Bromobenzene	ND		ug/kg	6.0	0.26	1
n-Butylbenzene	0.54	J	ug/kg	1.2	0.27	1
sec-Butylbenzene	0.83	J	ug/kg	1.2	0.26	1
tert-Butylbenzene	ND		ug/kg	6.0	0.30	1
o-Chlorotoluene	ND		ug/kg	6.0	0.26	1
p-Chlorotoluene	ND		ug/kg	6.0	0.22	1
1,2-Dibromo-3-chloropropane	ND		ug/kg	6.0	0.48	1
Hexachlorobutadiene	ND		ug/kg	6.0	0.42	1
Isopropylbenzene	ND		ug/kg	1.2	0.23	1
p-Isopropyltoluene	2.2		ug/kg	1.2	0.24	1
Naphthalene	1.7	J	ug/kg	6.0	0.16	1
n-Propylbenzene	0.44	J	ug/kg	1.2	0.26	1
1,2,3-Trichlorobenzene	ND		ug/kg	6.0	0.30	1
1,2,4-Trichlorobenzene	ND		ug/kg	6.0	0.26	1
1,3,5-Trimethylbenzene	0.69	J	ug/kg	6.0	0.19	1
1,2,4-Trimethylbenzene	1.3	J	ug/kg	6.0	0.22	1
p-Diethylbenzene	ND		ug/kg	4.8	4.8	1
p-Ethyltoluene	0.59	J	ug/kg	4.8	0.28	1
1,2,4,5-Tetramethylbenzene	0.75	J	ug/kg	4.8	0.19	1



Serial_No:02011815:33

Project Name: GCA1704

Lab Number: L1802820

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Project Number: GCA1704

Report Date: 02/01/18

SAMPLE RESULTS

Lab ID: L1802820-01
Client ID: SD-20
Sample Location: ST. JAMES, NY
Sample Depth:

Date Collected: 01/25/18 09:50
Date Received: 01/25/18
Field Prep: Not Specified

Parameter	Result	Qualifier	Units	RL	MDL	Dilution Factor
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Volatile Organics by GC/MS - Westborough Lab

Surrogate	% Recovery	Qualifier	Acceptance Criteria
1,2-Dichloroethane-d4	114		70-130
Toluene-d8	110		70-130
4-Bromofluorobenzene	123		70-130
Dibromofluoromethane	107		70-130



Project Name: GCA1704
Project Number: GCA1704

Serial_No:02011815:33

Lab Number: L1802820
Report Date: 02/01/18 Page G-33

SAMPLE RESULTS

Lab ID: L1802820-02
 Client ID: SD-19
 Sample Location: ST. JAMES, NY
 Sample Depth:
 Matrix: Soil
 Analytical Method: 1,8260C
 Analytical Date: 01/30/18 12:35
 Analyst: MV
 Percent Solids: 46%

Date Collected: 01/25/18 10:15
 Date Received: 01/25/18
 Field Prep: Not Specified

Parameter	Result	Qualifier	Units	RL	MDL	Dilution Factor
Volatile Organics by GC/MS - Westborough Lab						
Methylene chloride	ND		ug/kg	21	3.5	1
1,1-Dichloroethane	ND		ug/kg	3.2	0.57	1
Chloroform	ND		ug/kg	3.2	0.78	1
Carbon tetrachloride	ND		ug/kg	2.1	0.72	1
1,2-Dichloropropane	ND		ug/kg	7.4	0.48	1
Dibromochloromethane	ND		ug/kg	2.1	0.37	1
1,1,2-Trichloroethane	ND		ug/kg	3.2	0.66	1
Tetrachloroethene	ND		ug/kg	2.1	0.64	1
Chlorobenzene	ND		ug/kg	2.1	0.73	1
Trichlorofluoromethane	ND		ug/kg	10	0.88	1
1,2-Dichloroethane	ND		ug/kg	2.1	0.52	1
1,1,1-Trichloroethane	ND		ug/kg	2.1	0.74	1
Bromodichloromethane	ND		ug/kg	2.1	0.65	1
trans-1,3-Dichloropropene	ND		ug/kg	2.1	0.44	1
cis-1,3-Dichloropropene	ND		ug/kg	2.1	0.49	1
1,1-Dichloropropene	ND		ug/kg	10	0.69	1
Bromoform	ND		ug/kg	8.4	0.50	1
1,1,2,2-Tetrachloroethane	ND		ug/kg	2.1	0.63	1
Benzene	ND		ug/kg	2.1	0.41	1
Toluene	2.2	J	ug/kg	3.2	0.41	1
Ethylbenzene	0.49	J	ug/kg	2.1	0.36	1
Vinyl chloride	ND		ug/kg	4.2	0.66	1
Chloroethane	ND		ug/kg	4.2	0.66	1
1,1-Dichloroethene	ND		ug/kg	2.1	0.78	1
trans-1,2-Dichloroethene	ND		ug/kg	3.2	0.51	1
Trichloroethene	ND		ug/kg	2.1	0.64	1
1,2-Dichlorobenzene	ND		ug/kg	10	0.38	1
1,3-Dichlorobenzene	ND		ug/kg	10	0.46	1
1,4-Dichlorobenzene	ND		ug/kg	10	0.38	1



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Project Name: GCA1704

Lab Number: L1802820

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Project Number: GCA1704

Report Date: 02/01/18

SAMPLE RESULTS

Lab ID: L1802820-02
 Client ID: SD-19
 Sample Location: ST. JAMES, NY
 Sample Depth:

Date Collected: 01/25/18 10:15
 Date Received: 01/25/18
 Field Prep: Not Specified

Parameter	Result	Qualifier	Units	RL	MDL	Dilution Factor
Volatile Organics by GC/MS - Westborough Lab						
Methyl tert butyl ether	ND		ug/kg	4.2	0.32	1
p/m-Xylene	1.3	J	ug/kg	4.2	0.74	1
o-Xylene	2.2	J	ug/kg	4.2	0.71	1
Xylenes, Total	3.5	J	ug/kg	4.2	0.71	1
cis-1,2-Dichloroethene	ND		ug/kg	2.1	0.72	1
Dibromomethane	ND		ug/kg	21	0.50	1
Styrene	ND		ug/kg	4.2	0.84	1
Dichlorodifluoromethane	ND		ug/kg	21	1.0	1
Acetone	170		ug/kg	21	4.8	1
2-Butanone	70		ug/kg	21	1.4	1
4-Methyl-2-pentanone	ND		ug/kg	21	0.51	1
1,2,3-Trichloropropane	ND		ug/kg	21	0.37	1
Bromochloromethane	ND		ug/kg	10	0.75	1
2,2-Dichloropropane	ND		ug/kg	10	0.95	1
1,2-Dibromoethane	ND		ug/kg	8.4	0.42	1
1,3-Dichloropropane	ND		ug/kg	10	0.38	1
1,1,1,2-Tetrachloroethane	ND		ug/kg	2.1	0.67	1
Bromobenzene	ND		ug/kg	10	0.46	1
n-Butylbenzene	0.82	J	ug/kg	2.1	0.48	1
sec-Butylbenzene	ND		ug/kg	2.1	0.46	1
tert-Butylbenzene	ND		ug/kg	10	0.52	1
o-Chlorotoluene	ND		ug/kg	10	0.46	1
p-Chlorotoluene	ND		ug/kg	10	0.38	1
1,2-Dibromo-3-chloropropane	ND		ug/kg	10	0.83	1
Hexachlorobutadiene	ND		ug/kg	10	0.73	1
Isopropylbenzene	ND		ug/kg	2.1	0.41	1
p-Isopropyltoluene	1.1	J	ug/kg	2.1	0.42	1
Naphthalene	1.6	J	ug/kg	10	0.29	1
n-Propylbenzene	0.64	J	ug/kg	2.1	0.45	1
1,2,3-Trichlorobenzene	ND		ug/kg	10	0.53	1
1,2,4-Trichlorobenzene	ND		ug/kg	10	0.45	1
1,3,5-Trimethylbenzene	0.53	J	ug/kg	10	0.34	1
1,2,4-Trimethylbenzene	0.79	J	ug/kg	10	0.39	1
p-Diethylbenzene	ND		ug/kg	8.4	8.4	1
p-Ethyltoluene	ND		ug/kg	8.4	0.49	1
1,2,4,5-Tetramethylbenzene	0.72	J	ug/kg	8.4	0.33	1



Serial_No:02011815:33

Project Name: GCA1704

Lab Number: L1802820

Project Number: GCA1704

Report Date: 02/01/18

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SAMPLE RESULTS

Lab ID: L1802820-02
 Client ID: SD-19
 Sample Location: ST. JAMES, NY
 Sample Depth:

Date Collected: 01/25/18 10:15
 Date Received: 01/25/18
 Field Prep: Not Specified

Parameter	Result	Qualifier	Units	RL	MDL	Dilution Factor
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Volatile Organics by GC/MS - Westborough Lab

Surrogate	% Recovery	Qualifier	Acceptance Criteria
1,2-Dichloroethane-d4	122		70-130
Toluene-d8	115		70-130
4-Bromofluorobenzene	128		70-130
Dibromofluoromethane	109		70-130



Serial_No:02011815:33

Project Name: GCA1704
Project Number: GCA1704

Lab Number: Page G-36
 L1802820
Report Date: 02/01/18

**Method Blank Analysis
 Batch Quality Control**

Analytical Method: 1,8260C
 Analytical Date: 01/30/18 08:40
 Analyst: MV

Parameter	Result	Qualifier	Units	RL	MDL
Volatile Organics by 8260/5035 - Westborough Lab for sample(s): 01-02 Batch: WG1085079-5					
Methylene chloride	ND		ug/kg	10	1.6
1,1-Dichloroethane	ND		ug/kg	1.5	0.27
Chloroform	ND		ug/kg	1.5	0.37
Carbon tetrachloride	ND		ug/kg	1.0	0.34
1,2-Dichloropropane	ND		ug/kg	3.5	0.23
Dibromochloromethane	ND		ug/kg	1.0	0.18
1,1,2-Trichloroethane	ND		ug/kg	1.5	0.31
Tetrachloroethene	ND		ug/kg	1.0	0.30
Chlorobenzene	ND		ug/kg	1.0	0.35
Trichlorofluoromethane	ND		ug/kg	5.0	0.42
1,2-Dichloroethane	ND		ug/kg	1.0	0.25
1,1,1-Trichloroethane	ND		ug/kg	1.0	0.35
Bromodichloromethane	ND		ug/kg	1.0	0.31
trans-1,3-Dichloropropene	ND		ug/kg	1.0	0.21
cis-1,3-Dichloropropene	ND		ug/kg	1.0	0.23
1,1-Dichloropropene	ND		ug/kg	5.0	0.33
Bromoform	ND		ug/kg	4.0	0.24
1,1,2,2-Tetrachloroethane	ND		ug/kg	1.0	0.30
Benzene	ND		ug/kg	1.0	0.19
Toluene	ND		ug/kg	1.5	0.20
Ethylbenzene	ND		ug/kg	1.0	0.17
Vinyl chloride	ND		ug/kg	2.0	0.32
Chloroethane	ND		ug/kg	2.0	0.32
1,1-Dichloroethene	ND		ug/kg	1.0	0.37
trans-1,2-Dichloroethene	ND		ug/kg	1.5	0.24
Trichloroethene	ND		ug/kg	1.0	0.30
1,2-Dichlorobenzene	ND		ug/kg	5.0	0.18
1,3-Dichlorobenzene	ND		ug/kg	5.0	0.22
1,4-Dichlorobenzene	ND		ug/kg	5.0	0.18



Serial_No:02011815:33

Project Name: GCA1704

Lab Number:

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Project Number: GCA1704

Report Date:

L1802820

02/01/18

Method Blank Analysis
Batch Quality Control

Analytical Method: 1,8260C
Analytical Date: 01/30/18 08:40
Analyst: MV

Parameter	Result	Qualifier	Units	RL	MDL
Volatile Organics by 8260/5035 - Westborough Lab for sample(s): 01-02 Batch: WG1085079-5					
Methyl tert butyl ether	ND		ug/kg	2.0	0.15
p/m-Xylene	ND		ug/kg	2.0	0.35
o-Xylene	ND		ug/kg	2.0	0.34
Xylenes, Total	ND		ug/kg	2.0	0.34
cis-1,2-Dichloroethene	ND		ug/kg	1.0	0.34
Dibromomethane	ND		ug/kg	10	0.24
Styrene	ND		ug/kg	2.0	0.40
Dichlorodifluoromethane	ND		ug/kg	10	0.50
Acetone	ND		ug/kg	10	2.3
2-Butanone	ND		ug/kg	10	0.69
4-Methyl-2-pentanone	ND		ug/kg	10	0.24
1,2,3-Trichloropropane	ND		ug/kg	10	0.18
Bromochloromethane	ND		ug/kg	5.0	0.36
2,2-Dichloropropane	ND		ug/kg	5.0	0.45
1,2-Dibromoethane	ND		ug/kg	4.0	0.20
1,3-Dichloropropane	ND		ug/kg	5.0	0.18
1,1,1,2-Tetrachloroethane	ND		ug/kg	1.0	0.32
Bromobenzene	ND		ug/kg	5.0	0.22
n-Butylbenzene	ND		ug/kg	1.0	0.23
sec-Butylbenzene	ND		ug/kg	1.0	0.22
tert-Butylbenzene	ND		ug/kg	5.0	0.25
o-Chlorotoluene	ND		ug/kg	5.0	0.22
p-Chlorotoluene	ND		ug/kg	5.0	0.18
1,2-Dibromo-3-chloropropane	ND		ug/kg	5.0	0.40
Hexachlorobutadiene	ND		ug/kg	5.0	0.35
Isopropylbenzene	ND		ug/kg	1.0	0.19
p-Isopropyltoluene	ND		ug/kg	1.0	0.20
Naphthalene	ND		ug/kg	5.0	0.14
n-Propylbenzene	ND		ug/kg	1.0	0.22



Serial_No:02011815:33

Project Name: GCA1704
Project Number: GCA1704

Lab Number: Page G-38
 L1802820
Report Date: 02/01/18

**Method Blank Analysis
 Batch Quality Control**

Analytical Method: 1,8260C
 Analytical Date: 01/30/18 08:40
 Analyst: MV

Parameter	Result	Qualifier	Units	RL	MDL
Volatile Organics by 8260/5035 - Westborough Lab for sample(s): 01-02 Batch: WG1085079-5					
1,2,3-Trichlorobenzene	ND		ug/kg	5.0	0.25
1,2,4-Trichlorobenzene	ND		ug/kg	5.0	0.22
1,3,5-Trimethylbenzene	ND		ug/kg	5.0	0.16
1,2,4-Trimethylbenzene	ND		ug/kg	5.0	0.19
Freon-113	ND		ug/kg	20	0.51
p-Diethylbenzene	ND		ug/kg	4.0	4.0
p-Ethyltoluene	ND		ug/kg	4.0	0.23
1,2,4,5-Tetramethylbenzene	ND		ug/kg	4.0	0.16

Surrogate	%Recovery	Qualifier	Acceptance Criteria
1,2-Dichloroethane-d4	113		70-130
Toluene-d8	106		70-130
4-Bromofluorobenzene	110		70-130
Dibromofluoromethane	102		70-130



ALPHA

Parameter	LCS	LCSD	RPD	30
Methylene chloride	80	78	3	30
1,1-Dichloroethane	119	115	3	30
Chloroform	106	103	3	30
Carbon tetrachloride	108	104	4	30
1,2-Dichloropropane	116	116	0	30
Dibromochloromethane	106	104	2	30
1,1,2-Trichloroethane	105	104	1	30
Tetrachloroethene	99	96	3	30
Chlorobenzene	103	101	2	30
Trichlorofluoromethane	85	85	0	30
1,2-Dichloroethane	120	118	2	30
1,1,1-Trichloroethane	105	104	1	30
Bromodichloromethane	105	104	1	30
trans-1,3-Dichloropropene	115	113	2	30
cis-1,3-Dichloropropene	110	109	1	30
1,1-Dichloropropene	106	102	4	30
Bromoform	107	104	3	30
1,1,2,2-Tetrachloroethane	105	104	1	30
Benzene	99	97	2	30
Toluene	97	95	2	30
Ethylbenzene	102	100	2	30
Vinyl chloride	104	101	3	30
Chloroethane	86	77	11	30

Volatile Organics by 8260/5035 - Westborough Lab Associated sample(s): 01-02 Batch: WG1085079-3 WG1085079-4

Parameter	LCS	LCSD	RPD	RPD Limits
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Lab Control Sample Analysis

Batch Quality Control

Project Name: GCA1704

Project Number: GCA1704

Lab Number: L1802820

Report Date: 02/01/18

ALPHA

Parameter	LCS	%Recovery	Qual	LCS D	%Recovery	Qual	RPD	Qual	RPD
1,1-Dichloroethene	99			96			3		30
trans-1,2-Dichloroethene	99			95			4		30
Trichloroethene	100			98			2		30
1,2-Dichlorobenzene	96			95			1		30
1,3-Dichlorobenzene	98			97			1		30
1,4-Dichlorobenzene	98			98			0		30
Methyl tert butyl ether	96			96			0		30
p,m-Xylene	102			100			2		30
o-Xylene	107			105			2		30
cis-1,2-Dichloroethene	100			98			2		30
Dibromomethane	105			103			2		30
Styrene	101			99			2		30
Dichlorodifluoromethane	93			86			8		30
Acetone	159	Q		145	Q		9		30
2-Butanone	121			117			3		30
4-Methyl-2-pentanone	116			112			4		30
1,2,3-Trichloropropane	106			106			0		30
Bromochloromethane	106			104			2		30
2,2-Dichloropropane	116			112			4		30
1,2-Dibromomethane	99			99			0		30
1,3-Dichloropropane	108			106			2		30
1,1,1,2-Tetrachloroethane	104			103			1		30
Bromobenzene	99			97			2		30

Volatile Organics by 8260/5035 - Westborough Lab Associated sample(s): 01-02 Batch: WG1085079-3 WG1085079-4

Parameter	LCS	%Recovery	Qual	LCS D	%Recovery	Qual	RPD	Qual	RPD
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Lab Control Sample Analysis
Batch Quality Control

Project Name: GCA1704
Project Number: GCA1704

Lab Number: L1802820
Report Date: 02/01/18



Parameter	LCS	LCSD	RPD	Qual	RPD	Qual	RPD	Qual
n-Butylbenzene	105	102	70-130	3	30	3	30	30
sec-Butylbenzene	104	101	70-130	3	30	3	30	30
tert-Butylbenzene	101	98	70-130	3	30	3	30	30
o-Chloroluene	109	106	70-130	3	30	3	30	30
p-Chloroluene	104	102	70-130	2	30	2	30	30
1,2-Dibromo-3-chloropropane	92	93	68-130	1	30	1	30	30
Hexachlorobutadiene	90	88	67-130	2	30	2	30	30
Isopropylbenzene	104	102	70-130	2	30	2	30	30
p-Isopropylbenzene	101	99	70-130	2	30	2	30	30
Naphthalene	98	98	70-130	0	30	0	30	30
n-Propylbenzene	104	102	70-130	2	30	2	30	30
1,2,3-Trichlorobenzene	99	100	70-130	1	30	1	30	30
1,2,4-Trichlorobenzene	99	98	70-130	1	30	1	30	30
1,3,5-Trimethylbenzene	104	102	70-130	2	30	2	30	30
1,2,4-Trimethylbenzene	103	101	70-130	2	30	2	30	30
Freon-113	108	104	50-139	4	30	4	30	30
p-Diethylbenzene	105	103	70-130	2	30	2	30	30
p-Ethyltoluene	108	105	70-130	3	30	3	30	30
1,2,4,5-Tetramethylbenzene	102	101	70-130	1	30	1	30	30

Volatile Organics by 8260/5035 - Westborough Lab Associated sample(s): 01-02 Batch: WG1085079-3 WG1085079-4

Parameter	LCS	LCSD	RPD	Qual	RPD	Qual	RPD	Qual
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Lab Control Sample Analysis
Batch Quality Control

Project Name: GCA1704
Project Number: GCA1704

Lab Number: L1802820
Report Date: 02/01/18

ALPHA

Surrogate	LCS	LCS	LCS	LCSD	Acceptance
	%Recovery	Qual	%Recovery	Qual	Criteria
1,2-Dichloroethane-d4	112	113	70-130	113	70-130
Toluene-d8	108	107	70-130	107	70-130
4-Bromofluorobenzene	108	109	70-130	109	70-130
Dibromofluoromethane	107	106	70-130	106	70-130

Volatile Organics by 8260/5035 - Westborough Lab Associated sample(s): 01-02 Batch: WG1085079-3 WG1085079-4

Parameter	LCS	LCS	LCS	LCS	RPD
	%Recovery	Qual	%Recovery	Qual	RPD

Lab Control Sample Analysis
Batch Quality Control

Project Name: GCA1704
Project Number: GCA1704

Lab Number: L1802820
Report Date: 02/01/18

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SEMIVOLATILES

Serial_No:02011815:33

Project Name: GCA1704

Lab Number: L1802820 Page G-44

Project Number: GCA1704

Report Date: 02/01/18

SAMPLE RESULTS

Lab ID: L1802820-01
 Client ID: SD-20
 Sample Location: ST. JAMES, NY
 Sample Depth:
 Matrix: Soil
 Analytical Method: 1,8270D
 Analytical Date: 01/29/18 16:27
 Analyst: RC
 Percent Solids: 69%

Date Collected: 01/25/18 09:50
 Date Received: 01/25/18
 Field Prep: Not Specified
 Extraction Method: EPA 3546
 Extraction Date: 01/28/18 12:47

Parameter	Result	Qualifier	Units	RL	MDL	Dilution Factor
Semivolatile Organics by GC/MS - Westborough Lab						
Acenaphthene	ND		ug/kg	190	24.	1
Fluoranthene	190		ug/kg	140	27.	1
Benzo(a)anthracene	75	J	ug/kg	140	26.	1
Benzo(a)pyrene	89	J	ug/kg	190	57.	1
Benzo(b)fluoranthene	170		ug/kg	140	39.	1
Benzo(k)fluoranthene	49	J	ug/kg	140	37.	1
Chrysene	130	J	ug/kg	140	24.	1
Anthracene	ND		ug/kg	140	46.	1
Benzo(ghi)perylene	96	J	ug/kg	190	27.	1
Fluorene	ND		ug/kg	230	23.	1
Phenanthrene	91	J	ug/kg	140	28.	1
Dibenzo(a,h)anthracene	ND		ug/kg	140	27.	1
Indeno(1,2,3-cd)pyrene	93	J	ug/kg	190	32.	1
Pyrene	190		ug/kg	140	23.	1

Surrogate	% Recovery	Qualifier	Acceptance Criteria
Nitrobenzene-d5	52		23-120
2-Fluorobiphenyl	57		30-120
4-Terphenyl-d14	56		18-120



Serial_No:02011815:33

Project Name: GCA1704

Lab Number: L1802820 Page G-45

Project Number: GCA1704

Report Date: 02/01/18

SAMPLE RESULTS

Lab ID: L1802820-02
 Client ID: SD-19
 Sample Location: ST. JAMES, NY
 Sample Depth:
 Matrix: Soil
 Analytical Method: 1,8270D
 Analytical Date: 01/29/18 16:53
 Analyst: RC
 Percent Solids: 46%

Date Collected: 01/25/18 10:15
 Date Received: 01/25/18
 Field Prep: Not Specified
 Extraction Method: EPA 3546
 Extraction Date: 01/28/18 12:47

Parameter	Result	Qualifier	Units	RL	MDL	Dilution Factor
Semivolatile Organics by GC/MS - Westborough Lab						
Acenaphthene	ND		ug/kg	290	37.	1
Fluoranthene	760		ug/kg	210	41.	1
Benzo(a)anthracene	330		ug/kg	210	40.	1
Benzo(a)pyrene	460		ug/kg	290	87.	1
Benzo(b)fluoranthene	830		ug/kg	210	60.	1
Benzo(k)fluoranthene	240		ug/kg	210	57.	1
Chrysene	560		ug/kg	210	37.	1
Anthracene	ND		ug/kg	210	70.	1
Benzo(ghi)perylene	400		ug/kg	290	42.	1
Fluorene	36	J	ug/kg	360	35.	1
Phenanthrene	240		ug/kg	210	44.	1
Dibenzo(a,h)anthracene	95	J	ug/kg	210	41.	1
Indeno(1,2,3-cd)pyrene	410		ug/kg	290	50.	1
Pyrene	690		ug/kg	210	36.	1

Surrogate	% Recovery	Qualifier	Acceptance Criteria
Nitrobenzene-d5	77		23-120
2-Fluorobiphenyl	78		30-120
4-Terphenyl-d14	78		18-120

